

Agenda

Ordinary Meeting of Council

Monday, 10th December 2018

Commencing at 7.00pm

Council Chamber
1230 Nepean Highway, Cheltenham

kingston.vic.gov.au

John Nevins
Chief Executive Officer
Kingston City Council

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City of
KINGSTON

**City of Kingston
Ordinary Meeting of Council**

Agenda

10 December 2018

Notice is given that an Ordinary Meeting of Kingston City Council will be held at 7.00pm at Council Chamber, 1230 Nepean Highway, Cheltenham, on Monday, 10 December 2018.

1. Apologies

2. Confirmation of Minutes of Previous Meetings

Minutes of Ordinary Council Meeting 26 November 2018

3. Foreshadowed Declaration by Councillors, Officers or Contractors of any Conflict of Interest

Note that any Conflicts of Interest need to be formally declared at the start of the meeting and immediately prior to the item being considered – type and nature of interest is required to be disclosed – if disclosed in writing to the CEO prior to the meeting only the type of interest needs to be disclosed prior to the item being considered.

4. Petitions

Nil

5. Presentation of Awards

Southern United Hockey Club

6. Reports from Delegates Appointed by Council to Various Organisations

7. Question Time

8. Planning and Development Reports

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11.4	Tenders for Contract No 18/115 - Provision of Internal Audit Services	
	Appendix 1 CON-18 115 - Tender Evaluation Matrix	

8. Planning and Development Reports

Explanation of Meeting Procedure



Meeting Procedure is Regulated by Local Law

The procedures for this Ordinary Meeting of Council are regulated by Council's Meeting Procedures Local Law.

Chairperson

The Mayor as Chairperson is the ultimate authority for the conduct of the meeting.

Agenda

The business to be dealt with at the meeting is set out in the agenda. No other business can be dealt with, unless admitted as Urgent Business by resolution of Council.

Motions

A motion must be moved and seconded to be valid. The mover of the motion will then be permitted to speak to it. Other Councillors will then be permitted to speak either for or against the motion. The mover will be permitted a right-of-reply, which will conclude the debate.

Voting

The motion will then be voted on by show of hands. If the motion is carried, it becomes a resolution (decision) of the Council. Any Councillor may call for a Division, in order that the vote of each Councillor is formally recorded. The result of the Division supersedes the vote by show of hands.

Amendments

A Councillor may move an amendment to a motion. Any amendment moved shall be dealt with in the same way as a motion, except that there is no right of reply for the mover of the amendment and the mover of the motion if the amendment is carried. If carried, the amendment becomes the motion and the previous motion is abandoned.

Speaking at the Meeting

No visitor to a Council meeting may speak to the meeting, except for:

- The applicant (or his/her representative) and one objector in relation to an application for a planning permit;
- Special circumstances in which leave to speak is granted by the Chairperson.

Unless special circumstances apply, the Chairperson will limit the presentation of a speaker to three minutes duration.

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Explanation of Meeting Procedure

Questions

Members of the public present at the meeting may put questions in writing to Council which will be dealt with during Question Time. The Question Box is located in the foyer. Questions must be placed in the Question Box by 7.30pm. You don't have to be a resident to ask a question.

Questions are to be as succinct as possible. Questions which cannot be accommodated on the single sided question form provided are likely to require research, and are more appropriately directed to Council in the form of a letter. In such cases, the question/s may be answered in writing at the direction of the Chairperson subsequent to the meeting.

Questions will be answered in the Council Chamber only if the questioner is present in the gallery. Where a questioner is not present, a response will be provided in writing.

Individual members of the public are permitted to ask a maximum of three (3) questions.

Confidential Business

The meeting may be closed at any time to deal with confidential items in camera. In these instances members of the public will be asked to leave the Council Chamber, and the meeting re-opened once the confidential business is completed.

Courtesy to the Mayor

All Councillors are required to direct their attention towards the Chairperson when speaking. This is in accordance with protocols relating to respect for the Chairperson of a meeting, and is a requirement of Council's Meeting Procedures Local Law.

Emergency Evacuation of Chamber

Members of the public are requested to note the green and white EXIT signs.

In the event of an emergency requiring evacuation of the Chamber, the public should evacuate by way of the EXIT located to the right hand side of the Council Chamber. This leads to the foyer through which you passed in order to enter the Chamber. Proceed from the foyer through the revolving door/side door and out of the building. This is the primary evacuation route.

If the nature of the emergency is such that the primary evacuation route is impracticable, the public should evacuate by way of the EXIT located to the right of the Council table as viewed from the public gallery. Follow further EXIT signs thereafter, which lead to an exit point on the south side of the building. This is the secondary evacuation route.

Council staff will issue directions on how to proceed to evacuate in the event of an emergency.

Explanation of Meeting Procedure

Do You Have a Hearing Difficulty?

Phonic Ear Hearing Assistance is available to any member of the public gallery with a hearing disability. Just ask a member of staff for a unit prior to the meeting.

Language Line



Recording of Meetings

Council Meetings are recorded and streamed live on the internet.

Recordings are archived and available on Council's website www.kingston.vic.gov.au.

All care is taken to maintain your privacy; however as a visitor in the public gallery, your presence may be recorded.

Ordinary Meeting of Council

10 December 2018

Agenda Item No: 8.1

TOWN PLANNING APPLICATION DECISIONS - NOVEMBER 2018

Contact Officer: Carly De Mamiel, Senior Customer Liaison and Administration Officer

Town Planning Application Decisions – November 2018

Approved By: Jonathan Guttman - General Manager, Planning & Development

Author: Ian Nice – Manager, City Development

Attached for information is the report of Town Planning Decisions for the month of November, 2018.

A summary of the decisions is as follows:

Type of Decision	Number of Decisions Made	Percentage (%)
Planning Permits	84	77
Notice of Decision	16	15
Refusal to Grant a Permit	4	4
Other - Withdrawn (2) - Prohibited (0) - Permit not required (0) - Lapsed (2) - Failure to Determine (0)	4	4
Total	108	100

(NB: Percentage figures have been rounded)

OFFICER RECOMMENDATION

That the report be noted.

Appendices

Appendix 1 - Town Planning Application Decisions November 2018 (Ref 18/621208)



**City of Kingston
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10 December 2018

Author/s: Carly De Mamiel, Senior Customer Liaison and Administration Officer

Reviewed and Approved By: Naomi Crowe, Team Leader City Development Administration
Ian Nice, Manager City Development

8.1

TOWN PLANNING APPLICATION DECISIONS - NOVEMBER 2018

1	Town Planning Application Decisions November 2018	9
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Planning Decisions November, 2018							
APPL. No.	PROPERTY ADDRESS	SUBURB	APPL. DATE	DATE DECIDED	PROPOSAL DESCRIPTION	DECISION	VCAT DECISION
KP-1997/899/A	2 220 Balcombe Road	MENTONE	27/07/2018	1/11/2018	Develop and use this site for a dual occupancy	Notice of Decision	No
KP-2018/216	4 Tarago Crescent	CLAYTON SOUTH	22/03/2018	1/11/2018	The development of two (2) dwellings and associated works	Notice of Decision	No
KP-2018/884	310-312 Governor Road	BRAESIDE	22/10/2018	1/11/2018	Installation of a pylon sign	Permit Issued	No
KP-2018/352	15 Sandford Street	HIGHETT	11/05/2018	1/11/2018	Develop the land for the construction of two (2) dwellings	Lapsed	No
KP-2018/380	10 Lennox Street	MOORABBIN	22/05/2018	2/11/2018	Use the site for the storage and sales of motor vehicles	Lapsed	No
KP-2018/491	14 Ebb Street	ASPENDALE	21/06/2018	2/11/2018	Develop the land for the construction of two (2) double storey dwellings	Notice of Decision	No
KP-2017/742	107 Wells Road	CHELSEA HEIGHTS	25/09/2017	2/11/2018	Develop the land for the construction of 65 commercial warehouse buildings and ancillary office space with a reduction to car parking	Withdrawn	No
KP-2018/793	1 Aonach Street	CLAYTON SOUTH	1/10/2018	2/11/2018	The subdivision of land into three (3) lots	Permit Issued	No
KP-2018/757	165 Parkers Road	PARKDALE	18/09/2018	2/11/2018	Subdivide the Land into Three (3) Lots	Permit Issued	No
KP-2018/872	9-11 Barilla Road	MOORABBIN	26/10/2018	5/11/2018	Develop the land for the construction of building & works	Permit Issued	No
KP-2018/823	40 1 Kingston Road	HEATHERTON	8/10/2018	5/11/2018	The development of an office mezzanine extension within an existing warehouse	Permit Issued	No
KP-2018/270	5 Genoa Avenue	BONBEACH	13/04/2018	7/11/2018	Develop the land for the construction of four (4) dwellings	Permit Issued	No

Planning Decisions November, 2018							
APPL. No.	PROPERTY ADDRESS	SUBURB	APPL. DATE	DATE DECIDED	PROPOSAL DESCRIPTION	DECISION	VCAT DECISION
KP-2018/169	4 Broadway	BONBEACH	7/03/2018	7/11/2018	Develop the land for the construction of a single storey dwelling to the rear of an existing, and buildings and works associated with the existing dwelling	Permit Issued	No
KP-2018/64	2A Latrobe Street	MENTONE	30/01/2018	7/11/2018	The development of three (3) dwellings	Permit Issued	No
KP-2014/687/A	11 Turner Road	HIGHETT	4/04/2018	7/11/2018	Amending the ground floor layout of townhouse 1 and 3 and addition of a first floor to townhouse 3	Permit Issued	No
KP-2015/147/A	Unit 1 3 Canberra Street	PATTERSON LAKES	28/08/2018	7/11/2018	Develop the land for the construction of six (6) double storey dwellings	Permit Issued	No
KP-2018/574	3 150-156 Chesterville Road	CHELTENHAM	17/07/2018	7/11/2018	The use of a restricted recreation facility	Permit Issued	No
KP-2018/291	9 Saunders Street	CLAYTON SOUTH	18/04/2018	7/11/2018	Develop the land for the construction of three (3) double storey dwellings	Permit Issued	No
KP-2018/677	126 Bourke Road	CLARINDA	23/08/2018	7/11/2018	Develop the Land for the Construction of two (2) Dwellings	Notice of Decision	No
KP-2018/804	35 Levanswell Road	MOORABBIN	5/10/2018	7/11/2018	Subdivide the Land into seven (7) Lots	Permit Issued	No
KP-2017/952	42 Sutton Street	CHELSEA HEIGHTS	7/12/2017	7/11/2018	Develop the land for the construction of two (2) double storey dwellings	Notice of Decision	No
KP-2018/439	53 Rennison Street	PARKDALE	4/06/2018	7/11/2018	Develop the land for the construction of two (2) double storey side-by-side dwellings	Notice of Decision	No
KP-2018/759	503 Clayton Road	CLAYTON SOUTH	19/09/2018	7/11/2018	Subdivide the land into Four (4) Lots	Permit Issued	No

Planning Decisions November, 2018							
APPL. No.	PROPERTY ADDRESS	SUBURB	APPL. DATE	DATE DECIDED	PROPOSAL DESCRIPTION	DECISION	VCAT DECISION
KP-2018/871	318 Station Street	CHELSEA	25/10/2018	7/11/2018	Buildings and works associated with the existing building	Permit Issued	No
KP-2016/1054	6 Yalambee Avenue	ASPENDALE	19/12/2016	8/11/2018	Develop the land for the construction of two (2) double storey dwellings	Permit Issued	Yes
KP-2011/743/A	1 11 Hall Mark Road	MORDIALLOC	13/08/2018	9/11/2018	The use of the land for a medical centre and a reduction of the car parking requirement	Permit Issued	No
KP-2018/195	34 Henry Street	HIGHETT	16/03/2018	9/11/2018	Develop the land for the construction of two (2) dwellings which exceed the maximum building height requirement pertaining to Schedule 12 of the Design and Development Overlay	Permit Issued	No
KP-2018/819	573 Nepean Highway	BONBEACH	10/10/2018	9/11/2018	The subdivision of land into two (2) lots	Permit Issued	No
KP-2018/667	17 Jacobs Drive	CLARINDA	21/08/2018	9/11/2018	Subdivide the Land into Two (2) Lots	Permit Issued	No
KP-2018/632	12 Mount View Road	HIGHETT	2/08/2018	9/11/2018	The development of two (2) single storey dwellings (side by side)	Permit Issued	No
KP-2018/885	25 Harpley Street	CHELTENHAM	1/11/2018	9/11/2018	the construction of an extension to the front of the existing dwelling on land in a Special Building Overlay	Permit Issued	No
KP-2018/695	189 Chesterville Road	MOORABBIN	29/08/2018	9/11/2018	Use the land for leisure and recreation (dance studio)	Permit Issued	No
KP-2018/214	40 Elder Street	CLARINDA	26/03/2018	9/11/2018	Develop the land for the construction of four (4) dwellings	Permit Issued	No
KP-2018/569	246 Lower Dandenong Road	MORDIALLOC	13/07/2018	9/11/2018	Alterations and additions to existing dwelling and construction of one (1) double storey dwelling to the rear with associated parking	Permit Issued	No

Planning Decisions November, 2018							
APPL. No.	PROPERTY ADDRESS	SUBURB	APPL. DATE	DATE DECIDED	PROPOSAL DESCRIPTION	DECISION	VCAT DECISION
KP-2018/901	52 Royena Road	MOORABBIN	8/11/2018	12/11/2018	Subdivide the Land into Two (2) Lots	Permit Issued	No
KP-2017/794	21 Moore Avenue	CLAYTON SOUTH	12/10/2017	12/11/2018	Develop the land for the construction of four (4) double storey dwellings	Permit Issued	No
KP-2018/515	7 Derby Parade	BONBEACH	29/06/2018	12/11/2018	Develop the land for construction of a single storey dwelling to the rear of an existing dwelling	Permit Issued	No
KP-2017/131	7 Deals Road	CLAYTON SOUTH	23/02/2017	12/11/2018	Use and development of the land as a Residential Transfer Station	Permit Issued	No
KP-2017/983	86 Albert Street	MORDIALLOC	18/12/2017	12/11/2018	Develop the land for the construction of five (5) dwellings with basement car parking and associated works	Permit Issued	No
KP-2014/616/C	24 Rae Avenue	EDITHVALE	29/08/2018	12/11/2018	Develop the land for two (2) dwellings comprising the construction of a new double storey dwelling to the rear of an existing dwelling (to be retained)	Permit Issued	No
KP-2018/500	42 Fifth Street	PARKDALE	27/06/2018	12/11/2018	Develop the land for the construction of two (2) dwellings with associated works	Permit Issued	No
KP-2017/845	1 21 James Street	MORDIALLOC	1/11/2017	12/11/2018	Develop the land for the construction of two (2) dwellings	Notice of Decision	No
KP-2015/961/A	42 Centre Dandenong Road	CHELTENHAM	12/09/2018	12/11/2018	Develop the land for the construction of three (3) dwellings	Permit Issued	No
KP-2016/551/A	19 Hartwell Place	CHELTENHAM	23/10/2018	12/11/2018	Develop the land for the construction of two (2) dwellings	Permit Issued	No
KP-2018/276/A	1144 Nepean Highway	HIGHETT	12/11/2018	12/11/2018	Subdivide the Land into One Hundred and Fifty-four (154) Lots and to vary and remove easements	Permit Issued	No

Planning Decisions November, 2018							
APPL. No.	PROPERTY ADDRESS	SUBURB	APPL. DATE	DATE DECIDED	PROPOSAL DESCRIPTION	DECISION	VCAT DECISION
KP-2018/393	1 Tilley Street	CHELTENHAM	24/05/2018	13/11/2018	Develop the land for the construction of two (2) double storey dwellings	Notice of Decision	No
KP-2018/875	46 Railway Parade	HIGHETT	29/10/2018	13/11/2018	Develop the land for alterations and extension to an existing dwelling	Permit Issued	No
KP-2018/593	1127 Nepean Highway	HIGHETT	23/07/2018	14/11/2018	Develop the land for the construction of a two (2) storey residential building comprising of six (6) dwellings	Notice of Decision	No
KP-2018/533	6 Ivy Street	PARKDALE	4/07/2018	14/11/2018	The development of two (2) double storey dwellings (side by side)	Notice of Decision	No
KP-2018/878	1 24 Sherwood Avenue	CHELSEA	30/10/2018	14/11/2018	Subdivide the Land into Five (5) Lots	Permit Issued	No
KP-2018/820	23 Yackatoon Avenue	ASPENDALE	10/10/2018	14/11/2018	Removal of Easement	Permit Issued	No
KP-2017/841/A	2 Bream Bay	PATTERSON LAKES	18/09/2018	15/11/2018	Develop the land for the construction of two (2) dwellings	Permit Issued	No
KP-2015/114/B	40-46 Canterbury Road	BRAESIDE	23/01/2018	15/11/2018	Staged subdivision of the land	Permit Issued	No
KP-2018/227	100 Lochiel Avenue	EDITHVALE	26/03/2018	16/11/2018	The development of two (2) dwellings on land affected by a Special Building Overlay	Permit Issued	No
KP-2018/550	403-405 Lower Dandenong Road	DINGLEY VILLAGE	9/07/2018	19/11/2018	Develop the land for the construction of a fuel tank for the existing warehouse	Permit Issued	No
KP-2017/882	16 Edsall Street	HIGHETT	13/11/2017	20/11/2018	Develop the land for the construction of three (3) dwellings	Permit Issued	No

Planning Decisions November, 2018							
APPL. No.	PROPERTY ADDRESS	SUBURB	APPL. DATE	DATE DECIDED	PROPOSAL DESCRIPTION	DECISION	VCAT DECISION
KP-2018/387	310-312 Governor Road	BRAESIDE	24/05/2018	20/11/2018	Subdivide the Land into twenty seven (27) lots	Permit Issued	No
KP-2018/511	238-242 Chesterville Road	MOORABBIN	2/07/2018	20/11/2018	Use of land for Industry (Bakery) and an ancillary Food and Drink Premises (Cafe/Commercial Bakery), associated buildings and works, a reduction in the car parking requirements, alteration of access in a Road Zone Category 1 and erection of advertising signage	Permit Issued	No
KP-2018/675	8 57-59 Whiteside Road	CLAYTON SOUTH	22/08/2018	20/11/2018	Construction of a mezzanine level to provide ancillary office space and a reduction of the car parking requirement by one (1) space.	Permit Issued	No
KP-2018/652	8 417-419 Warrigal Road	CHELTENHAM	14/08/2018	20/11/2018	Use the site for the storage and sales of motor vehicles	Permit Issued	No
KP-2018/285	5 Higham Street	CHELTENHAM	18/04/2018	20/11/2018	Develop the land for the construction of two (2) dwellings	Permit Issued	No
KP-2018/513	4 Bayliss Street	CHELTENHAM	2/07/2018	20/11/2018	Develop the land for the construction of two (2) dwellings	Permit Issued	No
KP-2018/671	4 Larnook Crescent	ASPENDALE	21/08/2018	20/11/2018	The development of two (2) dwellings	Notice of Decision	No
KP-2017/896	Sixth Avenue	ASPENDALE	20/11/2017	20/11/2018	Remove native vegetation pursuant to Clause 52.17 of the Kingston Planning Scheme	Permit Issued	Yes
KP-2018/917	4 Laura Street	CLAYTON SOUTH	16/11/2018	22/11/2018	Subdivide the Land into Two (2) Lots	Permit Issued	No
KP-2018/923	16 Eighth Street	PARKDALE	20/11/2018	22/11/2018	Subdivide the Land into Two (2) Lots	Permit Issued	No

Planning Decisions November, 2018							
APPL. No.	PROPERTY ADDRESS	SUBURB	APPL. DATE	DATE DECIDED	PROPOSAL DESCRIPTION	DECISION	VCAT DECISION
KP-2018/307	1 Riviera Street	MENTONE	26/04/2018	22/11/2018	The development of a double storey dwelling, and buildings and works associated with the existing dwelling	Permit Issued	No
KP-2018/518	92 Turner Road	HIGHETT	2/07/2018	22/11/2018	The development of three (3) dwellings	Notice of Decision	No
KP-2018/879	8 10A Byng Avenue	HEATHERTON	23/10/2018	23/11/2018	Buildings and works (replacement dwelling) associated with existing retirement village	Permit Issued	No
KP-2018/472	25 Donald Grove	CHELSEA	14/06/2018	23/11/2018	Develop the land for the construction of three (3) dwellings	Notice of Decision	No
KP-2018/858	9 594 Nepean Highway	BONBEACH	19/10/2018	23/11/2018	Replace existing verandah roof on rear first storey balcony with opening and closing roof	Permit Issued	No
KP-2018/525	21-29 Inverness Street	CLARINDA	3/07/2018	27/11/2018	Develop the land for a three storey extension and works to the existing residential aged care facility	Notice of Decision	No
KP-2013/126/C	105-115 Cochranes Road	MOORABBIN	10/10/2018	27/11/2018	The construction of buildings and works comprising of thirty-seven (37) warehouse/industrial units and to use the land and the construction of buildings and works for one (1) food and drinks premises (cafe) and one (1) office and a reduction in the car parking requirements	Withdrawn	No
KP-2018/585	4 Owen Street	MORDIALLOC	19/07/2018	27/11/2018	Develop the land for the construction of two (2) dwellings	Notice of Decision	No
KP-2018/926	14 Balmoral Drive	PARKDALE	20/11/2018	27/11/2018	Develop the land for the construction of building & works	Permit Issued	No
KP-2018/639	48 Alex Avenue	MOORABBIN	7/08/2018	27/11/2018	Develop the land for the construction of buildings and works and one (1) Business Identification sign that exceeds an area of 8m ²	Permit Issued	No

Planning Decisions November, 2018							
APPL. No.	PROPERTY ADDRESS	SUBURB	APPL. DATE	DATE DECIDED	PROPOSAL DESCRIPTION	DECISION	VCAT DECISION
KP-2009/391/A	56 Levenswell Road	MOORABBIN	19/04/2018	28/11/2018	The construction of buildings and works on this site, in accordance with the endorsed plans, and a reduction in the car parking requirement.	Permit Issued	No
KP-2017/613	16 Ruvina Street	ASPENDALE	9/08/2017	28/11/2018	Develop the land for the construction of two (2) dwellings	Notice of Decision	No
KP-2018/333	21 Olive Grove	MENTONE	4/05/2018	28/11/2018	Develop the land for the construction of four (4) dwellings	Refused	No
KP-2018/741	7 Abbey Lane	EDITHVALE	11/09/2018	28/11/2018	Buildings and works for the construction of a roof top deck on an existing dwelling	Refused	No
KP-2015/480/A	Lot 1 Sixth Avenue	ASPENDALE	11/10/2018	28/11/2018	Subdivide the land into two (2) lots and removal of native vegetation	Permit Issued	No
KP-1991/3558/A	2 55 Fourth Street	PARKDALE	13/06/2018	28/11/2018	Dual Occupancy	Permit Issued	No
KP-2018/613	92 Lower Dandenong Road	PARKDALE	26/07/2018	28/11/2018	Develop the land for the construction of two (2) dwellings and alter access to a road zone, category 1	Permit Issued	No
KP-2018/256	572-574 Heatherton Road	CLAYTON SOUTH	9/04/2018	28/11/2018	Use and develop the land for a Transfer station (retrospective application) and in a Public Acquisition Overlay	Refused	No
KP-2050/1	1230 Nepean Highway	CHELTENHAM	26/06/2018	28/11/2018	Test Only - Please Ignore	Permit Issued	No
KP-2018/3	29 Neville Street	MENTONE	2/01/2018	28/11/2018	Develop the land for the construction of two (2) double storey dwellings	Permit Issued	No

Planning Decisions November, 2018							
APPL. No.	PROPERTY ADDRESS	SUBURB	APPL. DATE	DATE DECIDED	PROPOSAL DESCRIPTION	DECISION	VCAT DECISION
KP-2018/374	14 Maury Road	CHELSEA	21/05/2018	28/11/2018	Develop the land for the construction of three (3) dwellings with associated works and to subdivide the land into three (3) lots with common property	Permit Issued	No
KP-2017/730	19 Imes Street	PARKDALE	15/09/2017	28/11/2018	Develop the land for the construction of three (3) dwellings with associated works	Permit Issued	No
KP-2018/231	2 4 Brady Close	BRAESIDE	29/03/2018	28/11/2018	Use the site for animal boarding (cattery) and ancillary caretaker dwelling	Permit Issued	No
KP-2018/700	132-136 Keys Road	CHELTENHAM	31/08/2018	28/11/2018	Use of the land for the sale and consumption of liquor	Permit Issued	No
KP-2018/536	91 Valetta Street	CARRUM	9/07/2018	29/11/2018	Extensions and alterations to existing dwelling including extension to ground floor level footprint and addition of first floor level	Permit Issued	No
KP-2018/163	39 Bradshaw Street	MORDIALLOC	6/03/2018	29/11/2018	Develop the land for the construction of two (2) double storey dwellings	Permit Issued	No
KP-2018/626	170 Centre Dandenong Road	CHELTENHAM	1/08/2018	29/11/2018	The development of two (2) double storey dwellings	Permit Issued	No
KP-2018/265	101 Springs Road	CLAYTON SOUTH	10/04/2018	29/11/2018	Use the site as a medical centre with associated buildings and works, and the display of advertising signage	Permit Issued	No
KP-2018/182	1 Natal Avenue	EDITHVALE	14/03/2018	29/11/2018	Develop the land for the construction of two (2) dwellings	Permit Issued	No
KP-2017/72	11 Elora Road	OAKLEIGH SOUTH	3/02/2017	29/11/2018	Develop the land for the construction of more than one (1) dwellings	Permit Issued	Yes

Planning Decisions November, 2018							
APPL. No.	PROPERTY ADDRESS	SUBURB	APPL. DATE	DATE DECIDED	PROPOSAL DESCRIPTION	DECISION	VCAT DECISION
KP-2016/601/B	1 16 The Strand	CHELSEA	4/06/2018	29/11/2018	Develop the land for the construction of alterations and additions to an existing dwelling on a lot less than 300 square metres	Permit Issued	No
KP-2014/1025/A	Unit 1 97 Albert Street	MORDIALLOC	8/08/2018	29/11/2018	Develop the land for the construction of three (3) dwellings and the retention of an existing dwelling	Permit Issued	No
KP-2016/848/A	1318 Nepean Highway	CHELTENHAM	16/10/2018	29/11/2018	Develop the land for the construction of six (6) double storey dwellings on land affected by a Special Building Overlay	Permit Issued	No
KP-2018/881	3 Burns Avenue	CLAYTON SOUTH	31/10/2018	29/11/2018	Subdivide the Land into Four (4) Lots	Permit Issued	No
KP-2018/843	437 Main Street	MORDIALLOC	19/10/2018	29/11/2018	Subdivide the Land into eight (8) Lots	Permit Issued	No
KP-2018/821	18 Devon Street	CHELTENHAM	11/10/2018	29/11/2018	Subdivide the Land into Two (2) Lots	Permit Issued	No
KP-2016/841/B	10-14 Cochranes Road	MOORABBIN	5/10/2018	29/11/2018	Use the land for motor vehicle sales	Permit Issued	No
KP-2018/779	1 Kimpton Street	CHELTENHAM	26/09/2018	29/11/2018	Subdivide the land into Two (2) Lots	Permit Issued	No
KP-2018/666	1 Latrobe Street	CHELTENHAM	21/08/2018	29/11/2018	Subdivide the Land into Eleven (11) Lots	Permit Issued	No
KP-2018/413	4 Tennyson Avenue	CLAYTON SOUTH	30/05/2018	29/11/2018	Subdivide the land into three (3) lots	Permit Issued	No

Planning Decisions November, 2018							
APPL. No.	PROPERTY ADDRESS	SUBURB	APPL. DATE	DATE DECIDED	PROPOSAL DESCRIPTION	DECISION	VCAT DECISION
KP-2018/354	11 Venice Street	MENTONE	11/05/2018	29/11/2018	Partial demolition and alterations and additions to the pavilion in a Heritage Overlay Schedule 67, construction of a new double storey pavilion and informal car park in the Activity Centre Zone and Heritage Overlay Schedule 67 and Design and Development Overlay Schedule 1, use the land for an infor	Permit Issued	No
KP-2017/821	2 Trent Court	BONBEACH	25/10/2017	30/11/2018	Develop the land for the construction of three (3) dwellings	Refused	No

Ordinary Meeting of Council

10 December 2018

Agenda Item No: 8.2

KP-1993/5158 - 19-71 CARROLL ROAD OAKLEIGH SOUTH

Contact Officer: Hugh Charlton, Statutory Planner

Purpose of Report

This report is for Council to consider Planning Permit Application No. KP-1993/5158 - 19-71 Carroll Road Oakleigh South.

Disclosure of Officer / Contractor Direct or Indirect Interest

No Council officer/s and/or Contractor/s who have provided advice in relation to this report have declared a Conflict of Interest regarding the matter under consideration.

OFFICER RECOMMENDATION

That Council determine to support the proposal and issue a Notice of decision to grant an amended planning permit at 19-71 Carroll Road Oakleigh South, subject to the conditions contained within this report.

PLANNING OFFICER REPORT	
APPLICANT	Cleanaway Solid Waste Pty Ltd
ADDRESS OF LAND	19-71 Carroll Road Oakleigh South
PLAN OF SUBDIVISION REFERENCE	Allotment 4 and 5, Section B of the Parish of Mordialloc; • Lot 1 on Plan of Subdivision 148203A; • Lot 2 on Plan of Subdivision 419257B; • Lot 1 on Title Plan 130031A; • Lot 1 on Title Plan 558887Y; • Lot 1 on Title Plan 585075N; • Lot 1 on Title Plan 738419H; • Lot 1 on Title Plan 745262A; and • Lot 2 on Plan of Subdivision 328353M.
PROPOSAL	Retrospectively apply for an increase to the landfill volume and alter the landfill cap height and position of the southern part of the existing solid inert waste landfill
PLANNING OFFICER	Hugh Charlton
REFERENCE NO.	KP-1993/5158 /A
ZONE	Clause 37.01: Special Use Zone – Schedule 2
OVERLAYS	Clause 42.02: Environmental Significance Overlay 3
OBJECTIONS	170
CONSIDERED PLAN REFERENCES/DATE RECEIVED	Prepared by Golder, Figures 1 to 2, Rev 0 to 2, dated 15/8/18, received by Council on 25 October 2018
ABORIGINAL CULTURAL HERITAGE SENSITIVITY	Exempt

1.0 RELEVANT HISTORY

- 1.1. This application to amend plans was originally submitted on 28 April 2017 as part of a secondary consent amendment. Council's officers advised that a section 72 amendment must instead be applied for as the proposal, which then formed the current application.
- 1.2. Council records indicate the following relevant history:
- Landfill. Late 1960s to early 1970s. A portion of the site was used for landfill, known as the Oakleigh tip.
 - Sand mining was permitted in 1970 and operated until the 1980's by virtue of Permit 56615 issued by Melbourne Metropolitan Board of Works on 11 Nov 1970 for the purpose of Sand Extraction. This included land up to Old Dandenong Rd (records are not definitive if this included the Mavis Hutter reserve). There was no time limit on the permit for the completion of the extraction.
- 1.3. Planning Permit MBN 5158 (KP-5158/1993) was issued on 25 May 1993 which included:
- Issued in accordance with the decision of the Administrative Appeals Tribunal dated 18 March 1993. The permit required the use to commence by 25 May 1995 (condition 38) and for the tipping of waste material permitted until 25 May 2010 (condition 11).
 - Multiple extensions of time have since been granted, the final of which was granted on 1 July 2015 for the filling activities associated with condition 11 to be completed by 31 August 2015 and to allow the ongoing operations of the solid inert transfer waste station until 31 August 2017 in accordance with endorsed plans on 24 May 2000.

- Rehabilitation of the landfill does not have a prescribed timeframe under the permit.
- Included land up to Old Dandenong Rd.
- This included the ancillary weigh bridge and transfer station.
- The City of Moorabbin and Pioneer Concrete were joint applicants.
- Plans showing approved contour caps were endorsed on 24 May 2000.
- Council held approximately 1/5 of the available 2.5 million cubic metres of airspace.
- This permit was originally refused by the Minister for Planning and Urban Growth, but was overturned by the Administrative Appeals Tribunal.
- An amendment under secondary consent was granted on 30 September 2015 under delegation by Council (all ward Councillors consented to a delegated decision). The approved landfill cap was increased at two crests by 1-2 metres, and allowed to relocate the crests generally further away from the site boundaries. The southern crest (near the park) remained unaltered.

2.0 SITE & SURROUNDS

- 2.1. The subject site is located on the western side of Carroll Road, Oakleigh South.
- 2.2. The site is irregular in shape, has an area of approximately 37 hectares and a frontage width to Carroll Road of approximately 900 metres. There is a 6 metre fall within the site from the north to the south.
- 2.3. The specific site area, i.e. the area of change, is located on the southern end of the site adjacent to Mavis Hutter Reserve and the residential area. The proposed area of change measures approximately 6.5ha, and spans approximately a maximum depth of 335 metres from the southern boundary to the Reserve, set back 25 metres from the western side boundary and set back a minimum of 19 metres from the eastern (Carroll Road).
- 2.4. The remainder of the site is undergoing or completed the main rehabilitation works, thus the contours generally represent the final land height.
- 2.5. There are no restrictions or easements listed on the Certificate of Title for the affected site area.
- 2.6. The following map illustrates the subject site in its surrounding context.



- 2.7. The area surrounding the subject site is predominately residential in nature, zoned for General Residential on the opposite side of Carroll Road and Old Dandenong Road. This area contains a mix of single and double storey dwellings on predominantly on their original allotments.
- 2.8. Industrial zoned land is located on the north end of Carroll Road beyond Bunney Road.
- 2.9. Immediately to the west is land known as the Commonwealth Golf Club, immediately to the south is the 'Mavis Hutter' Public Open Space Reserve, and existing residential area beyond. The triangle of land abutting Old Dandenong Road, immediately west of the Mavis Hutter Reserve, is presently vacant. A planning application has been submitted by the Commonwealth Golf Club for this land to be used and developed for a maintenance building for the golf club. This is under consideration and has not reached advertising stage as yet.

3.0 PROPOSAL

- 3.1. The application proposes to retrospectively apply for an increase to the landfill volume and alter the landfill cap height and position of the southern part of the existing solid inert waste landfill.
- 3.2. The landfill capping has three crests — a northern, central and southern crest — with no changes proposed to the north and central crests. Changes are proposed to the southern crest on the southern end of the site in the area as previously described.
- 3.3. The application seeks permission for adding extra landfill from 2015 and prior, where in the past the then operator dumped a larger amount of landfill than was permitted. The applicant is proposing to keep the extra landfill on site instead of relocating it to another landfill.

- 3.4. The extra landfill can be seen today as high mounding on the southern side of the site near the park, Mavis Hutter Reserve. This is not the final form.
- 3.5. The final contours proposed will result in the southern crest to be 2.8 metres higher and 188,000 cubic metres greater than currently approved.
- 3.6. The applicant is not seeking to add more waste that is in there today or to reopen the landfill.
- 3.7. The future works proposed in this application is to reshape the hill of landfill and make it lower and rounder than it appears today, and then to cover it with grass, shrubs and some trees.
- 3.8. The reshaping works would take around 2 additional months, for a total of 12 months including the required rehabilitation works from the planning permit. This would include truck deliveries of additional soil and gravel and earthworks on site.
- 3.9. Cleanaway have advised that the alternative is to remove that extra landfill and relocate it outside of the City of the Kingston which would take around 1 additional year (2 years work in total). This would occur if Council were to refuse the application, and that refusal was not contested by the applicant. The landfill to be relocated is generally demolition and construction waste, and not food waste or green waste.
- 3.10. In detail the proposed works include:
- An additional 188,000m³ of over-height material (i.e. additional landfill) on the southern portion of the site (from Applicant's survey data), across an area of approx. 6.5 ha.
 - To increase the southern crest of the landfill by an additional 2.8 metres in overall height. The previously endorsed plan (Figure 2) shows the southern crest of the landfill as having a maximum relative level (RL) of 55. The proposed plan increases this level to 57.8RL.
 - Above Carroll Road (measured to its closest point) the peak height is proposed to be 12 metres above, compared to 9 metres above for the current approval.
 - Above Mavis Hutter Reserve, the peak height is proposed to be 10 metres above, compared to 7 metres above for the current approval.
 - The top of the crest is in generally the same location as the current approval, approximately 210 metres from Carroll Road and 180 metres from Mavis Hutter Reserve.
 - The proposed height of the southern crest remains lower than the highest point of the landfill to the north, approximately 3 metres lower. Note that there is a 6 metre fall within the site from north to south.
 - Additional indigenous ground shrub and ground cover along the southern and eastern boundaries and surrounding the southern crest on the remaining side. 'Small trees' have been indicatively labelled near the southern and eastern boundaries, however no specifics have been provided.
 - Northern boundary of the Mavis Hutter Reserve will be realigned by a depth of approx. 5.3m to 23.4m, to return an area of approx. 2080 sq.m. from the subject site which was incorrectly fenced in. There is no landfill in this location. Physically there will be a change to the boundary fencing location, but the plans do not change as they follow the correct title boundaries.
 - A 3m wide access track is proposed abutting and within the site boundary around the perimeters. This is detailed in writing but the alignment is not shown on plans.
 - No changes to the existing permit conditions are sought.

4.0 PLANNING PERMIT REQUIREMENTS

Zone

- 4.1. Special Use Zone – Schedule 2 (Earth and Energy Resources Industry). Pursuant to Schedule 2 of Clause 37.01 –2, a planning permit would be required for a new use for ‘Refuse disposal’ which includes a landfill. The proposal relies on the existing approved use as a solid inert landfill, thus there is **no** permit trigger for a new use under the zone. A planning permit **is required** under Clause 37.01-4 to construct or carry out works.

Overlay

- 4.2. Environmental Significance Overlay 3. This overlay recognises and protects significant trees. The mapped area is situated around a *Eucalyptus cornuta* tree on the western boundary of the subject site, although not within proximity to the affected site area. Thus there is **no** planning permit trigger under Clause 42.01-2, unless works are within its tree protection zone (TPZ). Council’s Vegetation officer has recommended conditions to ensure there are no works in the TPZ of this tree.

Particular Provisions

- 4.3. Clause 53.10 – Uses with Adverse Amenity Potential. The Table in Clause 53.10 specifies a threshold distance for uses which may cause additional detriment to sensitive land such as residential zoned land. The site was approved and used as a landfill prior to these controls being gazetted into the Kingston Planning Scheme. The proposal relies on the existing approved use, thus there is **no** planning permit trigger under Clause 53.10.
- 4.4. Clause 52.17 – Native vegetation. This applies to the remnant native trees along the boundaries as the land is in one ownership and greater than 0.4ha. The application indicates works near these trees. Should the works intrude into the tree protection zones of these trees, a planning permit is triggered under Clause 52.17 to remove, destroy, or lop native vegetation, including dead vegetation.

Conditions are recommended by Council’s Vegetation Officer, should a permit issue, to protect the tree protection zones of the trees on site to ensure that vegetation is not adversely impacted on, nor will a permit be triggered.

Council’s Vegetation Officer advises that:

- The site has three mapped Pre-1750 Ecological Vegetation Classes (EVC’s). Plains Grassy Woodland/Swamp Scrub/Plains Grassy Wetland Mosaic (927) in the middle of site, on either side of this is a strip of Grassy Woodland (175) in middle of site, and Heathy Woodland (48) north and south.
- The subject site has (remnant) native vegetation along the Carroll Road boundary including *Eucalyptus ovata* (Swamp Gum), *Eucalyptus viminalis* subsp. *pryoriana* (Coast Manna Gum) and *Leptospermum laevigatum* (Coast Tea-tree). There may be more but as access to the site was not available this can’t be confirmed.
- A 3m wide maintenance/access track is proposed abutting and within the site boundary around the perimeters. If this access track is constructed along the boundary to the site it will result in an encroachment into the TPZ, or removal of the native trees along the Carroll Road boundary. This will require the applicant to submit an application under Clause 52.17. Any encroachment greater than 10 % into the TPZ of a retained tree assumes that the tree is ‘lost’ (removed). Any application to remove native vegetation under Clause 52.17 also requires any tree removal from the site in the previous 5 years to be included and also any removal from land in contiguous ownership (Commonwealth Golf Club).

General Provisions

- 4.5. The Decision Guidelines of Clause 65 of the Kingston Planning Scheme are relevant to this application and require consideration to be given to a variety of matters including planning scheme policies, the purpose of the zone, orderly planning and the impact on amenity.

5.0 AMENDMENT TO THE APPLICATION AFTER NOTIFICATION

- 5.1. A formal amendment pursuant to Clause 57A of the Kingston Planning Scheme was submitted on 23 October 2018. This amendment made submitted in response to Council officer and objector concerns and made the following changes:

- Proposed peak of southern crest lowered by 2.2 metres;
- Southern peak is reshaped to be broader with generally a lower gradient;
- Note that no landfill is removed, as the amendment initially proposed.

6.0 ADVERTISING

- 6.1. The proposal was advertised by sending notices to adjoining and opposite property owners and occupiers and by maintaining a notice on site for fourteen (14) days.

- 6.2. 170 objections to the proposal were received. The valid grounds of objection raised are summarised as follows:

- Non-compliance with permit and request to remove fill
- Visual bulk of additional fill
- Dust impact and mitigation
- Noise impact
- Odours
- Health implications
- Neighbourhood character
- Overshadowing
- Potential contamination
- Unlawful dumping
- Extended activities on site, lack of notification, commitment to timelines

- 6.3. Following advertising period and receipt of objections, Council sent additional notification to those properties immediately opposite the subject site on Carroll Road and Old Dandenong Road. The letter gave further clarity to the proposal as there appeared in objections to be some misunderstandings about the proposal from the broader community.

7.0 PLANNING CONSULTATION MEETING

- 7.1. Two planning consultation meeting were held, on 28 February and 9 October 2018, with the relevant Planning Officer, the Permit Applicant and 11 or 13 objectors, respectively, in attendance. A Councillor and the EPA were also present for the first meeting. The above-mentioned issues were discussed at length.

- 7.2. The applicant presented the amended plans at the second meeting.

- 7.3. The above concerns were unable to be resolved at the meetings, and the objections still stand.

8.0 REFERRALS

Department / Area	Comments / Rationale / Recommended Conditions
Council's Vegetation Management Officer	No objection to the proposal subject to conditions for a detailed landscape plan showing dense on-boundary planting of trees and vegetation, trees and vegetation around the peak of the southern crest consistent with the master landscape plan provided, and all indigenous species with specific Ecological Vegetation Classes. The proposed access track adjacent to the boundary fencing is requested to be outside of tree protection zones for existing trees, along with protection of trees on site and adjoining the site, particularly the significant tree near the western boundary protected by the ESO.
Council's Engineering Design	No objection raised, and did not require any conditional changes.
EPA Victoria	EPA Victoria raised no objection subject to the following condition being included: 'The revised contour plan should be consistent with the Kingston City Council planning requirements, and council approval should be obtained'

9.0 PLANNING CONSIDERATIONS:

9.1. Planning Policy Framework

The application has been assessed against the Planning Policy Framework and it is considered that the proposed use and/or development is consistent with relevant policies contained within this section of the Kingston Planning Scheme.

The proposal allows for the continued use of industry in an area designated for industrial development. It supports **Clause 11** Settlement, **Clause 15** Built Environment and Heritage, and **Clause 17** Economic Development, which are closely aligned with the objectives and policy outcomes sought by the Metropolitan Strategy – 'Plan Melbourne: Metropolitan Planning Strategy' (Department of Transport, Planning and Local Infrastructure, 2014), through the location of well-designed industrial development within a designated industrial area, together with strengthening the competitiveness of Melbourne's employment land, and providing jobs closer to where people live.

The relevant objectives includes **Clause 13.05-1S** Noise Abatement which seeks "*to assist the control of noise effects on sensitive land uses*" and **Clause 13.06-1S** Air Quality management which seeks "*to assist the protection and improvement of air quality*", and Clause 13.07-1S Land use compatibility which seeks "*to safeguard community amenity while facilitating appropriate commercial, industrial or other uses with potential off-site effects*". Subject to further consideration and improvements, the proposal will be able to meet the stringent EPA conditions considered adequate to mitigate off-site amenity impacts and thus will meet these objectives.

Clause 15.03-2 – Aboriginal Cultural Heritage. Importantly, the Subject Land is identified in an area of Aboriginal Cultural Heritage Sensitivity. Council Officer's are satisfied knowing the site's history includes sand extraction of 2.5 million cubic metres of airspace, therefore significant ground disturbance has already occurred to a material part of the land. Therefore it has been established that the proposed activity is **exempt** from requiring a Cultural Heritage Management Plan.

Clause 19.03-6S (Waste and resource recovery) seeks to reduce waste and maximise resource recovery so as to reduce reliance on landfills and minimise environmental, community amenity and public health impacts. Strategies include to ensure waste and resource recovery facilities are sited, designed, built and operated so as to minimise impacts on surrounding communities and the environment. Council should consider as relevant the **Best Practice Environmental Management Guideline** (Siting, Design, Operation and Rehabilitation of Landfills) (Environment Protection Authority, 2001). This document has been superseded by EPA Victoria's Siting, design, operation and rehabilitation of landfills, Publication 788.3 August, 2015.

9.2. **Local Planning Policy Framework**

The application has been assessed against the Local Planning Policy Framework and it is considered that the proposed development is consistent with relevant policies contained within this section of the Kingston Planning Scheme.

Clause 21.03 'Land use challenges for the new millennium' identifies the following key land use planning objectives relevant to the proposed subdivision:

Sandbelt open space strategy: Rehabilitation of landfill sites should be properly co-ordinated to provide for the timely development of regional open space networks through the Sand belt Open Space Strategy.

Clause 21.04– 2 'Key land use themes' Kingston's vision for future land use planning and development is expressed around a number of key land use themes. These themes provide a basis for the more detailed objectives, strategies and implementation measures outlined in Section 5.0 of the MSS. Open space is one of the key themes been identified in Kingston Planning Schemes.

Clause 21.11 'Open Space' gives an overview, key issues, objectives, strategies and implementation within Kingston's community infrastructure and services required by future population.

Clause 21.11–2 'Key issues' identifies the following key issues in Kingston:

- *Impact of changes in population structure on the recreation and open space needs of current and future populations.*
- *Need for flexible, multi-functional open space areas which are able to be adapted to meet changing user needs.*
- *Greater consideration of transport and physical access issues required to ensure accessibility to open space areas for ageing population.*
- *Need to balance the competing demands of recreation users with management of environmentally sensitive areas.*
- *Opportunities for the creation of a major north-south spine of regional open space through Sandbelt Open Space Strategy.*

Clause 21.11–3 'Objectives, strategies and implementation' specifies the following objectives within the Kingston Planning Scheme:

Objective 1: To provide fair and equitable access to a range of high quality open space areas located within Kingston's urban and non-urban environments which aim to optimise community enjoyment of open space.

Objective 2: To promote a diverse range of social and recreational opportunities which provide for the changing leisure needs of the municipality's current and future populations.

Objective 3: To protect significant natural landscapes and open space areas with an identified environmental significance from degradation as a result of community recreational demands.

Objective 4: To promote the creation of a major regional north-south spine of open space within a predominantly non-urban context.

Clause 22.03 (Sandbelt Open Space Project Policy) applies to the land and land across Heatherton and Dingley areas, seeking to establish a chain of parks on land formerly used for sand extraction and landfill. Key objectives include to promote the development of a series of connected parks providing for a wide range of regional and local recreation opportunities for both passive and active recreational pursuits not widely accessible elsewhere. It is also policy that the landscape character of these areas along with previous vegetation patterns and character be promoted and restored. The policy seeks to achieve following objectives:

- *To implement the objectives of the Sandbelt Open Space Project and associated Development Plan.*
- *To promote the development of a series of connected parks providing for a wide range of regional and local recreation opportunities within a quality environment.*
- *To promote the rehabilitation and conversion of extractive industrial and landfill site to open space or other productive after uses which are compatible with the Sandbelt.*
- *Open Space Project. To recognise different concepts of leisure and recreation and provide for equitable access to both passive and active recreational pursuits not widely accessible elsewhere.*

Overall the proposed works are considered to be consistent with state and local policy and provide final contours that would allow a future use for open space. The proposal will not impact any current mitigation measures for off-site amenity controls for dust, water, air management, and has received favourable support from the relevant authority EPA Victoria.

9.3. Zoning Provisions

The purpose of the Special Use Zone Schedule 2 (SUZ2) seeks:

- *To recognise or provide for the use and development of land for earth and energy resources industry.*
- *To encourage interim use of the land compatible with the use and development of nearby land.*
- *To encourage land management practice and rehabilitation that minimises adverse impact on the use and development of nearby land.*

There are no specific guidelines in the zone or schedule to the zone.

The proposed works accords with the purpose to the zone. The zone identifies that the land should be used in conjunction with the earth and energy resources industry. The proposal maintains the approved use for a solid inert landfill which is in conjunction with the earth and energy resources industry.

The purpose to encourage an interim use of the land is not a relevant consideration in this instance. The landfill will need to rehabilitate over a period of decades and there are implications with health and safety and requirements that prevent any interim use. There is no end use known at the present stage.

With respect to the policy projections, local policy indicates that the future use of this land should be used for open space and recreation as part of Council's Sandbelt Open Space framework at Clause 22.03 which seeks to:

'promote the rehabilitation and conversion of extractive industrial and landfill sites to open space or other productive after uses which are compatible with the Sandbelt Open Space Project.'

Where it is policy that:

'High quality public and private open space areas be provided which cater for a range of active and passive recreational pursuits, including golf, outdoor adventure/education activities, playing fields, recreational trails, equestrian activities, environmental and cultural experiences.'

The revised change to the contours are unlikely to prejudice future open space opportunities. The amended proposal creates an undulating land form generally with gentle gradients. This reflects the undulating land that has already been created on the north and central parts of the site, and is considered consistent with open space. It is noted that a flat land form cannot be created on landfill sites as there is a need for a constant grade or slight hill in order to control and capture rainwater runoff.

In general the proposal presents an acceptable outcome that accords with the purpose of the zone to minimise adverse impacts on surrounding use and development, as:

- The revised contours proposed have been reviewed to minimise the overall height difference to 2.8 metres above the maximum approved contour height and shape the crest in a gentle manner.
- The crest of the hill remains closer to the golf course and sited away from the public realm and residential area, approximately 210 metres from Carroll Road and 180 metres from Mavis Hutter Reserve, thereby reducing any apparent visual impact.
- These changes are considered adequate to minimise any adverse impact on the use and development of nearby land.
- The proposal meets the EPA's best practice guidelines for rehabilitation as demonstrated by the following assessment. The revised contours sought as part of this application has been reviewed and approved by EPA Victoria.
- The application makes improvements to the landscape character across the site beyond what was previously approved. This minimises any adverse impacts on the surrounds and ensure the proposal better integrates with its residential and open space context.

The proposed landscaping improvements are an important gain as these will provide an indigenous setting across the southern end of the site. This will complement the existing mature treeline along the side boundaries and tie together the open space of the golf course and Council reserve. This will also meet Clause 22.02 open space objectives to re-establish the landscape character of these areas along with previous vegetation patterns, to promote and restore the native vegetation character. Further conditions are recommended should a permit issue to quantify the exact landscaping in a manner that does not structurally damage the landfill capping.

With this the proposal will be able to achieve the intended future strategic outcomes for a chain of parks sought under Clause 22.03, along with improving the landscape character and promoting and restoring previous vegetation patterns and character.

EPA Victoria’s Best Practice Guidelines: Siting, design, operation and rehabilitation of landfills, Publication 788.3 August, 2015.

The majority of these guidelines are not relevant to this assessment as the site is an existing landfill and its base, its separation from boundaries, and its management remains unchanged. All emission controls, dust, air, water, and post-management such as gas monitoring and extraction will not be altered by this proposal.

An assessment against the relevant considerations of the Best Practice Guidelines is as follows:

BEST PRACTICE	ASSESSMENT	COMPLIANCE
5 Best-practice siting considerations		
5.1 Screening of potential of the potential landfill site	Additional landscaping comprising shrubs and trees is proposed around the periphery of the site against the Council reserve to screen the contours, and across the southern end of the site where the capping can support such vegetation.	Complies, subject to condition should a permit issue, for provision of a full landscape plan comprising details of the plantings, species, density, height etc and predominantly indigenous plantings.
5.1.1 - 5.1.10	These sections are not relevant as the operation of the landfill and its core design remains unchanged by the amendment.	N/A
6 Best-practice design		
6.1-6.5	These sections are not relevant as the operation of the landfill and its core design remains unchanged by the amendment.	N/A
6.6 Groundwater	Council's Drainage Engineer has reviewed the revised contours and does not raise any concern as to its impact on the runoff or groundwater.	
6.7 Air Quality	The applicant advises the objectives will be met for air quality management: <ul style="list-style-type: none"> • <i>no health, safety or environmental impacts due to landfill gas and dust</i> • <i>minimise greenhouse gas emissions</i> • <i>the prevention of offsite nuisance odours and dust</i> • <i>meet requirements of relevant EPA regulations and State Environment Protection Policies (SEPP)</i> 	Complies
6.9 Noise	The applicant advises that the objectives will be met for noise management by compliance with EPA's SEPP (Control of Noise from Commerce, Industry and Trade)	Complies
6.10 Traffic considerations	The proposal will continue to maintain the limited access routes, speed, hours of operation, parking locations.	Complies

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BEST PRACTICE	ASSESSMENT	COMPLIANCE
	Site access for all vehicles remains near the industrial area. Truck routes will continue to be via the industrial area, and will obey the no truck signage along Carroll Road towards Old Dandenong Road.	
6.11 Site security and fencing	Existing cyclone fencing will retained with shade cloth that has already been installed to reduce dust emissions.	Complies
8 Best-practice rehabilitation and aftercare	Assessment	Compliance
8.1.1 Rehabilitation plan	The plans show as required <ul style="list-style-type: none"> • surface contours before and after settlement • specifications and materials to be used in the final cap • preservation/installation of environment performance control or monitoring features. 	Complies
8.1.2 Progressive rehabilitation	Progressive rehabilitation has occurred as desired by the closure and rehabilitation of each cell once filling has been completed during the operating life of the landfill. This is the last cell to be capped, vegetated and rehabilitated.	Complies
8.1.3 Triggers for rehabilitation	It is understood that intermediate (temporary) capping has been installed on the present cell before it can be fully rehabilitated.	Complies
8.1.4 Site afteruse	As previously discussed there is no known afteruse, but it is likely due to policy direction and its abuttal to be used for open space. The final contours provide gentle slopes that comply with the direction to avoid steep slopes to ensure it could be are consistent with public open-space use.	Complies
8.1.5 Settlement and final surface profile	The proposal complies with the recommended grades: <ul style="list-style-type: none"> • Cap gradients are not less than 1% thus avoids issues with water ponding • Caps are not steeper than 20%. 	Complies
8.1.6 Landfill cap	The cap comprising soil, clay, geomembranes etc to achieve a long-term stable barrier between waste and the environment is understood to remain as per the previously approved proposal. The existing permit Condition 35b) specifies a 500mm depth of cap which is an appropriate depth of soil to support adequate vegetation for erosion control.	Complies

Noise, air and water emissions

As noted the EPA supports the proposal generally in its current form. The EPA is satisfied that the proposal meets the relevant guidelines that measure air, noise, and water management including EPA Guidelines and *State Environment Protection Policy on air quality and noise* without any further changes.

10.0 RESPONSE AGAINST GROUNDS OF OBJECTIONS

10.1. The objection concerns are discussed as follows:

Ground of Objection	Response
Non-compliance with permit and request to remove fill, Unlawful dumping	<p>The <i>Planning and Environment Act 1987</i> and associated regulations allows an applicant to submit an application for consideration by the Responsible Authority for retrospective proposals.</p> <p>Council and EPA Victoria can consider any enforcement action deemed necessary. This is outside the scope of the planning considerations.</p>
Visual bulk of additional fill	<p>Council officer's do not considered there is any visual bulk. The current approval allowed for a crest that rose to 9 metres above Carroll Road at a distance of approx. 210 metres, a slope that would be visible.</p> <p>The proposed crest of the hill would rise to 12 metres above Carroll Road, approximately 210 metres from Carroll Road and 180 metres from Mavis Hutter Reserve. This crest creates a slightly more prominent yet gentle slope that peaks closer to the golf course and away from the public realm and residential area. Along with a substantial increase to landscaping, this adequately mitigates any potential visual impact.</p>
Dust impact and mitigation Noise impact Odours	<p>The proposal will not impact any current mitigation measures for off-site amenity controls for dust, water, air management, specified under the existing permit conditions 14 to 20.</p> <p>The proposal has received favourable support from the relevant authority EPA Victoria without any further changes.</p> <p>Any enforcement matters are beyond the scope of this planning assessment.</p> <p>Approval of this proposal would reduce the amount of truck movements, dust, noise and odours as landfill will not need to be removed off-site.</p>
Health implications Potential contamination	<p>Detailed off-site amenity controls for dust, water, air management, are specified under the existing permit conditions 14 to 20, Detailed contamination controls are specified under the existing permit conditions 4 and 5.</p> <p>The application does not seek to alter any approved measures or conditions and will continue to be bound by these conditions and the EPA regulations for off-site amenity controls. These measures and conditions are considered</p>

Ground of Objection	Response
	adequate to maintain air quality and control contamination without detriment to health of surrounding persons. Approval of this proposal would reduce the amount of truck movements, dust, noise and odours generation, as landfill would not need to be removed off-site.
Neighbourhood character	Council policy indicates that the preferred future character is for open space, either private or publicly held. Overall the proposed works are considered to be consistent with state and local policy as they provide a final contour that allow for the after use of the land for open space.
Overshadowing	Given the substantial setback of the hill and gentle gradients, the proposal will not result in any overshadowing to the public realm or the private properties.
Extended activities on site, lack of notification, commitment to timelines	These objections are outside the scope of the planning considerations. It is noted that should the proposal receive support, it would reduce the timeframes for completion by approximately 1 year as the landfill will not need to be removed.

11.0 CONCLUSION:

- 11.1. On balance, the proposal is considered to substantially comply with the relevant planning policy and therefore should be supported.
- 11.2. As outlined above, it has been determined that prior to deciding on this application all factors pursuant to section 60(1) of The Act have been considered. Further to this, the proposal does not give rise to any significant social and economic effects.
- 11.3. The proposed works are considered appropriate for the site as evidenced by:
- The design and siting of the proposed works to be compatible with the surrounding area;
 - The proposal should not have a detrimental impact on surrounding properties (subject to appropriate conditions); and,
 - The proposal satisfies the requirements of the Kingston Planning Scheme, including the PPF, MSS, Zoning / Overlay controls.

12.0 RECOMMENDATION

(NB: Changes to permit shown in **bold** text)

That the Council determine to support the proposal and issue a Notice of Decision to Grant an Amended Planning Permit to Use and Develop the Land for a Solid Inert Waste Landfill at 19-71 Carroll Road Oakleigh South, subject to the following conditions:

1. Prior to the commencement of the development/use hereby permitted, three copies of the layout plans drawn to scale and dimensioned shall be submitted to and approved by the Responsible Authority. Such plans shall be generally in accordance with the plan submitted with the application **and prepared by Golder, Figures 1 to 2, Rev 0 to 2, dated 15/8/18, received by Council on 25 October 2018**, but shall be modified/alterd to show:-

- (a) the provision of a landscape plan in accordance with the submitted SMEC Landscape Master Plan Revision 04 dated 8/11/18 received by Council on 8 November 2018, but modified to show::
- i. the retention of existing established vegetation along the site's Carroll Road (eastern) property boundary and notated on plans;
 - ii. A planting schedule of all proposed trees and shrubs, including botanical names, common names, pot sizes, sizes at maturity, and quantities of each plant;
 - iii. A survey, including, botanical names of all existing trees to be retained or removed on the site including the Diameter at Breast Height (DBH) of all trees native to Victoria and Tree Protection Zones for trees to be retained calculated in accordance with AS4970-2009;
 - iv. A survey including botanical names, of all existing trees on neighbouring properties where the Tree Protection Zones of such trees calculated in accordance with AS4970-2009 fall partially within the subject site, in particular the TPZ of the ESO3 protected tree located in the Commonwealth Golf Course is to be illustrated;
 - v. The delineation of all planting areas, grassed area, maintenance tracks, runoff ponds, fences and other landscape works;
 - vi. A range of plant types from ground covers to large shrubs and trees, provided at adequate planting densities (e.g. plants 1 metre width at maturity planted 1 metre apart); with the species chosen to comprise of 100% site specific indigenous species, at a minimum to the areas shown;
 - vii. The plants chosen are to be specific to the Ecological Vegetation Classes (EVC) found onsite and in the location on the site they are found;
 - viii. The EVC specific plantings are to be consistent with the % coverage described in the 'Department of Sustainability and Environment, EVC/Bioregion Benchmark for Vegetation Quality Assessment, Gippsland Plain bioregion' for EVC's Heathy Woodland (48), Grassy Woodland (175) and Plains Grassy Woodland/Swamp Scrub/Plains Grassy Wetland Mosaic (927);
 - ix. All trees provided at a minimum of 2 metres in height at time of planting, medium to large shrubs to be provided at a minimum pot size of 200mm;
 - x. Notes regarding site preparation, including the removal of all weeds, proposed mulch, soil types and thickness, subsoil preparation and any specific maintenance requirements; and
 - xi. Tree protection measures including for street trees accurately drawn to scale and labelled as per the endorsed Tree Management Plan and Condition 44 of this permit.
- (b) A tree protection area notated on plan for land along the site's eastern boundary, south of the Carroll Road vehicle entry point, for a depth of 15 metres from the site's eastern boundary (Carroll Road) and:
- i. the maintenance track located outside of this tree protection area;
 - ii. no works or changes to the topography other than any approved landscaping, within this tree protection area;
- (c) The removal of all overfill material associated with the subject site removed from the land within the relocated southern fenceline of the abutting property to the south (Mavis Hutter Reserve) and that portion of the land reinstated at the full cost of the permit holder, and noted on plans;

**City of Kingston
Ordinary Meeting of Council**

Agenda

10 December 2018

- (d) The stages of the landfill proposed, including the direction of the landfill from the northern boundary to the southern boundary.
- (e) Car parking for employees in accordance with the provisions of Clause 19 of the Regional Section of the Moorabbin Planning Scheme.
- (f) The location and details of all existing and proposed buildings (including elevations) to the satisfaction of the Responsible Authority.
- (g) The existing and proposed surface levels of the subject land specified to a satisfactory datum.
- (h) The existing and proposed drainage lines.
- (i) The location of proposed monitoring bores.
- (j) The location and details of the transfer station including access thereto.
- (k) The location and details of the recycling station and mulching area including access thereto.
- (l) Location of all sealed and unsealed roads on the land (proposed).
- (m) Access/egress to the land.

Endorsed copies of the plans shall form part of this permit. The operator/permitholder shall at all times comply with the endorsed plans.

2. Prior to commencement of the use hereby permitted, three copies of a plan showing proposed traffic management measures in the streets adjacent to the land shall be submitted to the approved by the Responsible Authority ("traffic management plan"). Such plans shall reflect those matters specified in this permit and shall be to the satisfaction of the Responsible Authority. When approved, the traffic management plan shall be an endorsed plan. The traffic management measures unless otherwise indicated on the plan shall be carried out before commencement of use.
3. The layout of the site, the size of the proposed buildings and works, staging of the landfill, requirements of the landscape plan and any other plan or statement endorsed to form part of this permit shall not be altered or modified (whether or not in order to comply with any statute, statutory rule, by-law or for any other reason) without the consent in writing of the Responsible Authority.

Operating Conditions

Type of Fill

4. 4.1 The waste deposited shall consist only of the following components.
 - (a) Clean fill (rock, soil bricks and concrete);
 - (b) Solid inert waste (domestic, commercial and industrial) as defined by the Environment Protection Authority.
- 4.2 Notwithstanding the preceding condition, the following waste shall not be deposited on the site:

- (a) Soluble chemical wastes;
 - (b) Hazardous wastes;
 - (c) Putrescible garbage including domestic garbage and commercial garbage containing food stuffs;
 - (d) Liquid wastes including domestic grease - trap waste;
 - (e) Waste specified by the Environment Protection (Prescribed Waste) Regulations 1987;
5. (a) All wastes, other than wastes for recycling shall be deposited in layers not exceeding a vertical height of 2 metres.
- (b) The length of the primary tipping face shall not exceed 50 metres unless otherwise approved by the Responsible Authority. Any other tipping face(s) shall operate to the satisfaction of the Responsible Authority.
- (c) Every layer of waste deposited in the landfill shall be evenly and properly consolidated by mechanical plant.
- (d) All wastes other than wastes for recycling shall be covered by a dense incombustible material not less than 150mm in depth on a daily basis.
- (e) Not less than once a week, all exposed wastes other than wastes for recycling shall be covered by a layer of earth not less than 300mm in depth.
- (f) Large metal articles for burial in the landfill shall be compacted prior to deposit in the tipping area.
- (g) Drums shall be crushed prior to deposit in the tipping area.
- (h) Compacted metal articles shall be deposited at the base of the tipping area or in accordance with the requirements - of the Environment Protection Authority.
- (i) Adequate cover material for at least 2 weeks operation shall be stored and readily available on the site at all times.
- (j) No waste shall be permitted to discharge beyond the boundaries of the site.
- (k) Stormwater and groundwater accumulated on the site shall only be disposed of off the site with the written permission of the Responsible Authority.
- (l) All surface drainage shall be diverted away from those portions of the site which have been or are being used for the deposit of wastes.
- (m) After consultation with the Environment Protection Authority, if required by the Responsible Authority the holder of this permit shall, to the satisfaction of the Responsible Authority comply with the following conditions:-
- No leachate including leachate in storm water shall be permitted to pond on the surface of the landfill but shall be conducted to a leachate holding pond.
 - The leachate holding pond shall at all times be maintained so as to prevent any discharge through the bed or banks.
 - The leachate holding pond shall at all times be operated and maintained so that the banks are:
 - i. Stable: and
 - ii. Have a freeboard of not less than 1 metre.
 - iii. In the event of the leachate in the leachate pond requiring in the opinion of the Environment Protection Authority, treatment to suppress any existing pollution any necessary action shall be undertaken by the Responsible Authority in consultation with the Environment Protection authority

- (n) No Material shall be offered for sale or salvage on site other than from the designated recycling areas.
- (o) Vehicles less than 1.6 tonnes tare shall be:
- (i) Excluded from the primary tipping face;
 - (ii) Permitted to dispose of wastes only at a transfer station or recycling facility constructed on the site for this purpose unless otherwise authorised by the Responsible Authority and Environment Protection Authority
- (p) Automotive tyres shall be only be deposited after shredding into pieces not exceeding 250mm in size in any dimension
6. No part of the previously filled area of the land shall be disturbed or removed for the works required by this permit.
7. (a) The use/development hereby permitted shall only accept rubbish and waste as specified above between the following hours:
- In the area north of Bunney Road;
6:00a.m. to 5:00 p.m. Monday to Saturday
 - In the area south of Bunney Road;
7:00a.m. to 5:00 p.m. Monday to Saturday
 - In all areas;
8:00a.m. to 4:00 p.m. Sunday & Public Holidays
- (b) There shall be no activities on the site on Christmas Day or Good Friday.
- (c) On-site operations shall only be permitted during the above hours and within one hour after the above closing time.
- (d) The Responsible Authority may consent, upon application in writing to any extension of the above hours.
8. (a) The holder of this permit shall ensure that those persons responsible for the day to day operations at the site are familiar with and comply with the conditions of this permit and any licence issued by the Environment Protection Authority under the provisions of the Environment Protection Act 1970.
- (b) A copy of this permit and any such licence shall be displayed in the site office.
9. The use once commenced shall operate in accordance with plan specification and information that is endorsed to form part of the permit to the satisfaction of the Responsible Authority.
10. A competent supervisor/manager shall be present during times of operation in order to supervise the site and to ensure that operating standards, including the use and maintenance of suitable equipment, permit and licence conditions are met by the holder of this permit at all times.

Life of Permit

11. The tipping of waste material hereby permitted shall only be permissible for 15 years from the date of this permit. The permit holder may apply for an extension of time, such application to be made no later than six months before expiry of this permit.
12. The operator/holder of this permit shall use its best endeavours to continuously fill the site as soon as practicable.
13. No construction work on site shall commence prior to the issue of a works approval issued by the Environment Protection Authority.

Amenity

14. The main items of plant, shall at all times be maintained in good condition to the satisfaction of the Responsible Authority.

15. Road cleaning machines and water tankers for dust suppression/control shall be provided as required to comply with the requirements of this permit and to the satisfaction of the Responsible Authority.
16. Odours arising from the operation of the landfill shall not be offensive to persons beyond the boundary of the premises.
17. Noise arising from activities on the site shall not create a nuisance or cause unreasonable disturbance to persons beyond the boundaries of the premises.
18. The use shall at all times conform with the State Environment Protection Policy No. N1 (control of noise from commercial, industrial or trade premises within the Melbourne Metropolitan Area), and the requirements of the Environment Protection Authority.
19. (a) A wheel washing facility suitable for trucks leaving the premises shall be installed and maintained in an operating condition to the satisfaction of the Responsible Authority.
 - (b) The wheel washing facility shall be designed so that the effluent is disposed of in a satisfactory manner.
 - (c) All vehicles in excess of 1.6 tonne which have access to the primary tip face, shall, prior to leaving the site, use the wheel wash facility such that waste, stones, soil or clay from wheels are not deposited on the exit road from the tip or adjacent roads.
 - (d) For all other vehicles, the operator/holder of this permit shall ensure, to the satisfaction of the Responsible Authority, that internal access to and from the transfer station/recycling facilities provided on the land are generally free from all mud and waste.
20. Dust Control.
 - (a) Control measures shall be maintained to ensure that site operations do not result in dust emission that may cause offence to people beyond the site boundary.
 - (b) All practicable measures shall be taken by the permit holder to minimise dust emissions arising from the operation of the landfill.
 - (c) Dust arising from the landfill operation shall be minimised by:
 - i. Revegetation of filled areas as soon as practicable on the completion of waste disposal operations; and
 - ii. Regular light watering of internal access roads.
21. A program for the monitoring and control of rodents and insect pests shall be established and maintained to the completion of tipping operations on the site to the satisfaction of the Responsible Authority.

Bird Control

22. Bird Control

The operator shall limit bird numbers attracted to the subject land by appropriate control techniques to the satisfaction of the Responsible Authority. The Responsible Authority may, if the circumstances require, advise the holder of this permit in writing, to undertake such additional control measures as specified to the satisfaction of the Responsible Authority.

23. (a) Fires shall not be lit on the site.
 - (b) In the event of a fire occurring at the landfill
 - i. The permitholder shall take prompt action to extinguish the fire.
 - ii. The permitholder shall promptly notify the Responsible Authority.
 - iii. The permitholder shall submit to the Responsible Authority within 14 days of the fire a written report detailing the date, time, location and suspected cause of the fire and when and how it was extinguished.

- (c) A water supply main and hydrant with distribution facilities shall be established to the requirements of the Metropolitan Fire Brigade and shall be capable of deployment to any part of the site for fire fighting purpose.
24. The holder of this permit shall at all times ensure that a suitable fence to the satisfaction of the Responsible Authority shall be erected around the perimeter of the site to prevent both unauthorized access to the land and the escape of waste materials onto adjoining land.
25. (a) All fences and gates shall be maintained and kept clean to the satisfaction of the Responsible Authority.
- (b) All gates except those required for official access shall be kept locked at all times and ingress-egress gates shall be kept locked except during official operating times.
26. (a) Litter arising from the operations of the site shall at all times be confined within the boundaries of the site.
- (b) All litter screens shall be maintained and cleaned regularly.
- (c) A program for the collection of litter and unauthorized dumping shall be conducted on or abutting all site boundaries and on land accessible to the permit holder within 250 metres of the tip entrance to ensure the area is kept clean to the satisfaction of the Responsible Authority.
- (d) Litter screens shall be erected around the active tipping area as required and to the satisfaction of the Responsible Authority.
27. The operator/holder of this permit shall take all reasonable steps to ensure that there will be no deposit of waste outside the subject site.
28. The landfill shall be progressively filled and restored in stages north to south, as indicated on the endorsed plan.
29. (a) Ground water monitoring bores shall be established and ground monitoring shall be carried out to the satisfaction of the Environment Protection and the Responsible Authority
- (b) The operator/permit holder shall forward to the Responsible Authority within 14 days of the completion of analysis for each sampling occasion, a copy of the record of all analysis, measurement and observations.

Signs and Lighting

30. Suitable signs to the satisfaction of the Responsible Authority must be prominently displayed at the entrance to the site indicating:
- (a) The hours of opening of the site.
- (b) Those wastes which may be deposited and those which shall not be deposited.
- (c) Where wastes shall be deposited within the site and by whom including recycling and transfer station.
- (d) A fire shall not be lit on the site.
31. Security lights provided in respect to the use hereby permitted shall be adequately baffled to the satisfaction of the Responsible Authority to ensure no direct light is emitted beyond the boundaries of the land.
32. (a) Prior to the commencement of the use hereby permitted, arrangements shall be made for the suitable direct ingress and egress to the site to the satisfaction of the Responsible Authority from Carroll Road generally opposite Cleeland Street.
- (b) Access at all times shall continue to be to the satisfaction of the Responsible Authority.

- (c) A properly constructed bitumen sealed access road shall be constructed from the entrance of the site to the weigh bridge to the satisfaction of the Responsible Authority.

Complaints

33. The permit holder shall keep a written record of complaints received in writing concerning the operating of the landfill. Complaints shall be asked to submit their complaints in writing and the record shall be kept which should include the following details:
- (a) The name and address of the complainant.
 - (b) The date and time of the complaint.
 - (c) the location from which the complaint arose
 - (d) Prevailing wind conditions at the time.
 - (e) The likely cause of the complaint.
 - (f) The action taken by the permit holder.

The complaint register shall be made available to the Responsible Authority with 24 hours of a request

34. Each twelve months from the date of issue" of the permit the holder of the permit shall meet with the Responsible Authority and any other representative which the Responsible Authority may nominate to discuss the operation of the landfill hereby permitted. The permit permit holder/operator shall provide to the Responsible Authority such reasonable information as may be requested including details of filling during the previous 12 months and the estimated air space yet to be filled.

Site Rehabilitation

35. (a) At least 18 months prior to the landfill operations ceasing at the premises, the permit holder shall provide written advice to the Responsible Authority of a proposal, including the requirements of the Environment Protection Authority for the rehabilitation and future development and use of the premises.
- (b) Site rehabilitation shall include the following features:
- i. When each stage of the waste disposal operations ceases, a final cover of earth shall be placed over the landfill to a depth of not less than 500 mm.
 - ii. The final surface of any stage shall be covered with topsoil graded, drained and vegetated to promote vegetation growth minimise erosion and to prevent ponding of stormwater.
 - iii. After settlement the final surface levels shall be in accordance with the endorsed plan.
- (c) Final contours shall be established within 6 months of the cessation of tipping.
- (d) The tipping activities shall be managed so that site rehabilitation is completed to the satisfaction of the Responsible Authority within the life of this permit.
- (e) No tipping shall take place within 6 months of expiry of this permit to allow for the completions of site rehabilitation.
36. Methane gas monitoring collection and disposal if required shall be carried out to the satisfaction of the Responsible Authority and the Environment Protection Authority.
37. The use and development hereby permitted shall at all times comply with the conditions of any works approval or licence under the Environment Protection Act, the requirements of the Health Act 1958 or any other Act relevant to the on-site

operations, or any re-enactment or consolidation thereof or any similar enactment or any regulations made thereunder.

Commencement

38. This permit shall expire if the development or use hereby permitted is not commenced within 2 years from the date of issue.

Legal Agreement

39. An agreement pursuant to Section 173 of the Planning and Environment Act 1987 which incorporates the following matters shall be entered into between the owner of the land and the Responsible Authority to the satisfaction of the Responsible Authority. The Agreement shall contain the following provisions:

- i. The cost of the execution of the legal agreement to be borne by the applicant company.
 - ii. Specified local area traffic management measures to be employed in Carroll Road/Bunney Road and at the intersection of Bourke and Carroll Roads to the satisfaction of the Responsible Authority generally in accordance with the Turnbull Morgan Report dated December 1991 entitled "Supplementary Traffic Assessment. Solid Inert Landfill" at the cost of the permit holder operator.
 - iii. The provision of specified traffic management works at the intersection of Centre and Carroll Roads that shall be generally limited to widening within the Carroll Road reservation to provide left and right turn lanes into Centre Road. Further agreement that should these measures prove not sufficient, a commitment that traffic lights shall be installed after consideration of a report by a mutually acceptable traffic engineer. The cost of these measures are to be apportioned to the satisfaction of the Responsible Authority.
 - iv. The exhibition of suitable signage at various points by agreement in the local area to indicate the location of the tip and the major entrance point and traffic route. Cost to be borne by the permit holder/operator.
 - v. That rehabilitation and future development or use of the land shall have regard to the chain of parks concept as set out in the document entitled "Implementation Strategy for the Chain of Parks March 1992 by Deloitte Ross Tohmatsu".
40. The use and development hereby permitted shall at all times comply with the conditions of any licence issued by the Environment Protection Authority pursuant to the provisions of the Environment Protection Act 1970.
41. In the event of there being any inconsistency between the conditions and requirements of this permit and the conditions of any works approval and licence issued by the Environment Protection Authority under the provisions of Environment Protection Act 1970, the conditions and requirements of such works approval and licence shall prevail.

EPA Victoria condition

42. 'The revised contour plan should be consistent with the Kingston City Council planning requirements, and council approval should be obtained'

Tree Management and Protection

- 43. The landscaping shown on the endorsed plans must be maintained to the satisfaction of the Responsible Authority, including that any dead, diseased or damaged plants are to be replaced**
- 44. Tree Protection Fencing is to be established in a line 15 m from the Carroll Road boundary and parallel to the Carroll Road boundary for the length of the works;**

- a. The fencing is to be a 1.8 metre high temporary fence constructed using steel or timber posts fixed in the ground or to a concrete pad, with the fence's side panels to be constructed of cyclone mesh wire.
 - b. The fencing is to be installed prior to any works on site commencing and remain in place until all works on site, with the exception of landscaping are completed.
45. The retention of all trees on site native to Victoria as listed in the endorsed landscape master plan.
46. No works are to be undertaken within the Tree Protection Zone (TPZ) of the Environmentally Significant Overlay Schedule 3 protected *Eucalyptus cornuta* located on the adjoining land to the west.
47. Concurrent with the endorsement of plans, a Tree Management Plan prepared by a suitably qualified arborist in accordance with AS4970-2009, must be submitted to and be endorsed by the Responsible Authority and incorporating:
- a. A Tree Management Plan (written report) must provide details of:
 - i. How excavation impacts, including soil level changes, on trees to be retained will be managed.
 - ii. How the canopy of trees nominated on the Tree Protection Plan will be protected.
 - iii. Any other measures required to demonstrate the successful ongoing retention and viability post-construction of any trees nominated on the Tree Protection Plan.
 - b. A Tree Protection Plan (scale drawing) must provide details of:
 - i. The Tree Protection Zone and Structural Root Zone, calculated in accordance with AS4970-2009, for all trees to be retained on the site and for all trees on neighbouring properties where the Tree Protection Zone falls partially within the subject site.
 - ii. Tree protection fencing, or ground protection where required, provided in accordance with AS4970-2009 including the tree protection fence specified above.
 - iii. Stages of development at which inspections are required to ensure tree protection measures are adhered to must be specified.
 - iv. Appropriate signage on any tree protection fencing prohibiting access, excavation, changes in soil levels, or any storage within the Tree Protection Zone in accordance with AS4970-2009 unless with the prior written consent and under the direct supervision of the consulting arborist.
 - v. Maintenance of the area(s) within the Tree Protection Zone in accordance with AS4970-2009.
 - vi. Any pruning to be undertaken being in accordance with AS4373-2007.
 - vii. A notation to refer to the Tree Management Plan.
48. All protection measures identified in the Tree Management Plan must be implemented, and development works undertaken on the land must be undertaken in accordance with the Tree Management Plan, to the satisfaction of the Responsible Authority.
49. Prior to the commencement of works, the name and contact details of the project arborist responsible for implementing the Tree Management Plan must be submitted to the Responsible Authority.

Relocated boundary fencing

- 50. Before the final rehabilitation commences on the site, the southern boundary fence must be relocated to within the subject site's title boundary and all overfill material must be removed and that land (portion of Mavis Hutter Reserve) reinstated, unless an alternative timeframe is agreed to in writing by the Responsible Authority.**
- 51. Once the overfill material referred to in the above condition has been removed, the permit holder must provide to Council a written and certified statement that the works have been completed and the boundary fence appropriately relocated, to the satisfaction of the Responsible Authority.**

This permit has been issued in accordance with the decision of the Administrative Appeals Tribunal dated March 18. 1993.

Note; Minor typographical errors corrected on June 10. 1993.

THIS PERMIT HAS BEEN AMENDED PURSUANT TO SECTION 72 OF THE PLANNING AND ENVIRONMENT ACT 1987 AS FOLLOWS:

Amendment	Date of Amendment	Description of Amendment	Name of responsible authority that approved the amendment
A	INSERT DATE	<ul style="list-style-type: none"> • Retrospectively apply for an increase to the landfill volume and alter the landfill cap height and position of the southern part of the existing solid inert waste landfill • Varied Condition 1a • Insert Conditions 1b and c and renumbering of condition 1's accordingly • Insert Conditions 42 to 51 	City of Kingston

OR

In the event that Council wishes to refuse the amendment application, it could do so on the following grounds:

1. The proposal does not comply with the Clauses: 13.05-1S, Clause 13.06-1S, 19.03-6S of the Kingston Planning Scheme.
2. The proposal is inconsistent with the EPA Best Practice Environmental Management - 788.3 Siting, design, operation and rehabilitation of landfills.
3. The proposal does not comply with Clause 52.17 – Native vegetation of the Kingston Planning Scheme.

Appendices

Appendix 1 - KP-1993/5158/A - 19-71 Carroll Road, OAKLEIGH SOUTH VIC
3167 - Plans for consideration by Committee (Ref 18/590042) [↓](#)

Author/s: Hugh Charlton, Statutory Planner
Reviewed and Approved By: Jeremy Hopkins, Team Leader Statutory Planning
Ian Nice, Manager City Development

8.2

KP-1993/5158 - 19-71 CARROLL ROAD OAKLEIGH SOUTH

- 1 KP-1993/5158/A - 19-71 Carroll Road, OAKLEIGH SOUTH VIC
3167 - Plans for consideration by Committee 49**

Site Analysis



- LEGEND**
- Continuation of Soft Course Landscape Character into Rehabilitation Site.
 - Site Boundary Interfacing with Built Form.
 - Existing Site Boundary Planting.
 - Potential future connection to Commonwealth Golf Club.
 - Recognise view from existing street network.
 - Existing High Point and view from Mavis Hutter Reserve.
 - High Point on Landfill Rehabilitation Site.

Note - This plan is conceptual only for landscaping and site analysis and shows the initial older contours. Refer to other plans for latest contours

Master Plan Concept



SMEC Environments Advice

- Large trees with deep root systems are not to be planted on top of the capped landfill as tree roots have the potential to damage the cap layer allowing water penetration.
- It is recommended to restrict planting to smaller plants and shrubs including the use of tussock grasses which would be the preferred species.
- Where additional planting is proposed use smaller species which do not have an extensive vertical root system. Whilst the planting of larger plants are required, the soil depth should be increased in line with the anticipated depth of the mature root system to allow lateral root movement.

Planting Considerations

- Species selected to be indigenous to local area to enhance local habitat.
- Species selection to be suitable to local climatic conditions to ensure longevity.
- Within grassed areas, seed mix to incorporate native grass and wild flower mix.
- Planting layout and species selection to reflect potential future use of land by Commonwealth Golf Club.

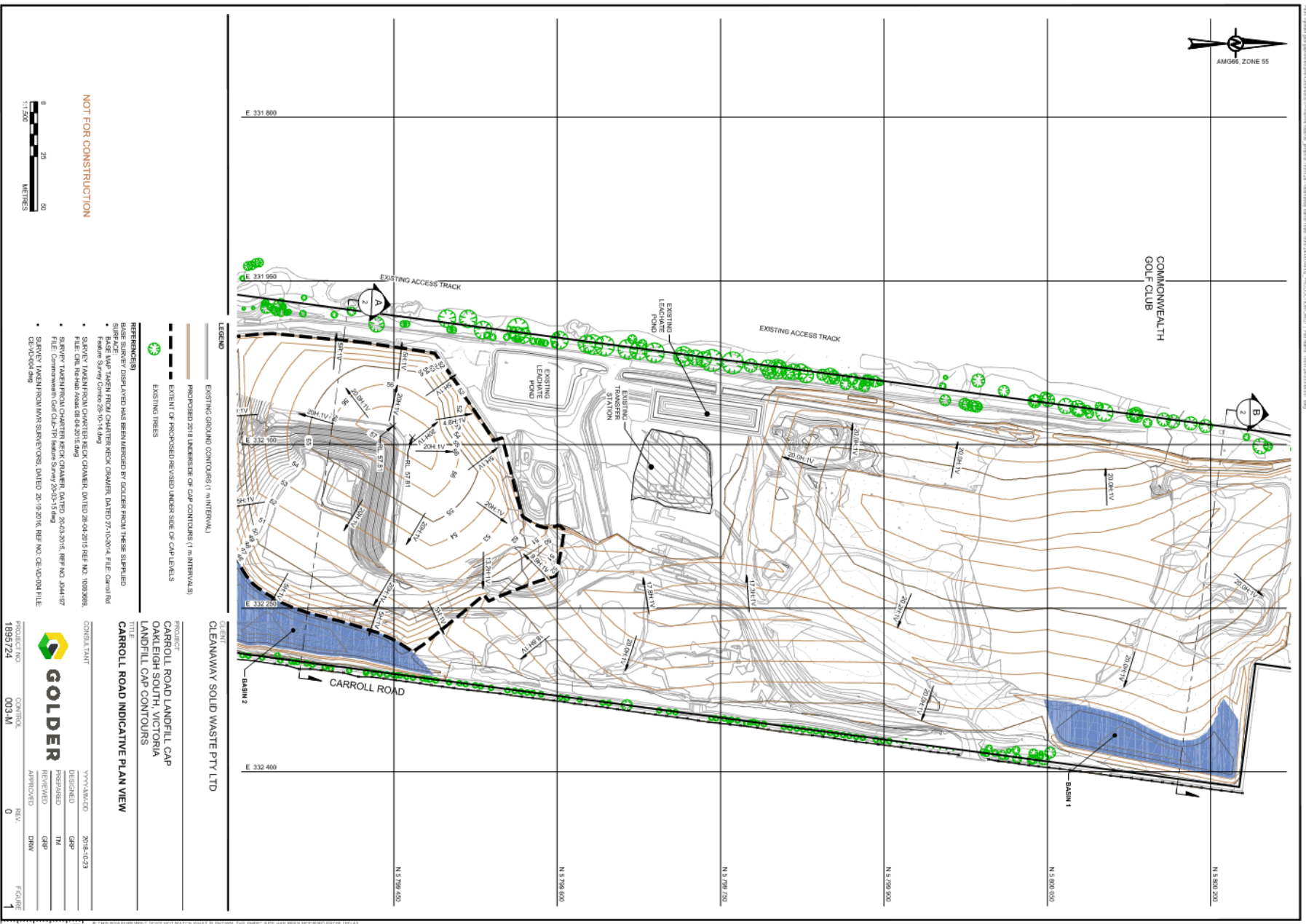
LANDSCAPE MASTER PLAN SKETCH CONCEPT

CARROLL ROAD LANDFILL, CLAYTON

Date Issued: 21/11/2016 | Revised: 8
 SMEC Reference: 2004-036-00 | Drawing ID:
 Drawn by: T. HUBBARD / Checked by: S. O'CALLAGHAN

Copyright © 2016 SMEC. This plan is based on information provided and is subject to change as more detailed information is confirmed by the relevant authorities.





NOT FOR CONSTRUCTION

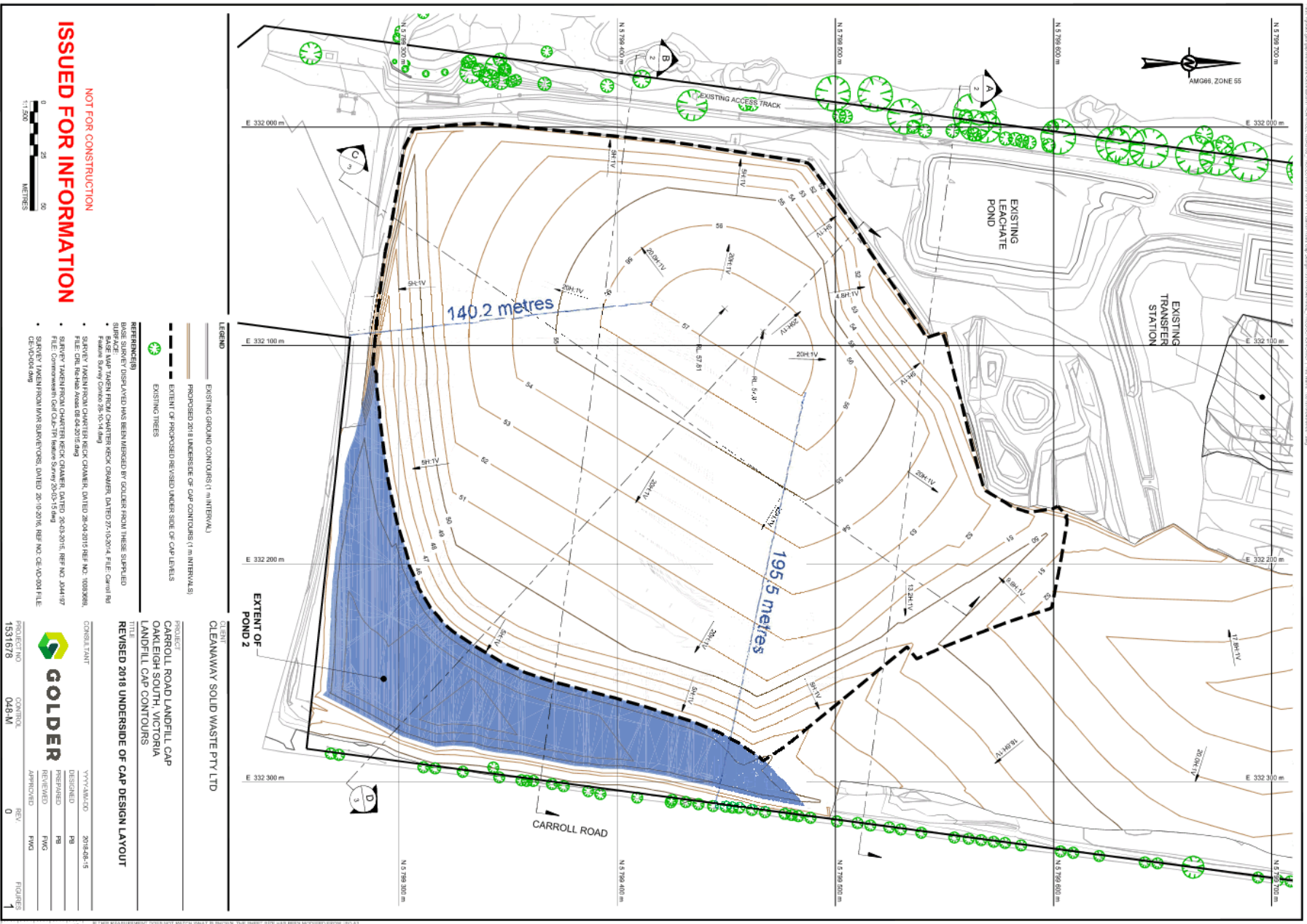


- LEGEND**
- EXISTING GROUND CONTOURS (1 m INTERVALS)
 - PROPOSED 2018 UNDERFILL OF CAP CONTOURS (1 m INTERVALS)
 - EXISTING TREES
 - EXTENT OF PROPOSED/REVISED UNDERFILL UNDER SIDE OF CAP LEVELS
- REFERENCES**
- BASE SURVEY DISPLAYED HAS BEEN MENDED BY GOLDER FROM THESE SUPPLIED
 - BASE MAP TAKEN FROM CHARTER CHECK CHARTER, DATED 27-10-2014 FILE: Carroll Rd
 - Field Book Survey Contd 25-10-14.dwg
 - SURVEY TAKEN FROM CHARTER CHECK CHARTER, DATED 28-04-2015 REF. NO. 1003986
 - FILE: CR - R.H.H. Area 08-04-2015.dwg
 - SURVEY TAKEN FROM CHARTER CHECK CHARTER, DATED 26-03-2015 REF. NO. 1044197
 - FILE: Commonwealth Golf Club - R.H.H. Area Survey 23-03-15.dwg
 - SURVEY TAKEN FROM WMS SURVEYS, DATED 26-10-2016, REF. NO. CE-10-004 FILE: CE-10-004.dwg

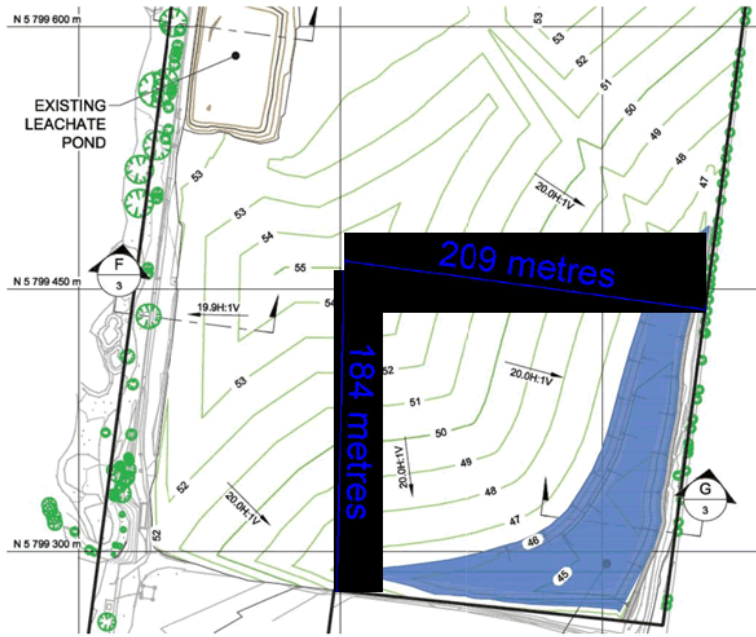
CARROLL ROAD INDICATIVE PLAN VIEW

PROJECT NO.	CONTROL	DATE	FIGURE
1895724	003.M	0	1

CONSULTANT	DESIGNED	PREPARED	APPROVED
GOLDER	V.V. YAMALDO	T.M.	D.R.V.
	2018-023		



The Updated Proposal



Endorsed Contours



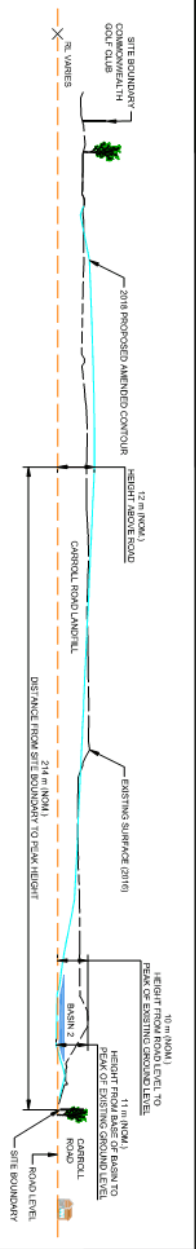
Updated Proposed Contours

Note.

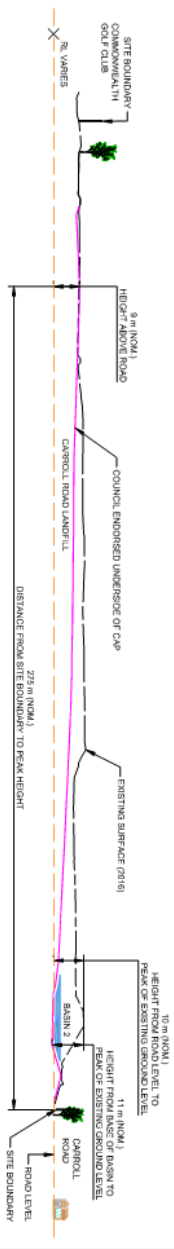
- Contours along the Carroll Road boundary (including pond) remain unchanged
- Contours in the North do not change
- Difference in contour peak is 2m (3m less than previous proposed contours). Peak located in the West.
- Southern peak will now be 3m below Northern peak



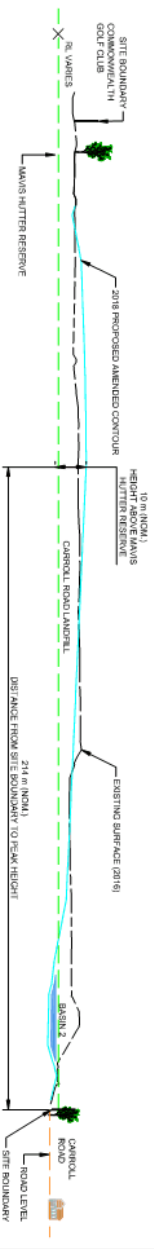
\p\1993\5158\A\19-71\2018 Proposed Amended Contour Compared to Road.dwg, 20/10/2018, 11:44:46 AM, 100% (243133) 1:200



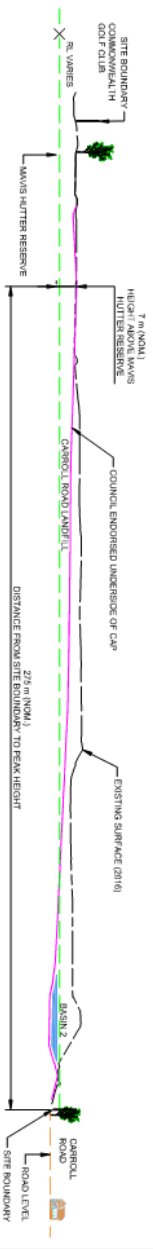
SCALE: 1:1,500 **A** 2018 PROPOSED AMENDED CONTOUR COMPARED TO ROAD



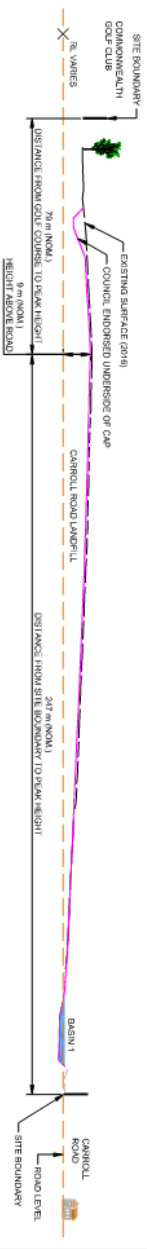
SCALE: 1:1,500 **B** COUNCIL ENDORSED UNDERSIDE OF CAP COMPARED TO ROAD



SCALE: 1:1,500 **C** 2018 PROPOSED AMENDED CONTOUR COMPARED TO MAVIS HUTTER RESERVE



SCALE: 1:1,500 **D** COUNCIL ENDORSED UNDERSIDE OF CAP COMPARED TO MAVIS HUTTER RESERVE



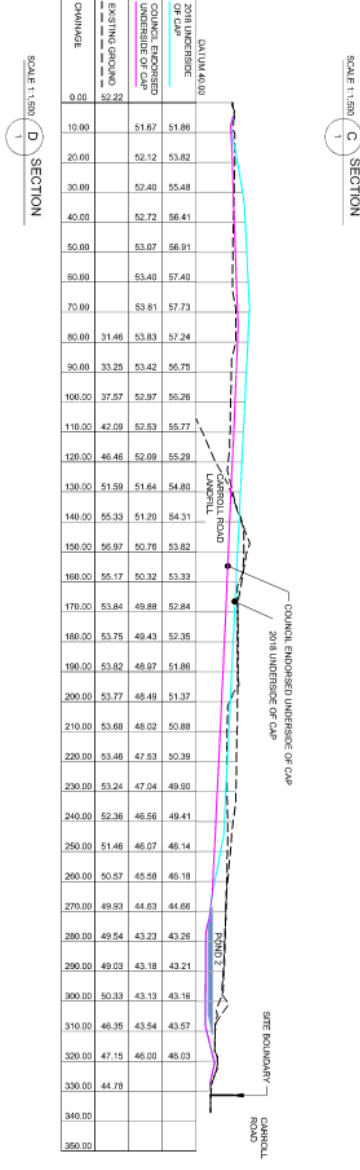
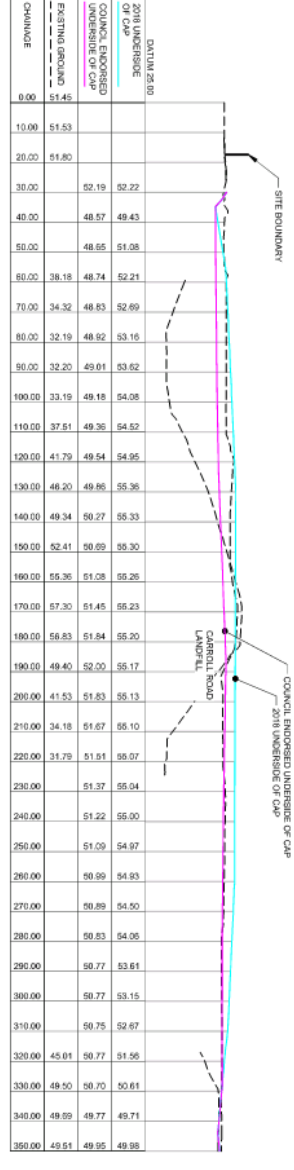
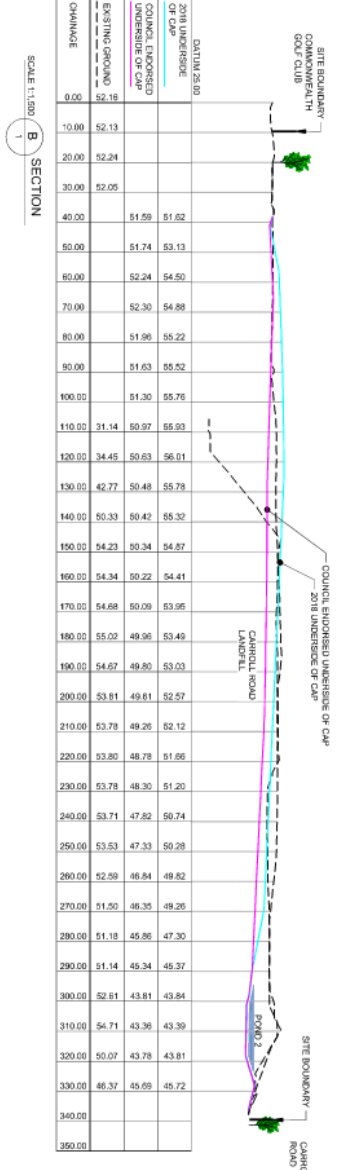
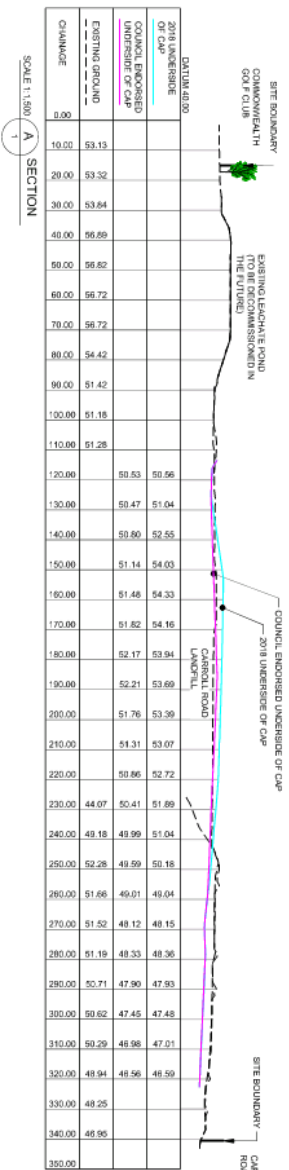
SCALE: 1:1,500 **E** REHABILITATED AREA

NOT FOR CONSTRUCTION



REFERENCES
 EXISTING SURFACE SUPPLIED BY WWSR PTY LTD. DRAWING NUMBER CR-V03-004 DATED 20 OCTOBER 2016

CONSULTANT	YVY-AWACD	2018-023
DESIGNED	GRP	
REVIEWED	TM	
APPROVED	GRP	
	DMV	
PROJECT NO	003-M	REV
CONTRACT NO		1
DATE		
FIGURE		2



CLIENT
CLEANAWAY SOLID WASTE PTY LTD

PROJECT
CARROLL ROAD LANDFILL CAP
OAKLEIGH SOUTH, VICTORIA
LANDFILL CAP COUNTURS

TITLE
REVISED UNDERSIDE OF CAP DESIGN SECTIONS

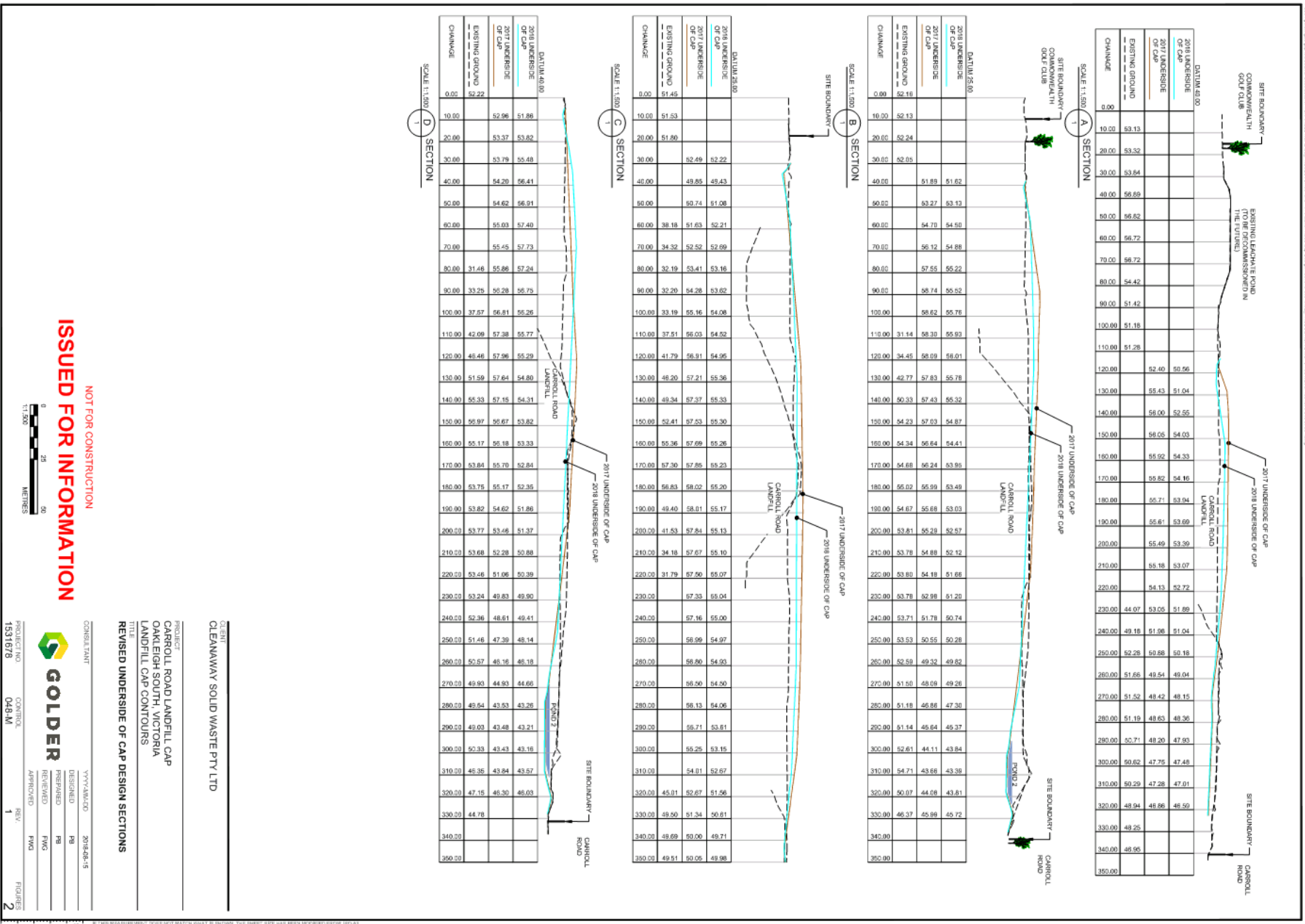
CONSULTANT
VVA/AM/DO 2018-08-21

DESIGNED PB 2018-08-21
PREPARED PB
REVIEWED PMS
APPROVED PMS

PROJECT NO 1531678 CONTROL Q45-M REV 2 FIGURES 2

ISSUED FOR INFORMATION
NOT FOR CONSTRUCTION

0 10 20 30 METRES
1:1,500



NOT FOR CONSTRUCTION
ISSUED FOR INFORMATION



CLIENT	CLEANAWAY SOLID WASTE PTY LTD
PROJECT	CARROLL ROAD LANDFILL CAP OAKLEIGH SOUTH, VICTORIA LANDFILL CAP CONTOURS
TITLE	REVISED UNDERSIDE OF CAP DESIGN SECTIONS
CONSULTANT	YVY-A&C/D
DESIGNED	FB
PREPARED	FB
REVIEWED	FBG
APPROVED	FBG
PROJECT NO	1531678
CONTROL	045.M
REV	1
FIGURES	2

The Updated Proposal



Current



Proposed Amendment

Northern visualisations from southern public open space

North-western visualisations from south-eastern boundary

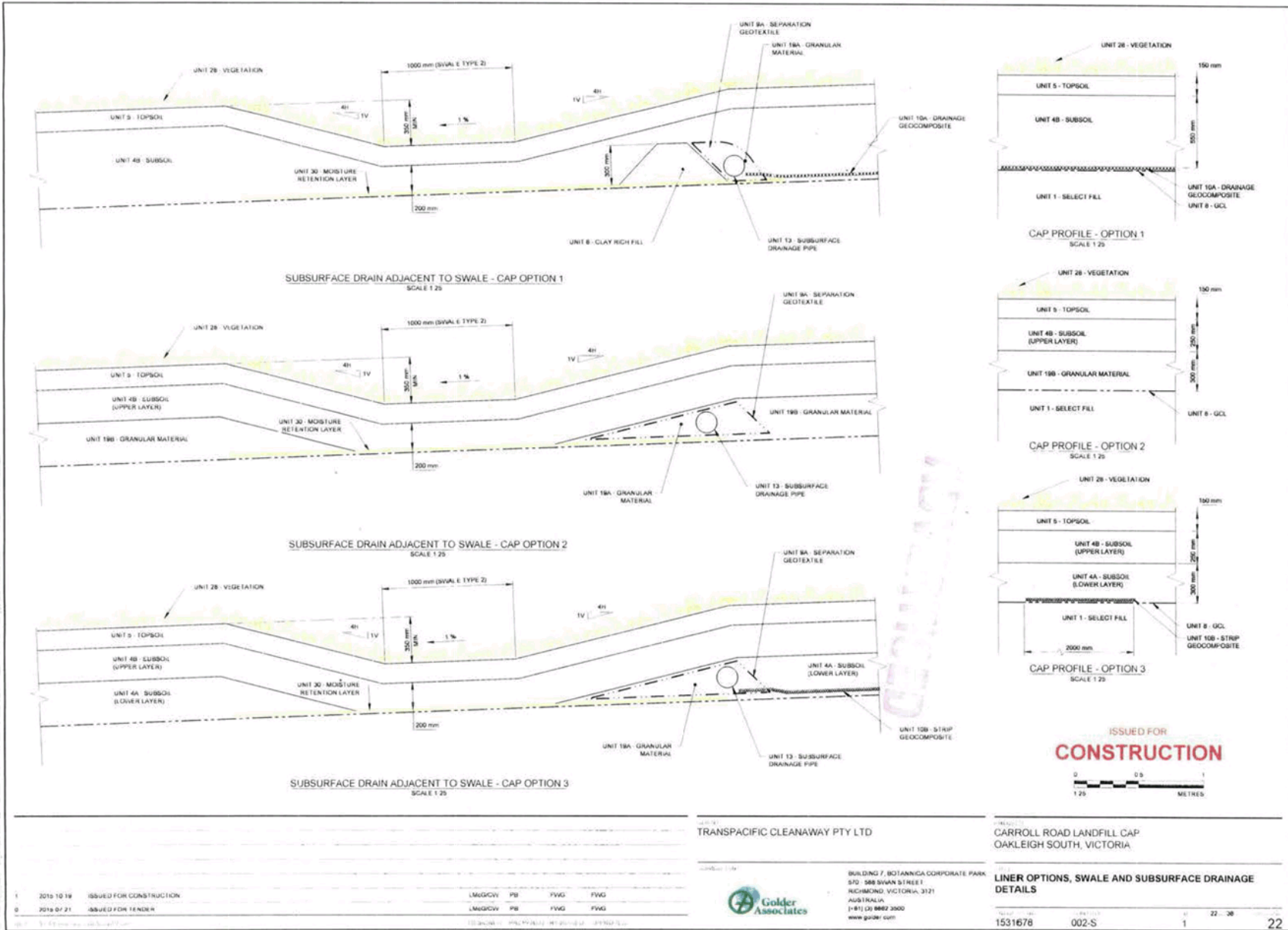


Current



Proposed Amendment





TRANSPACIFIC CLEANAWAY PTY LTD

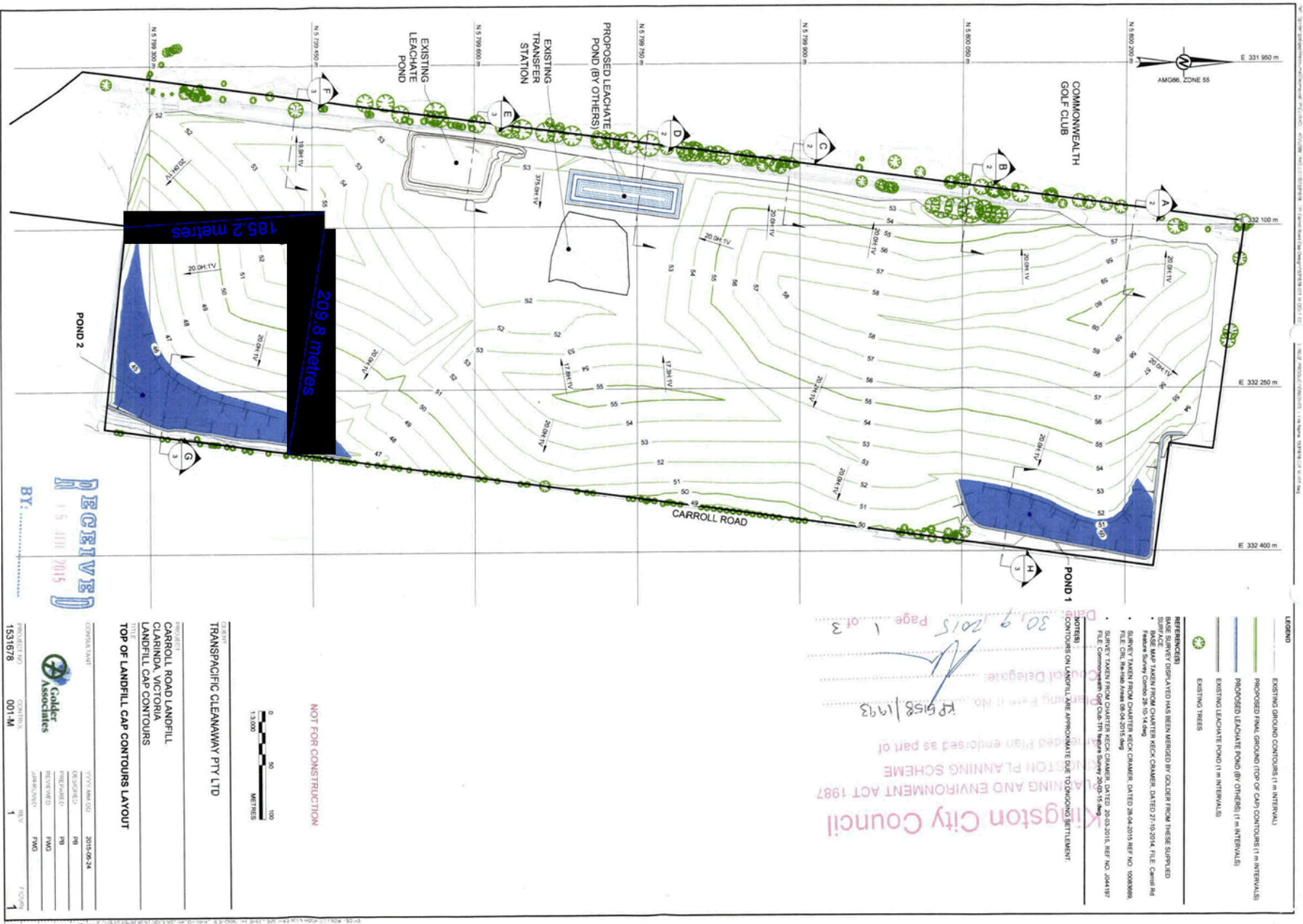
CARROLL ROAD LANDFILL CAP
OAKLEIGH SOUTH, VICTORIA

1	2019 10 19	ISSUED FOR CONSTRUCTION	LMG/GW	FW	FWG	FWG
0	2019 07 21	ISSUED FOR TENDER	LMG/GW	FW	FWG	FWG



BUILDING 7, BOJANNICA CORPORATE PARK
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RICHMOND, VICTORIA, 3121
AUSTRALIA
(+61) 03 9862 3500
www.golda.com

LINER OPTIONS, SWALE AND SUBSURFACE DRAINAGE DETAILS





Kingston City Council
 PLANNING AND ENVIRONMENT ACT 1987
 KINGSFORD PLANNING SCHEME
 This Plan endorsed as part of
 Council Delegate: [Signature]
 KPS15/8/1993
 Date: 30/9/2015 Page 2 of 3

NOT FOR CONSTRUCTION
 RECEIVED
 15 JUN 2015
 BY: [Signature]

PROJECT: TRANS-PACIFIC CLEANAWAY PTY LTD
 PROJECT NO: 15316/78
 SHEET: 001-1M
 DATE: 2015-06-09
 DRAWN BY: PM
 CHECKED BY: PM
 APPROVED BY: PM



Kingston City Council
 PLANNING AND ENVIRONMENT ACT 1987
 KINGSTON PLANNING SCHEME
 Approved Plan endorsed as part of
 Planning Permit No. KP5158/1993
 Council Delegate: [Signature]
 Date: 30/09/2015 Page 3 of 5

NOT FOR CONSTRUCTION

RECEIVED
 15 JUL 2015

GOLDER ASSOCIATES

PROJECT NO: 1531678
 CONTROL: 001-M
 DATE: 2015-06-26

DESIGNED: [Signature]
 PREPARED: [Signature]
 REVIEWED: [Signature]
 APPROVED: [Signature]

REV: 0

SCALE: 1:500

DATE: 4/5/2015

Ordinary Meeting of Council

10 December 2018

Agenda Item No: 8.3

KP-2017/981 - 1 PARK STREET MORDIALLOC

Contact Officer: Girija Shrestha, Senior Statutory Planner

Purpose of Report

This report is for Council to consider Planning Permit Application No. KP-2017/981 - 1 Park Street Mordialloc.

Disclosure of Officer / Contractor Direct or Indirect Interest

No Council officer/s and/or Contractor/s who have provided advice in relation to this report have declared a Conflict of Interest regarding the matter under consideration.

OFFICER RECOMMENDATION

That Council determine to support the proposal and issue a Notice of Decision to Develop the land for the construction of five (5) dwellings at 1 Park Street Mordialloc, subject to the conditions contained within this report.

**City of Kingston
Ordinary Meeting of Council**

Agenda

10 December 2018

PLANNING OFFICER REPORT	
APPLICANT	Bowden Planning
ADDRESS OF LAND	1 Park Street, Mordialloc
PLAN OF SUBDIVISION REFERENCE	Lot 1 and 2 on TP 749047H
PROPOSAL	Develop the land for the construction of five (5) dwellings comprising three (3) double-storey dwellings and two (2) three-storey dwellings
PLANNING OFFICER	Girija Shrestha
REFERENCE NO.	KP-2017/981
ZONE	Clause 32.08: General Residential Zone (Schedule 2)
OVERLAYS	N/A
OBJECTIONS	Three (3)
CONSIDERED PLAN REFERENCES/DATE RECEIVED	archsigh, Job No 17322, Sheets TP1.0 to TP5.0 and SH1.0 to SH8.0 (Revision C) received on 16 October and 8 November 2018.
ABORIGINAL CULTURAL HERITAGE SENSITIVITY	Yes but Exempt

1.0 RELEVANT LAND HISTORY

1.1 Council records indicate that there is no relevant planning history relating to this site.

2.0 SITE PARTICULARS

2.1 The subject site is irregular in shape with a frontage to Park Street of 39.04 metres, a secondary frontage to Albert Street of 28.98 metres, a north-western boundary length of 14.38 metres, and a south-eastern boundary length of 38.01, yielding an overall site area of 810 square metres. The topography of the land is generally flat.

2.2 The site is presently occupied by a single storey weatherboard dwelling, with a pitched tiled roof. To the north of the dwelling, within the secluded private open space (SPOS) area, are several outbuildings. Vehicle access is provided from Albert Street to the north via a double width crossover to a garage, and a single width crossover to informal parking within the SPOS.

2.3 No easements are contained within the property. There appears to be no restrictions listed on the Certificate of Title.

2.4 The subject site is located within Area 7B (Local area Mordialloc – central, south, west and east) of the *Kingston Open Space Strategy* approved by Council in June 2012. The Strategy notes that the Epsom Racecourse redevelopment has been completed since the development of the 2005 Open Space Strategy and in total will create an approximate 440 additional dwellings. Other issues in the study area include increased traffic flows along Governor Road, and the residential and industrial interface in the south east corner of the study area. The outcomes of the Mordialloc Structure Plan and the Mordialloc Creek Master Plan 2011 will assist in further guiding land use within the study area.

2.5 A site inspection was carried out by the Planning Officer on 17 August 2018.

2.6 View from Albert Street

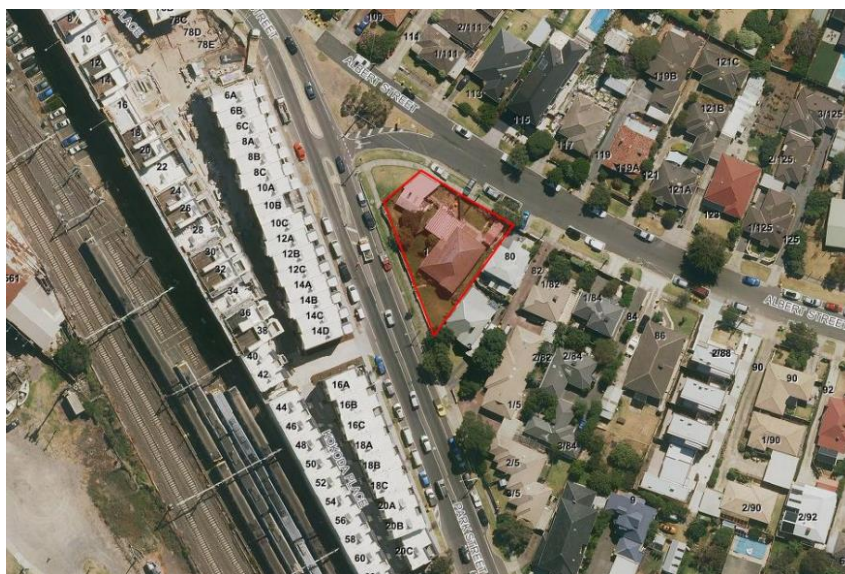


2.7 View from Park Street



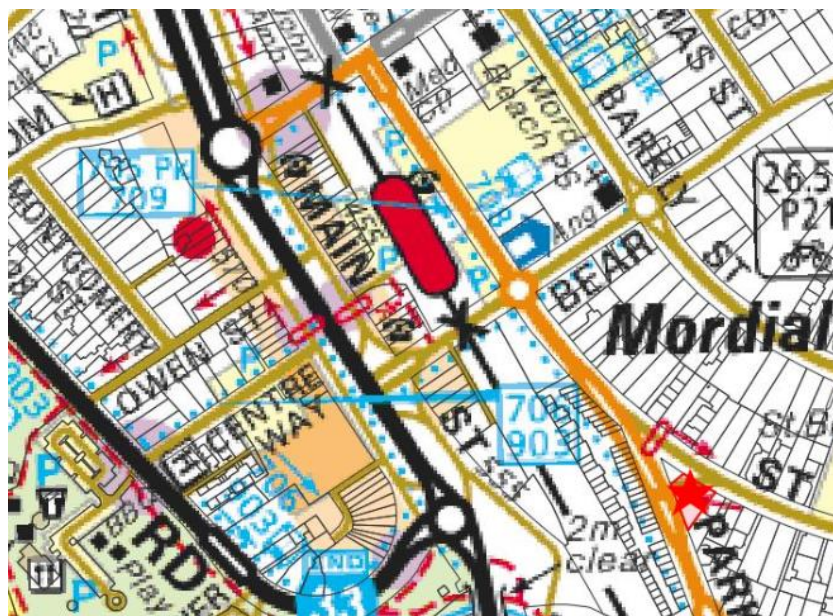
3.0 SURROUNDING ENVIRONS

3.1 The following map illustrates the subject site in its surrounding context.



3.2 The surrounding area typically comprises of a combination of older housing stock comprising single dwellings on single allotments with more recent examples of contemporary medium density housing, including the existing three (3) storey high residential development located on the south-west side of Park Street.

3.3 The subject site is located within close proximity to the Mordialloc Railway Station (approximately 350 metres to the north-west) and the Mordialloc Activity Centre. The surrounding residential area is well serviced by public transport (including bus routes along Nepean Highway/Main Street), primary and secondary schools, community facilities and public reserves.



3.4 Land directly abutting the subject site and opposite is described as follows:

North-west (Side): Road reserve and Road connection between two roads, Albert Street and Park Street.

North-east (Front): Road and Nos. 113, 115 and 117 Albert Street – These three properties consists of two single storey dwellings and one double storey dwelling. is property has recently been developed for four (4) dwellings, comprising of three (3) double-storey dwellings and a single-storey dwelling at the rear (south). The existing development on this site features a minimum front building setback of 5.9m.

South (Rear): No. 7 Park Street – A large double-storey dwelling occupies this property and is setback 5.7m from the shared boundary with the subject site.

West (Side): No. 84 Albert Street – Three (3) dwellings exist on this property, consisting of two (2) double-storey dwellings and a single-storey dwelling. The existing development on this property is setback a minimum distance of 6.5m from Albert Street.

4.0 **PROPOSAL**

4.1 It is proposed to demolish the existing dwelling and outbuildings on the land and construct five (5) dwellings comprising two (2) three-storey and three (3) double storey dwellings on this site, generally in accordance with the revised plans received by Council on the 31 October 2018.

4.2 A summary of the proposed development is, as follows:

Dwelling No.	No. of Bedrooms	Total Floor Area (m²)	Total Private Open Space Area (m²)	No. of Car Parking Spaces
1	3	188	130	2
2	2	137	28	1
3	2	130	78	1
4	3	191	82	2
5	3	211	83	2

4.3 A total of eight (8) on-site car parking spaces would be provided at ground level accessed either via Albert Street or Park Street. All vehicle access for Dwellings 2, 3, 4 and 5 would be provided via an existing 3.0 metre wide crossover at Albert Street but the vehicle access for Dwelling 1 would be provided from a new vehicle crossing at Park Street. An existing vehicle crossing located at the north-west corner of Albert Street would be removed.

4.4 External building materials for the proposed dwellings would comprise of a combination of face brickwork (Miro (La Paloma Range), *Austral Bricks*) and Natural Timber Cladding (Expression Sorrento Profile) – Pacific Teak, *Woodform Architectural*, with Colorbond metal sheet roof cladding (*Colorbond "surfmist"*).

4.5 No front fencing is proposed to be erected along the site's Albert Street frontage.

4.6 The proposal has an overall site coverage of 50% and a permeability of 39%.

5.0 PLANNING PERMIT PROVISIONS

Zone

5.1 General Residential Zone: Pursuant to Clause 32.08-4 of the Kingston Planning Scheme a planning permit is required to construct two (2) or more dwellings on a lot. A development must meet the requirements of Clause 55 of the Scheme. Schedule 2 to the General Residential Zone includes a variation to one (1) standard within Clause 55.

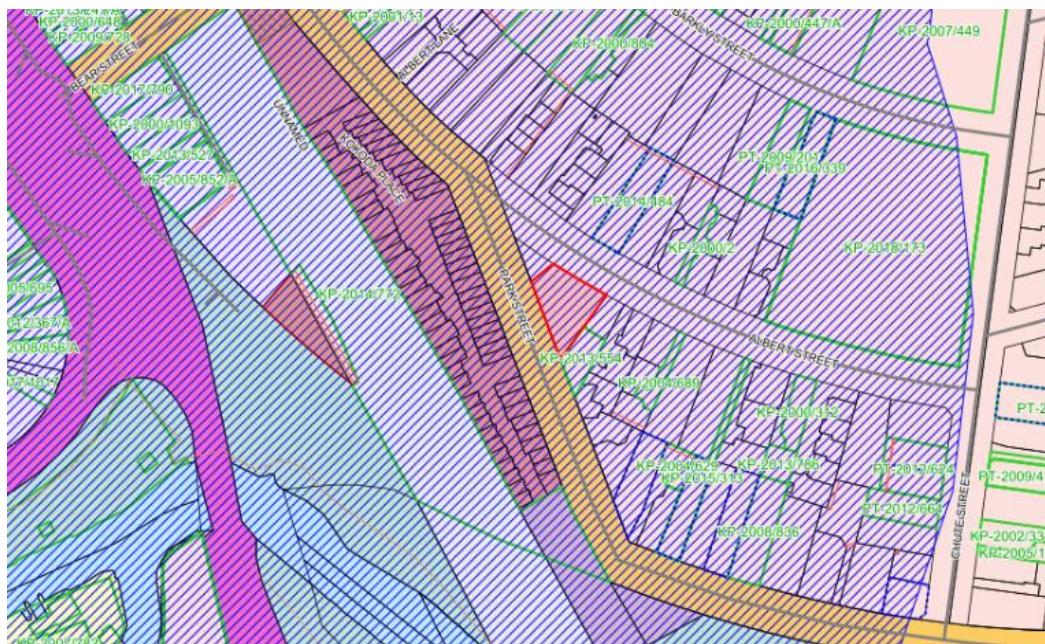
Overlay

5.2 Not applicable.

Particular Provisions

5.3 **Car parking** – It is noted that recent introduction of Amendment VC148 gazetted on 31 July 2018, has changed the planning permit requirements and number of spaces to be provided in certain circumstances, particularly located within *Principal Public Transport Network Area Maps*, (State Government of Victoria, 2018). Accordingly the visitors' car parking requirement to every 5 dwellings for developments of 5 or more dwellings is 0 in this instance.

5.4 Below map shows that the subject land is located within *Principal Public Transport Network Area* (blue hatched area), where no visitors' car parking is required to be provided.



5.5 Car Parking contains the following residential car parking rates:

- 1 space to each 1 or 2 bedroom dwelling;
- 2 spaces to each 3 or more bedroom dwelling; and
- 0 visitor space for every 5 dwellings.

This equates to a parking requirement of 8 spaces for the proposed development.

As the required number of car parking spaces is provided on the site, a planning permit is not required for a reduced car parking rate pursuant to Clause 52.06-3.

- 5.6 Clause 55 - Two or More Dwellings on a Lot & Residential Buildings – (Refer to Appendix A for the Planning Officer’s full assessment against this report).

General Provisions

- 5.7 The Decision Guidelines of Clause 65 of the Kingston Planning Scheme are relevant to this application and require consideration to be given to a variety of matters including planning scheme policies, the purpose of the zone, orderly planning and the impact on amenity.

6.0 RELEVANT POLICIES

6.1 State Planning Policy Framework (SPPF)

- Clause 11 Settlement
- Clause 15 Built Environment
- Clause 16 Housing

6.2 Local Planning Policy Framework (LPPF)

- Clause 21.05 Residential Land Use
- Clause 22.11 Residential Development Policy

7.0 ADVERTISING

- 7.1 The proposal was advertised by sending notices to adjoining and opposite property owners and occupiers and by maintaining a notice on site for fourteen (14) days. Three (3) objection(s) to the proposal were received. The grounds of objection raised are summarised as follows:

- Parking and traffic
- Neighbourhood character and three storey
- Built form
- Vegetation
- Acoustic windows

8.0 SECTION 57A – AMENDMENT TO PLANS

- 8.1 Following advertising the Permit Applicant lodged amended plans on 16 October 2018, pursuant to Section 57A of the *Planning and Environment Act 1987* the amended plans incorporated the following changes:

- Relocation of bicycle parking closer to Albert
- Dwellings 4 and 5 first floor reduced cantilever over accessway through internal rearrangement and, a 50mm reduction to the front setback to the north western boundary of dwelling 4. Street entry.
- Dwelling 2 balcony setback from dwelling 4 and 5 increased through reconfigured layout of balcony and internal layout. Setback between both forms increased to 3.43m.
- Dwelling 1 and 5 have their balconies shifted to a location with a clear northern aspect to receive direct solar access.
- Bedroom 1 of Dwelling 2 has been setback further from the adjoining properties an additional 1.52m.
- Dwelling 3 increased upper floor setback an additional 12mm.
- Dwelling 1 increased upper floor setback an additional 10mm.

- 8.2 It is these plans that form the basis of this recommendation and are described at section 4 of this report.

8.3 The revised plans were also forwarded to the objectors and one objector responded to the revised proposal however, all three (3) objections still stand.

9.0 REFERRALS

9.1 The application was referred (and re-referred, where necessary) to the following internal departments:

- Council's Development Engineer – no objection, subject to a number of permit conditions including a requirement for the provision of a storm water management plan.
- Council's Roads and Drains Department – no objection, subject to a number of standard conditions on any permit issued.
- Council's ESD Officer – no objection to the revised plans, given that each of the initial design details requested by the ESD have now been satisfied with minor changes on the SDA report and some shadings to the glazed windows are requested.
- Council's Traffic Engineers – has no objection subject to provision of the swept path analysis to show that the garages can be accessed and leave in a forward direction.
- Council's Vegetation Management Officer / Street Trees – no objection, subject to the inclusion of suitable permit conditions including a landscape plan and associated planting schedule and the protection of the existing street tree at Albert Street.

9.2 The application was not required to be referred to any external authorities, pursuant to Clause 66.02 of the Kingston Planning Scheme.

10.0 PLANNING CONSIDERATIONS:

State Planning Policy Framework

10.1 The Planning Policy Framework sets out the relevant state-wide policies for residential development at **Clause 11** (Settlement), **Clause 15** (Built Environment and Heritage) and **Clause 16** (Housing). Essentially, the provisions within these clauses seek to achieve the fundamental objectives and policy outcomes sought by "Plan Melbourne 2017-2050: Metropolitan Planning Strategy" (Department of Environment, Land Water and Planning, 2017).

10.2 **Clause 11** seeks to ensure planning anticipates and respond to the needs of existing and future communities through provision of zoned and serviced land for housing, employment, recreation and open space, commercial and community facilities and infrastructure.

Planning is to prevent environmental and amenity problems created by siting incompatible land uses close together.

Planning is to facilitate sustainable development that takes full advantage of existing settlement patterns and investment in transport, utility, social, community and commercial infrastructure and services.

- 10.3 **Clause 15** (Built Environment and Heritage) aims to ensure all new land use and development appropriately responds to its landscape, valued built form and cultural context, and protect places and sites with significant heritage, architectural, aesthetic, scientific and cultural value.
- 10.4 **Clause 15.01-1S** encourages development to achieve high quality architectural and urban design outcomes that contribute positively to neighbourhood character, minimises detrimental amenity impacts and achieves safety for future residents, and the community, through good design. The provisions of **Clause 15.02-1S** promote energy and resource efficiency through improved building design, urban consolidation and promotion of sustainable transport.
- 10.5 **Clause 15.03-2S (Aboriginal Cultural Heritage)** seeks to ensure the protection and conservation of places of Aboriginal cultural heritage significance.
- 10.6 The subject land **is** identified in an area of Aboriginal Cultural Heritage Sensitivity, however, the proposed activity is **exempt** from requiring a Cultural Heritage Management Plan, as the development involves three or more dwellings on a lot where:
- It is less than 0.11 hectares in size; and
 - Is not within 200 metres of the coastal waters of Victoria, any sea within the limits of Victoria or the Murray River.
- 10.7 Housing objectives are further advanced at Clause 16, which seek encourage increased diversity in housing.
- 10.8 Clause 16.01 (Residential Development) seeks to promote a housing market that meets community needs, and is located in areas which offer good access to jobs, services and transport. Clause 16.01-2R specifically requires consideration of population growth in locations that are considered major and neighbourhood activity centres, especially those with good public transport connections.
- 10.9 It is submitted that the proposed development satisfies the aforementioned State strategies and policy direction. Specifically, the subject site is located on land earmarked for residential purposes, whereby residential development is an 'as of right' use under the zoning provisions. Subject to appropriate conditions on any permit issued, the development itself achieves an acceptable design outcome for the site and its immediate abuttals, whilst enjoying convenient and direct access to community facilities and the like, including public transport nodes.

Local Planning Policy Framework

- 1010 The City of Kingston's MSS at **Clause 21.05 (Residential Land Use)** of the Kingston Planning Scheme, seeks to provide guidance to development in residential zoned land, mixed use zoned lands and land within activity centres. The Residential Land Use Framework Plan illustrates the range of housing outcomes sought across the City of Kingston.

10.11 Relevant objectives and strategies in Clause 21.05-3: Residential Land Use include:

- *To provide a range of housing types across the municipality to increase housing diversity and cater for the changing housing needs of current and future populations, taking account of the capacity of local areas in Kingston to accommodate different types and rates of housing change. This is to be achieved through encouraging residential development within activity centres via mixed-use development, and on transitional sites at the periphery of activity centres.*
- *To ensure new residential development respects neighbourhood character and is site responsive, and that medium density dwellings are of the highest design quality. This is to be achieved through promoting new residential development, which is of a high standard, responds to the local context and positively contributes to the character and identity of the local neighbourhood.*
- *To promote more environmentally sustainable forms of residential development. To be achieved through promoting medium density housing development in close proximity to public transport facilities, particularly train stations.*
- *To manage the interface between residential development and adjoining or nearby sensitive/strategic land uses.*
- *To ensure residential development does not exceed known physical infrastructure capacities.*
- *To recognise and response to special housing needs within the community.*

10.12 Council's Local Planning Policy at Clause 21.05 essentially reinforces State Planning Policy relevant to housing, stressing the need to encourage urban consolidation in appropriate locations and to accommodate projected population increases.

10.13 **Clause 22.11 Residential Development Policy** extends upon the provision contained at **Clause 21.05 (Residential Land Use)**, relating to increased housing diversity areas, incremental housing change areas, minimal housing change areas, residential renewal areas and neighbourhood character. It provides design guidance on how new residential development should achieve architectural and urban design outcomes that positively respond to neighbourhood character.

10.14 Relevant objectives in Clause 22.11-2 Residential Development Policy include:

- *To promote a managed approach to housing change, taking account of the differential capacity of local areas in Kingston to accommodate increased housing diversity, incremental housing change, residential renewal or minimal housing change, as identified within the MSS.*
- *To encourage new residential development to achieve architectural and urban design outcomes that positively respond to neighbourhood character having particular regard to that identified in the Kingston Neighbourhood Character Guidelines – August 2007.*
- *To promote on-site car parking which is adequate to meet the anticipated needs of future residents.*

- *To ensure that landscaping and trees remain a major element in the appearance and character of the municipality's residential environments.*
- *To limit the amount and impact of increased stormwater runoff on local drainage systems.*
- *To ensure that the siting and design of new residential development takes account of interfaces with sensitive and strategic land uses.*

10.16 It is considered that the proposed development generally with the State and Local Planning Policy Framework guidelines which aim to encourage well-designed medium density housing in appropriate locations. This is discussed in the Clause 55 assessment, later within this report.

Zoning Provisions

10.17 The mandatory height requirement of 11 metres (and not more than 3 storeys) under Schedule 2 to the General Residential Zone has been satisfied by the proposal, with a maximum overall building height of 9.6 metres proposed. Additionally, the mandatory requirement for the provision of minimum garden area associated with new dwellings under Clause 32.08-4 of the Scheme is applicable to this application as it was lodged after the introduction of Amendment VC110 on the 27th March, 2017. The proposal satisfies the minimum requirement of 35% garden area (given that the site area exceeds 650m²) with 297m² or 37% of the site to be set aside for this purpose.

Overlay Provisions

10.18 Not Applicable

11.0 CLAUSE 55 (RESCODE ASSESSMENT)

11.1 The proposal has been assessed against the objectives and standards of Clause 55 (ResCode) of the Kingston Planning Scheme (refer to Appendix A). Clause 55 requires that a development **must** meet all of the objectives, and all of the standards of this clause **should** be met. Variations to the standards are able to be considered where it is determined that the overall objective is met.

11.2 The following assessment gives further discussion to that in the attached Appendix, particularly those standards where concessions are sought. Overall, it is noted that the application achieves a high level of compliance with the ResCode provisions, with only minor variations sought. Only two (2) of the thirty-three (33) ResCode standards are sought to be varied, which is discussed within the Appendix A.

Clause 55.02 – Neighbourhood Character & Infrastructure

Standard B1 – Neighbourhood Character

11.3 One of the key objectives of Clause 55.02-1 is "*to ensure that the design respects the existing neighbourhood character and responds to the features of the site and surrounding area*". Standard B1 of ResCode suggests that the proposed design should respect the existing or preferred neighbourhood character and respond to the features of the site. The subject site is located within a predominately residential area where medium density housing is already prevalent, due to the area being within close proximity to an established "major activity centre" (i.e. Mordialloc) and an increasing demand for greater housing choice. It should be noted that proposed two or three storey development with a contemporary built form is a

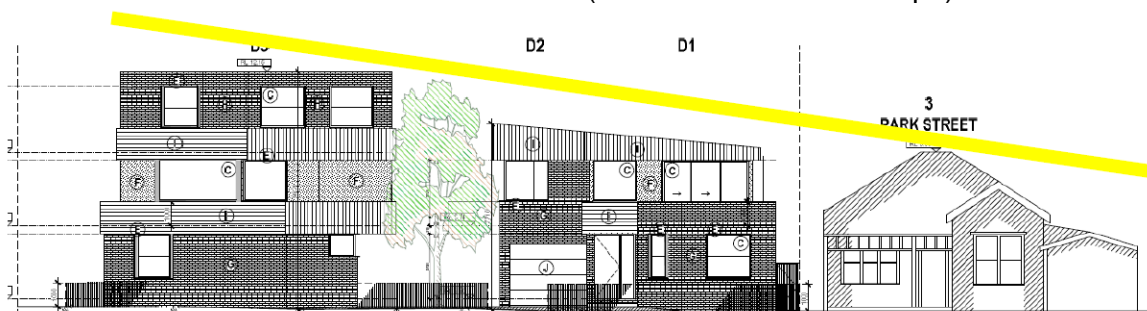
new evolving built form in the area. This is evidenced by the opposite site (south-west), the Kokoda Place has been developed for three storey, attached contemporary type of developments. Similarly No 88 Albert Street has been developed with a three (3) double-storey dwellings and a single-storey dwelling which are contemporary in nature. In addition to this No 86 Albert Street has been recently approved for 5 dwellings with a contemporary built form. These examples show proposed built form is a trend of new neighbourhood character / built form of the area.

Whilst the three-storey height associated with the new dwellings diverges from the more traditional types of housing within Albert Street, it is clear that the established residential areas located on the periphery of the activity centre are being targeted for more intensive forms of housing. It is acknowledged that the existing residential zone area currently excludes specific examples of three storey development. However, the policy guidance in these types of areas provides strong support for intense housing supply. It is also acknowledged as stated above that housing form in this area is changing rapidly.

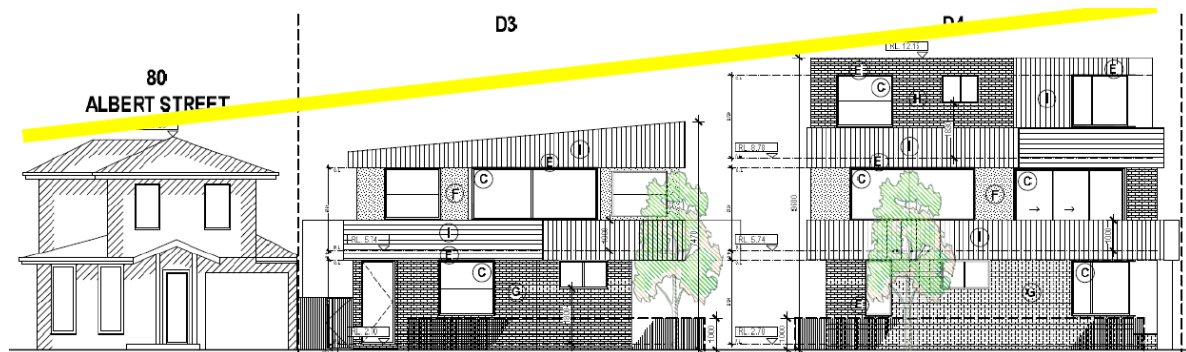
It should be noted that as shown below, the opposite side of the subject site is developed for 66 dwellings consisting of three and two storey dwellings.



The scale and height of the proposal has provided a respectful graduation to south adjoining dwellings, 80 Albert Street (2 storeys) and 3 Park Street (single storey). Dwellings 1-3 are limited to 2 storeys and dwellings 4-5 to 3 storeys. These heights are relative in scale and allow transition at a moderate rate as well. (Refer to below streetscape)



South-west elevation along the Park Street. The yellow line notes low grade change over 42m



North-east elevation along the Albert Street. Yellow line notes low grade change over 36m.

It is noted that the proposed sloped roof form provides a graduation in height and responds to the roofed character of the area, generally of pitched within older stock of the area.

Council officers consider that the proposed dwellings have been designed and sited in a manner which is reflective of the emerging character of the surrounding area of Mordialloc and would make efficient use of the land. The site being a corner of two streets (Albert and Park), and having streets at the three sides of the subject site, the third storey element would be reduced amenity impacts and visual bulk concerns from the adjoining properties.

Accordingly, the design and siting of the proposed development should not unduly affect the directly adjoining residential properties to the subject land and, as such, offers an appropriate planning outcome for the site.

11.4 It is considered that the proposed development generally complies and satisfies the State and Local Planning Policy Framework guidelines which aim to encourage well-designed medium density housing in appropriate locations.

12.0 RESPONSE TO GROUNDS OF OBJECTIONS

12.1 The objector concerns have largely been addressed in the body of this report.

12.2 **Parking and traffic concerns** - The issue of increasing car parking demand and additional local traffic has also been raised as an objection to the proposal. As previously noted, the proposal provides total of eight (8) car spaces with seven spaces in the garage and one (1) tandem space in the drive way, which meets the statutory requirements for car parking under Clause 52.06 of the Scheme. Council's Traffic Engineers have also offered no objection to the proposal and has indicated that the proposed development should not result in an unreasonable increase in traffic movements within the surrounding street network. Further, it has been stated that no parking permits are likely to be issued to residents of this development. As discussed earlier in this report, Mordialloc Station and many bus lines are within less than 350m of the site. It is also noted that all dwellings would be provided with a wall mounted bike rack in the garage and one (1) visitor bike space to be provided on the site located within communal area, which is a encouragement for a green development within the area.

12.3 **Vegetation** – it was requested to ensure landscape plantings include 50-60% local native plants and no invasive species. Council's Vegetation Department recommended a landscape plan should be chosen the species to comprise of 100% coastal indigenous species by plant type and total quantities as a condition.

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12.4 **Acoustic windows** – An objector requested acoustic windows to be incorporated in the design. The proposal provides double glazing windows to all living areas and bedrooms. Further, the SDA report and drawings were referred to Council's ESD Officer who has no objection to the proposal subject to the conditions.

13.0 **CONCLUSION:**

13.1 On balance, the proposal is considered to substantially comply with the relevant planning policy and therefore should be supported.

13.2 As outlined above, it has been determined that prior to deciding on this application all factors pursuant to section 60(1) of the Act have been considered. Further to this, the proposal does not give rise to any significant social and economic effects.

13.3 The proposed development is considered appropriate for the Site, subject to conditions, as evidenced by:

- The compatibility of the design and siting with the surrounding area;
- The mitigation of off-site amenity impacts; and
- A suitable level of compliance with all relevant policies, including Clause 55 of the Kingston Planning Scheme

14.0 **RECOMMENDATION**

14.1 That Council determine to support the proposal and issue a Notice of Decision to Grant a Permit **to develop the land for the construction of five (5) dwellings** at No. 1 Park Street, Mordialloc, subject to the following conditions:

1. Before the development starts amended plans to the satisfaction of the Responsible Authority must be submitted to and approved by the Responsible Authority. When approved, the plans will be endorsed and will then form part of the permit. The plans must be drawn to scale with dimensions and three copies must be provided. The plans must be substantially in accordance with the plans submitted to Council on archsigh, Job No 17322, Sheets TP1.0 to TP5.0 and SH1.0 to SH8.0 (Revision C) received on 16 October and 8 November 2018, but modified to show:
 - a. the provision of an improved landscape plan and associated planting schedule for the site showing the proposed location, species type, mature height and width, pot sizes and number of species be planted on the site, with such plans to be prepared by a suitably qualified landscape professional and incorporating:
 - i. A planting schedule of all proposed trees and shrubs, including botanical names, common names, pot sizes, sizes at maturity, and quantities of each plant;
 - ii. A survey, including, botanical names of all existing trees to be retained or removed on the site including Tree Protection Zones for trees to be retained calculated in accordance with AS4970-2009;
 - iii. A survey including botanical names, of all existing trees on neighbouring properties where the Tree Protection Zones of such trees calculated in accordance with AS4970-2009 fall partially within the subject site;
 - iv. The delineation of all garden beds, paving, grassed area, retaining walls, fences and other landscape works;

- v. A range of plant types from ground covers to large shrubs and trees, provided at adequate planting densities (e.g. plants 1m width at maturity planted 1m apart); with the species chosen to comprise of 100% coastal indigenous species by plant type and total quantities;
 - vi. The provision of four (4) canopy trees capable of growing to minimum mature dimensions of 6m height and 4m width to be planted within the front setback of the property along Albert Street, with the species chosen to be approved by the Responsible Authority, and;
 - vii. The provision of two (2) canopy trees capable of growing to minimum mature dimensions of 10m height and 6m width to be planted within the front setback of the property along Park Street, with the species chosen to be approved by the Responsible Authority, and;
 - viii. The provision of two (2) canopy trees capable of growing to minimum mature dimensions of 6m height and 4m width to be planted within the front setback of the property along Park Street, with the species chosen to be approved by the Responsible Authority, and;
 - ix. All trees provided at a minimum of 2 metres in height at time of planting, medium to large shrubs to be provided at a minimum pot size of 200mm;
 - x. No trees with a mature height over five (5) meters are to be planted over proposed or existing easements;
 - xi. The provision of notes regarding site preparation, including the removal of all weeds, proposed mulch, soil types and thickness, subsoil preparation and any specific maintenance requirements;
 - xii. The location of any tree protection measures including for street trees accurately drawn to scale and labelled;
 - xiii. The landscaping shown on the endorsed plans must be maintained to the satisfaction of the Responsible Authority, including that any dead, diseased or damaged plants are to be replaced
- b. the provision of minimum 2000 litre rainwater tank clearly nominated for each dwelling with water re-used for toilet flushing
 - c. vehicle crossings must constructed at a 90 degree alignment with the kerb on Albert Street and all internal driveways be aligned with the existing / proposed vehicle crossing.
 - d. a note on the ground floor plan stating "The proposed vehicle crossing not to be within the prohibited zone (6 metres from the tangent of the corner)"
 - e. the provision of a swept path analysis to show the garages can be accessed and exit in a forward direction
 - f. provision of operable external shading to North, East and West facing glazing (not shaded by balconies or shading devices) to prevent glare and overheating
 - g. all relevant commitments identified within the Sustainable Design Assessment (including comments from Council's ESD Officer), required under condition 11 of this permit, shown on plans
 - h. all the boundary and internal fences height and type nominated on the ground floor plan corresponding with the elevation plan
 - i. the surface material of all driveways / accessways and car parking spaces nominated in all-weather coloured concrete sealcoat, or similar

- j. a notation that confirms property boundaries, footpaths and vehicle crossing levels are to be raised to the satisfaction of Council's Roads and Drains Department

Endorsed Plans

- 2. The development as shown on the endorsed plans must not be altered without the prior written consent of the Responsible Authority.

Street Tree

- 3. Tree Protection Fencing is to be established around the *Callistemon viminalis* (Weeping Bottlebrush) located in the Albert Street nature strip and the *Eucalyptus leucoxylon* (Yellow Gum) located in the Park Street nature strip, prior to demolition and maintained until all works on site are complete. The fencing must:
 - i. be a 1.8 metre high temporary fence constructed using steel or timber posts fixed in the ground or to a concrete pad, with the fence's side panels to be constructed of cyclone mesh wire or similar strong metal mesh or netting
 - ii. encompass the entire nature strip with each end 3 metres from the base of the tree.

Construction Management

- 4. Prior to the commencement of any buildings and works on the land (including demolition), a Construction Management Plan (CMP), to the satisfaction of the Responsible Authority, must be submitted to and approved by the Responsible Authority. The CMP must be prepared in accordance with the City of Kingston Construction Management Policy, July 2015 and Construction Management Guidelines, 1 November 2015 (and any superseding versions and / or documents). The CMP must specify and deal with, but is not limited to, the following elements:
 - a. Public Safety, Amenity and Site Security
 - b. Traffic Management
 - c. Stakeholder Management
 - d. Operating Hours, Noise and Vibration Controls
 - e. Air Quality and Dust Management
 - f. Stormwater and Sediment Control
 - g. Waste and Materials Re-use

When approved, the plan will be endorsed and will then form part of the permit and shall thereafter be complied with during the undertaking of all works.

In the event of damage during construction to any adjacent Council roads, footpaths and park land, such damage will be required to be repaired by and at the full cost to the developer, to the satisfaction of the Responsible Authority.

Drainage and Water Sensitive Urban Design

5. Unless with the prior written consent of the Responsible Authority, before the development commences, the following Integrated Stormwater Management documents must be prepared, by a suitably qualified person, to the satisfaction of the Responsible Authority.
 - a. Stormwater Management/drainage (drainage) Plan(s) must be prepared, with supporting computations, showing the stormwater (drainage) works to the nominated point of discharge. The plan(s) must show all details of the proposed stormwater (drainage) works including all existing and proposed features that may have impact on the stormwater (drainage) works, including landscaping details.
 - b. The Stormwater Management (drainage) Plan must address the requirements specified within Council's "Civil Design requirements for Developers – Part A: Integrated Stormwater Management".
 - c. A STORM modelling report with results demonstrating water sensitive urban design treatments that achieve Victorian best practice objectives with a minimum 100% rating must be provided as part of the Stormwater Management (drainage) Plan to the satisfaction of the Responsible Authority. These may include the use of an infiltration or bio-retention system, rainwater tanks connected for reuse, or other treatments to the satisfaction of the Responsible Authority.
 - d. The water sensitive urban design treatments as per conditions 5a, 5b & 5c above must be implemented on-site, unless an alternative agreement for stormwater quality in-lieu contribution is reached with the Responsible Authority.
6. Stormwater/drainage works must be implemented in accordance with the approved stormwater management/drainage plan(s) and to the satisfaction of the Responsible Authority including the following:
 - a. All stormwater/drainage works must be provided on the site so as to prevent overflows onto adjacent properties.
 - b. The implementation of stormwater/drainage detention system(s) which restricts stormwater discharge to the maximum allowable flowrate of 7L/s.
 - c. All stormwater/drainage works must be maintained to the satisfaction of the Responsible Authority.

Infrastructure and Road Works

7. The replacement of all footpaths, including offsets, must be constructed to the satisfaction of the Responsible Authority.
8. All reinstatements and vehicle crossings must be constructed to the satisfaction of the Responsible Authority.
9. All redundant vehicle crossings must be removed (including redundant portions of vehicle crossings) to the satisfaction of the Responsible Authority.

Parking and Traffic Management

10. Prior to the occupation of the dwellings hereby permitted, areas set aside for parking vehicles, access lanes and paths as shown on the endorsed plans must be:
 - a. Constructed to the satisfaction of the Responsible Authority.
 - b. Properly formed to such levels that they can be used in accordance with the plans.

- c. Surfaced in accordance with the endorsed plans under this permit or in an all weather coloured concrete seal-coat, to the satisfaction of the Responsible Authority.
- d. Drained and maintained to the satisfaction of the Responsible Authority.

Parking areas and access lanes must be kept available for these purposes at all times and maintained to the satisfaction of the Responsible Authority.

Sustainable Design Assessment

- 11. Prior to the endorsement of the plans required pursuant to Condition 1 of this permit, the provision of a Sustainable Design Assessment (SDA) to be prepared by a suitably qualified professional must be submitted to and approved by the Responsible Authority. The SDA must include, but is not limited to, detailing initiatives for stormwater harvesting, insulation, building materials, daylighting, collective rainwater tanks and/or individual rainwater tanks, public and private landscape irrigation and car washing, energy efficient concepts, glazing and internal ventilation and the like.

Completion of Works

- 12. Prior to the occupation of the dwellings hereby permitted, the landscaping works as shown on the endorsed plans must be completed to the satisfaction of the Responsible Authority. The landscaping must then be maintained to the satisfaction of the Responsible Authority.
- 13. Prior to the occupation of the dwellings hereby permitted, all buildings and works and the conditions of this permit must be complied with, unless with the further prior written consent of the Responsible Authority.
- 14. All works on or facing the boundaries of adjoining properties must be finished and surface cleaned to a standard that is well presented to neighbouring properties in a manner to the satisfaction of the Responsible Authority.
- 15. All piping and ducting above the ground floor storey of the development (other than rainwater guttering and downpipes) must be concealed to the satisfaction of the Responsible Authority.
- 16. Once the development has started it must be continued and completed to the satisfaction of the Responsible Authority.

Expiry

- 17. This permit as it relates to **development (buildings and works)** will expire if one of the following circumstances applies:
 - a. The development is not started within two (2) years of the issue date of this permit.
 - b. The development is not completed within four (4) years of the issue date of this permit.

In accordance with Section 69 of the Planning and Environment Act 1987, an application may be submitted to the responsible authority for an extension of the periods referred to in this condition.

Note: Prior to the commencement of the development you are required to obtain the necessary Building Permit.

Note: The applicant/owner must provide a copy of this planning permit to any appointed Building Surveyor. It is the responsibility of the applicant/owner and Building Surveyor to ensure that all building development works approved by any building permit is consistent with the planning permit.

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Note: Environment Protection Authority (EPA) Victoria set out the requirements pertaining to site construction hours and permissible noise levels.

Note: The allocation of street numbering and addressing of properties is vested in Council. Any reference to addressing or dwelling/unit/apartment and street numbers or street names on any endorsed plan is indicative only. The onus is on the Permit Applicant/Land Owner to contact Council's Property Data Department to determine the official dwelling/unit/apartment street numbers, street name details and the like for the approved development.

If the Permit Applicant/Land Owner adopts the street numbering or addressing from the endorsed plans, or where advertising and/or sales transact (off the plan) prior to Council's official allocation of the street numbering and addressing, it will be viewed to be non-compliant with the guideline and standard applied (*Australian/New Zealand Standard for Rural & Urban Addressing / AS/NZS 4819:2011*).

Note: The owner(s), occupiers and visitors of the development allowed by this permit may not be eligible for Council resident or visitor parking permits.

Note: Permit applicant/owner to contact United Energy in relation to the proposed crossover.

OR

In the event Council wishes to oppose the Officer Recommendation to support the application, it can do so on the following grounds:

1. The proposal fails to meet the objectives and strategic directions of the Municipal Strategic Statement – Residential Land Use contained at Clause 21.05 of the Kingston Planning Scheme.
2. The proposal does not satisfy the requirements of Clause 22.11 – Residential Development Policy, of the Kingston Planning Scheme.
3. The proposal fails to satisfy all the requirements of Clause 55 of the Kingston Planning Scheme (ResCode), in particular Clause 55.02-1 Neighbourhood Character Objective; Clause 55.02-2 Residential Policy Objective; Clause 55.03-1 Street setback objective; and Clause 55.04-5 Overshadowing open space objective.
4. The proposal fails to provide an acceptable built form outcome having regard to the physical and policy context.

APPENDIX A – RESCODE ASSESSMENT

Standard of the Kingston Planning Scheme

Two or more dwellings on a lot and residential buildings (Clause 55 and Schedule 2 to the General Residential Zone)

OBJECTIVE	STANDARD	LEVEL OF COMPLIANCE AGAINST STANDARD
<p>Clause 55.02-1 Neighbourhood Character objectives</p> <ul style="list-style-type: none"> • To ensure that the design respects the existing neighbourhood character or contributes to a preferred neighbourhood character. • To ensure that development responds to the features of the site and the surrounding area. 	<p>Standard B1</p> <ul style="list-style-type: none"> • The design response must be appropriate to the neighbourhood and site. • The proposed design must respect the existing or preferred neighbourhood character and respond to site features. 	<p>Complies with standard & meets objective</p>
<p>Assessment: Refer to Section 11 of this report for further discussion.</p>		

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<p>Clause 55.02-2 Residential Policy objectives</p> <ul style="list-style-type: none"> To ensure that residential development is provided in accordance with any policy for housing in the MPS and the PPF. To support medium densities in areas where development can take advantage of public transport and community infrastructure and services. 	<p>Standard B2</p> <ul style="list-style-type: none"> An application must be accompanied by a written statement that describes how the development is consistent with relevant housing policy in the PPF & MPS 	<p>Complies with standard & meets objective</p>
<p>Assessment: The proposal is considered to be consistent with all relevant sections of the PPF, LPPF (including Council's MSS) and local planning policies, namely Council's Residential Development Policy under Clause 22.11 of the Kingston Planning Scheme. The subject site is located within an area targeted for "increased housing diversity" and is located on the periphery of the Mordialloc Activity Centre.</p>		
<p>Clause 55.02-3 Dwelling Diversity objective</p> <p>To encourage a range of dwelling sizes and types in developments of ten or more dwellings.</p>	<p>Standard B3</p> <p>Developments of ten or more dwellings should provide a range of dwelling sizes and types, including:</p> <ul style="list-style-type: none"> Dwellings with a different number of bedrooms. At least one dwelling that contains a kitchen, bath or shower, and a toilet and wash basin at ground floor level. 	<p>N/A</p>
<p>Assessment: Less than ten (10) dwellings proposed.</p>		
<p>Clause 55.02-4 Infrastructure objectives</p> <ul style="list-style-type: none"> To ensure development is provided with appropriate utility services and infrastructure. To ensure development does not unreasonably overload the capacity of utility services and infrastructure. 	<p>Standard B4</p> <ul style="list-style-type: none"> Connection to reticulated services/sewerage, electricity, gas and drainage services Capacity of infrastructure and utility services should not be exceeded unreasonably Provision should be made for upgrading and mitigation of the impact of services or infrastructure where little or no spare capacity exists 	<p>Complies with standard & meets objective</p>
<p>Assessment: It is recommended that suitable condition(s) be included in any permit issued to address infrastructure considerations. No easements are contained within the property.</p>		
<p>Clause 55.02-5 Integration with the street objective</p> <ul style="list-style-type: none"> To integrate the layout of development with the street. 	<p>Standard B5</p> <ul style="list-style-type: none"> Provides adequate vehicle and pedestrian links that maintain or enhance local accessibility. 	<p>Complies</p>
	<ul style="list-style-type: none"> Development oriented to front existing/proposed streets 	<p>Complies</p>
	<ul style="list-style-type: none"> High fencing in front of dwellings should be avoided if practicable. 	<p>Complies</p>

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	<ul style="list-style-type: none"> Development next to existing public open space should be laid out to complement the open space. 	Complies
<p>Assessment: The proposed development would be orientated towards Albert Street and Park Street. Only 1m high front fencing is proposed.</p>		
<p>Clause 55.03-1 Street setback objective</p> <ul style="list-style-type: none"> To ensure that the setbacks of buildings from a street respect the existing or preferred neighbourhood character and make efficient use of the site. 	<p>Standard B6 Walls of buildings should be set back from streets:</p> <ul style="list-style-type: none"> If no distance is specified in a schedule to the zone, the distance specified in Table B1 <p>Required: 5.1 metres</p>	Variation sought to standard & meets objective
<p>Assessment: It is noted that the subject site is irregular in shape and has faces two side streets. The pattern of development in the neighbourhood is generally maintained. Staggered setbacks are provided to Park Street of between 3.00 metres to 5.14 metres, which is consistent with the streetscape character. From Albert Street the front walls are setback approximately 3 metres and will generally align with the built form of the adjacent dwelling. The proposed setbacks will provide an appropriate transition to adjoining properties on both streetscapes, make efficient use of this irregular shaped site and ensure the visual impact of the building when viewed from the street and from adjoining properties would be reasonable in this instance.</p>		
<p>Clause 55.03-2 Building height objective</p> <ul style="list-style-type: none"> To ensure that the height of buildings respects the existing or preferred neighbourhood character. 	<p>Standard B7 Maximum: 11 metres and no more than 3 storeys.</p>	Complies with standard & meets objective
<p>Assessment: The proposal meets the heights parameters specified in this Standard, i.e. 9.02 metres above natural ground level (maximum) is proposed.</p>		
<p>Clause 55.03-3 Site Coverage objective</p> <ul style="list-style-type: none"> To ensure that the site coverage respects the existing or preferred neighbourhood character and responds to the features of the site. 	<p>Standard B8 Maximum: 60%</p>	Complies with standard & meets objective
<p>Assessment: The proposal achieves a site coverage statistic of 50%, which is lower than allowable site coverage of this standard.</p>		

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<p>Clause 55.03-4 Permeability & stormwater management objectives</p> <ul style="list-style-type: none"> • To reduce the impact of increased stormwater run-off on the drainage system. • To facilitate on-site stormwater infiltration. • To encourage stormwater management that maximises the retention & reuse of stormwater 	<p>Standard B9</p> <p>At least: 20%</p>	<p>Complies with standard & meets objective</p>
<p>Assessment: The permeability figure proposed (i.e. 39%) exceeds that specified in the Standard.</p>		
<p>Clause 55.03-5 Energy Efficiency objectives</p> <ul style="list-style-type: none"> • To achieve and protect energy efficient dwellings and residential buildings. • To ensure the orientation and layout of development reduce fossil fuel energy use and make appropriate use of daylight and solar energy. 	<p>Standard B10</p> <p>Orientation, siting & design of buildings should make appropriate use of solar energy. Further, siting & design should ensure that the energy efficiency of existing dwellings on adjoining lots is not unreasonably reduced. Siting & design should also ensure that the performance of existing rooftop solar energy facilities on dwellings on adjoining lots in GRZ, NRZ or TZ are not unreasonably reduced. The existing rooftop solar energy facility must exist at the date the application is lodged.</p> <p>Living areas & private open space should be located on the north side of the development, if practicable.</p> <p>Solar access to north-facing windows is maximised.</p>	<p>Complies with standard & meets objective</p>
<p>Assessment: The orientation and layout of the proposed development should make good use of daylight and solar energy. The primary living area and POS/balconies for the new dwellings benefit from a northern, eastern or western orientation, and north-facing windows have been provided wherever practicable to maximise internal amenity</p>		
<p>Clause 55.03-6 Open Space objective</p> <ul style="list-style-type: none"> • To integrate the layout of development with any public and communal open space provided in or adjacent to the development. 	<p>Standard B11</p> <p>Public or communal open space should:</p> <ul style="list-style-type: none"> • Be substantially fronted by dwellings • Provide outlook for dwellings • Be designed to protect natural features. • Be accessible and useable. 	<p>N/A</p>
<p>Assessment: No communal open space areas would be created as a part of the proposal. However, the north side road reserve area exists. The proposed layout has been integrated with this reserve by providing windows facing it.</p>		

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<p>Clause 55.03-7 Safety objectives</p> <ul style="list-style-type: none"> To ensure the layout of development provides for the safety and security of residents and property. 	<p>Standard B12</p> <p>Entrances to dwellings and residential buildings should not be obscured or isolated from the street and internal accessways.</p> <p>Planting should not create unsafe spaces along streets and accessways</p> <p>Good lighting, visibility and surveillance of car parks and internal accessways should be achieved.</p> <p>Private spaces should be protected from inappropriate use as public thoroughfares.</p>	<p>Complies with standard & meets objective</p>
<p>Assessment:</p> <ul style="list-style-type: none"> The new dwellings each have readily identifiable front entrances and provide active street frontages with habitable room windows to the facades. Developments has also provided with good visibility and surveillance in internal access ways through windows and doors. Private spaces within developments has been protected from inappropriate use as public thoroughfares. 		
<p>Clause 55.03-8 Landscaping objectives</p> <ul style="list-style-type: none"> To encourage development that respects the landscape character of the neighbourhood. To encourage development that maintains and enhances habitat for plants and animals in locations of habitat importance. To provide appropriate landscaping. To encourage the retention of mature vegetation on the site. 	<p>Standard B13</p> <p>In summary, landscape layout & design should:</p> <ul style="list-style-type: none"> Protect predominant landscape features of the neighbourhood. Take into account the soil type and drainage patterns of the site. Allow for intended vegetation growth and structural protection of buildings. Provide a safe, attractive and functional environment for residents. <p>In summary, development should:</p> <ul style="list-style-type: none"> Provide for the retention or planting of trees, where these are part of the character of the neighbourhood. Provide for the replacement of any significant trees that have been removed in the 12 months prior to the application being made. Specify landscape themes, vegetation (location and species), paving and lighting. 	<p>Complies with standard & meets objective</p>
<p>Assessment:</p> <ul style="list-style-type: none"> The application was referred to Vegetation Department. According to response, the subject site contains limited vegetation, specifically 2 small exotic fruit trees within the front setback along Park Street. No vegetation on the subject site is worthy of retention. Vegetation Department supports the removal of all vegetation on the subject site, however appropriate replacement landscaping will be a condition of any permit issued. Given the proximity of the subject site to the foreshore and Mordialloc creek, indigenous plants will be a condition of the permit. There is a <i>Fraxinus</i> sp. (Ash) located on the neighbouring property to the south-east, approximately 2m from the shared boundary. This tree will be adjacent to the front setback garden area of the proposed development and is unlikely to be impacted by the proposed development. There are two street trees adjacent to the subject site. Along Albert Street there is a mature <i>Callistemon viminalis</i> (Weeping Bottlebrush) and along Park Street there is a young <i>Eucalyptus leucoxylon</i> (Yellow Gum). Both street trees are to be retained and protected during development. The application provides adequate space for the planting of various species, including canopy trees. As a condition of any permit issued, a detailed landscape plan and street tree protection will be required. 		

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<p>Clause 55.03-9 Access objective</p> <ul style="list-style-type: none"> To ensure the number and design of vehicle crossovers respects the neighbourhood character. 	<p>Standard B14 The width of accessways or car spaces should not exceed:</p> <ul style="list-style-type: none"> 33 per cent of the street frontage, or if the width of the street frontage is less than 20 metres, 40 per cent of the street frontage. 	Complies
	No more than one single-width crossover should be provided for each dwelling fronting a street.	Complies
	The location of crossovers should maximise the retention of on-street car parking spaces.	Complies
	The number of access points to a road in a Road Zone should be minimised.	N/A
	Access for service, emergency and delivery vehicles must be provided.	Complies
<p>Assessment:</p> <ul style="list-style-type: none"> A new crossover is to be constructed at Park Street. One existing crossover at Albert Street to be removed and one another to be utilised to provide vehicle access to four (4) of the proposed dwellings. The proposal raises no concern with respect to traffic or access related matters, however a condition will be included to provide swept path analysis to show that the garages can be accessed. 		
<p>Clause 55.03-10 Parking location objectives</p> <ul style="list-style-type: none"> To provide convenient parking for resident and visitor vehicles. To protect residents from vehicular noise within developments 	<p>Standard B15 Car parking facilities should:</p> <ul style="list-style-type: none"> Be reasonably close and convenient to dwellings and residential buildings. Be secure. Be well ventilated if enclosed. <p>Shared accessways or car parks of other dwellings and residential buildings should be located at least 1.5 metres from the windows of habitable rooms. This setback may be reduced to 1 metre where there is a fence at least 1.5 metres high or where window sills are at least 1.4 metres above the accessway.</p>	Complies with standard & meets objective
<p>Assessment:</p> <ul style="list-style-type: none"> The proposal raises no concern with respect to the layout and design of on-site car parking. Each dwelling would be provided with convenient and accessible car parking within the proposed car parking area. No habitable rooms are proposed within shared accessways setback. 		

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<p>Clause 55.04-1 Side and rear setbacks objective</p> <ul style="list-style-type: none"> To ensure that the height and setback of a building from a boundary respects the existing or preferred neighbourhood character and limits the impact on the amenity of existing dwellings. 	<p>Standard B17 A new building not on or within 200mm of a boundary should be set back from side or rear boundaries:</p> <ul style="list-style-type: none"> 1 metre, plus 0.3 metres for every metre of height over 3.6 metres up to 6.9 metres, plus 1 metre for every metre of height over 6.9 metres. 	<p>Complies with standard & meets objective</p>
<p>Assessment: The proposal satisfies and in some cases exceeds the formula to Standard B17.</p>		
<p>Clause 55.04-2 Walls on boundaries objective</p> <ul style="list-style-type: none"> To ensure that the location, length and height of a wall on a boundary respects the existing or preferred neighbourhood character and limits the impact on the amenity of existing dwellings. 	<p>Standard B18 A new wall constructed on or within 200mm of a side or rear boundary of a lot or a carport constructed on or within 1 metre of a side or rear boundary of lot should not abut the boundary:</p> <ul style="list-style-type: none"> 10 m plus 25% of the remaining length of the boundary of an adjoining lot, or Where there are existing or simultaneously constructed walls or carports abutting the boundary on an abutting lot, the length of the existing or simultaneously constructed walls or carports, whichever is the greater. 	<p>Complies with standard & meets objective</p>
<p>Assessment: The proposal satisfies and in some cases exceeds the requirement of this schedule.</p> <ul style="list-style-type: none"> Maximum allowable: 17m on north-east boundaries (length) Proposed: 2.3m (as measured) on north-east boundaries (length) with maximum height of 3.2m 		
<p>Clause 55.04-3 Daylight to existing windows objective</p> <ul style="list-style-type: none"> To allow adequate daylight into existing habitable room windows. 	<p>Standard B19 Buildings opposite an existing habitable room window should provide for a light court to the existing window that has a minimum area of 3m² and minimum dimension of 1m clear to the sky.</p>	<p>Complies with standard & meets objective</p>
	<p>Walls or carports more than 3m in height opposite an existing habitable room window should be set back from the window at least 50% of the height of the new wall if the wall is within a 55 degree arc from the centre of the existing window. The arc may be swung to within 35 degrees of the plane of the wall containing the existing window.</p>	<p>Complies with standard & meets objective</p>
<p>Assessment: All existing habitable room windows would be located more than 1.0 metre from any wall associated with the proposed development.</p>		
<p>Clause 55.04-4 North facing windows objective</p> <ul style="list-style-type: none"> To allow adequate solar access to existing north-facing habitable room windows. 	<p>Standard B20 Buildings should be setback 1m if an existing HRW is within 3m of the abutting lot boundary (add 0.6m to this setback for every metre of height over 3.6m & add 1m for every metre of height over 6.9m)</p>	<p>Complies with standard & meets objective</p>
<p>Assessment: The south-east facing wall associated with the proposed development would not be considered as a wall at the north side, therefore, the proposed setback complies in accordance with the technical requirements of this Standard.</p>		

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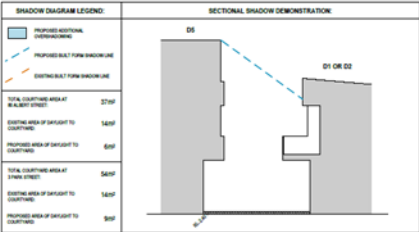
<p>Clause 55.04-5 Overshadowing open space objective</p> <ul style="list-style-type: none"> To ensure buildings do not significantly overshadow existing secluded private open space 	<p>Standard B21 Where sunlight to the SPOS of an existing dwelling is reduced, at least 75%, or 40m² with min. 3m, whichever is the lesser area, of the SPOS should receive a min of 5hrs of sunlight btw 9am & 3pm on 22 September. If existing sunlight to the SPOS of an existing dwelling is less than the requirements of this standard, the amount of sunlight should not be further reduced.</p>	<p>Partly Complies with standard & meets objective</p>
<p>Assessment: Standard B21 requires where sunlight to the secluded private open space of an existing dwelling is reduced, at least 75 per cent, or 40 square metres with minimum dimension of 3 metres, whichever is the lesser area, of the secluded private open space should receive a minimum of five hours of sunlight between 9 am and 3 pm on 22 September. If existing sunlight to the secluded private open space of an existing dwelling is less than the requirements of this standard, the amount of sunlight should not be further reduced. For at least 5 hours between 9am and 3pm, the adjoining SPOS is not further reduced by the proposed development.</p>		
<p>Clause 55.04-6 Overlooking objective</p> <ul style="list-style-type: none"> To limit views into existing secluded private open space and habitable room windows. 	<p>Standard B22 Standard B22 A HRW, balcony, terrace, deck or patio should be located & designed to avoid direct views into the SPOS of an existing dwelling within 9m (refer to clause for exact specifications). Where within it should be either:</p> <ul style="list-style-type: none"> Offset a minimum of 1.5m from the edge of one window to the edge of the other. Have sill heights of at least 1.7m above floor level. Have fixed, obscure glazing in any part of the window below 1.7m above floor level. Have permanently fixed external screens to at least 1.7m above floor level & be no more than 25% transparent. 	<p>Complies with standard & meets objective</p>
	<p>Obscure glazing in any part of the window below 1.7 metres above floor level may be openable provided that there are no direct views as specified in this standard.</p>	<p>Complies with standard & meets objective</p>
	<p>Screens used to obscure a view should be:</p> <ul style="list-style-type: none"> Perforated panels or trellis with a maximum of 25% openings or solid translucent panels. Permanent, fixed and durable. Designed and coloured to blend in with the development. 	<p>N/A</p>
<p>Assessment: The proposed dwellings should not generate any unreasonable overlooking opportunities into any existing habitable room windows or secluded private open spaces areas on the adjoining properties located within a horizontal distance of 9.0 metres. A minimum 2000mm high timber paling fence would be provided along the south-east property boundaries, which is nominated on the elevations but not on the floor plans. .</p>		

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<p>Clause 55.04-7 Internal views objective</p> <ul style="list-style-type: none"> To limit views into the secluded private open space and habitable room windows of dwellings and residential buildings within a development. 	<p>Standard B23 Windows and balconies should be designed to prevent overlooking of more than 50% of the SPOS of a lower-level dwelling or residential building directly below and within the same development.</p>	<p>Complies with standard & meets objective</p>
<p>Assessment: It is unlikely that any internal overlooking would be generated by the proposal.</p>		
<p>Clause 55.04-8 Noise impacts objectives</p> <ul style="list-style-type: none"> To contain noise sources in developments that may affect existing dwellings. To protect residents from external noise. 	<p>Standard B24 Noise sources should not be located near bedrooms of immediately adjacent existing dwellings. Noise sensitive rooms and SPOS of new dwellings and residential buildings should take account of noise sources on immediately adjacent properties. Dwellings and residential buildings close to busy roads, railway lines or industry should be designed to limit noise levels in habitable rooms.</p>	<p>Complies with standard & meets objective</p>
<p>Assessment: Any external heating and/or cooling units associated with the proposed development has been provided.</p>		
<p>Clause 55.05-1 Accessibility objective</p> <ul style="list-style-type: none"> To encourage the consideration of the needs of people with limited mobility in the design of developments. 	<p>Standard B25 The dwelling entries of the ground floor of dwellings and residential buildings should be accessible or able to be easily made accessible to people with limited mobility.</p>	<p>Complies with standard & meets objective</p>
<p>Assessment: The design and layout of the proposed development appears to have considered the needs of persons with limited mobility, with the main living area and bedrooms to be provided at the ground floor level of some of the dwellings.</p>		
<p>Clause 55.05-2 Dwelling entry objective</p> <ul style="list-style-type: none"> To provide each dwelling or residential building with its own sense of identity. 	<p>Standard B26 Entries to dwellings and residential buildings should:</p> <ul style="list-style-type: none"> Be visible and easily identifiable from streets and other public areas. Provide shelter, a sense of personal address and a transitional space around the entry. 	<p>Complies with standard & meets objective</p>
<p>Assessment: The entry area of the new dwellings would be identifiable either from Albert Street (Dwelling 3 and Dwelling 4) or via a shared Drive way (Dwelling 2), or Park Street (Dwelling 1 and Dwelling 5).</p>		
<p>Clause 55.05-3 Daylight to new windows objective</p> <ul style="list-style-type: none"> To allow adequate daylight into new habitable room windows. 	<p>Standard B27 HRW should be located to face:</p> <ul style="list-style-type: none"> Outdoor space clear to the sky or a light court with a minimum area of 3m² and min. dimension of 1m clear to the sky or Verandah provided it is open for at least 1/3 of its perimeter, or A carport provided it has 2 or more open sides and is open for at least 1/3 of its perimeter. 	<p>Complies with standard & meets objective</p>
<p>Assessment: Each new habitable room window would be provided with a minimum 1.0 metre clearance to the sky.</p>		

<p>Clause 55.05-4 Private open space objective</p> <ul style="list-style-type: none"> To provide adequate private open space for the reasonable recreation and service needs of residents. 	<p>Standard B28 A dwelling or residential building should have POS consisting of:</p> <ul style="list-style-type: none"> An area of 40m², with one part of the POS to consist of SPOS at the side or rear of the dwelling or residential building with a min. 25m², a min. dimension of 3m and convenient access from a living room, or A balcony of 8m² with a min. width of 1.6m and convenient access from a living room, or A roof-top area of 10m² with a min. width of 2m and convenient access from a living room. 	<p>Complies with standard & meets objective</p>
<p>Assessment: Each dwelling has been provided with adequate POS that meets the area and dimension requirements specified above and will service the social, recreational and passive needs of future residents as per below proposal:</p> <ul style="list-style-type: none"> Dwelling 1 - 130m² (including 15m² balcony with 2m width) Dwelling 2 - 28m² (including 15m² balcony with 2m width) Dwelling 3 - 78m² (including 10m² balcony with 2.85m width) Dwelling 4 - 82m² (including 14m² balcony with 3.77m width) Dwelling 5 - 83m² (including 14m² balcony) 		
<p>Clause 55.05-5 Solar Access to Open Space</p> <ul style="list-style-type: none"> To allow solar access into the secluded private open space of new dwellings and residential buildings. 	<p>Standard B29 The private open space should be located on the north side of the dwelling or residential building, if appropriate.</p> <p>The southern boundary of secluded private open space should be set back from any wall on the north of the space at least $(2 + 0.9h)$ metres, where 'h' is the height of the wall.</p>	<p>Complies with standard & meets objective</p>
<p>Assessment: Each secluded private open space area would feature direct solar access to the north. It is noted that Dwelling 1 and Dwelling 2 balconies are overshadowed whole day in the shadow diagram even locating at the north side of the proposed development. However, the location of the balconies are at the first floor level and would be overshadowed only at 3pm, which would be only visible from the sectional shadow diagram.</p> <div style="text-align: center;">  </div> <p>3pm shadow diagram</p>		
<p>Clause 55.05-6 Storage objective</p> <ul style="list-style-type: none"> To provide adequate storage facilities for each dwelling. 	<p>Standard B30 Each dwelling should have convenient access to at least 6 cubic metres of externally accessible, secure storage space.</p>	<p>cut & paste applicable wording above</p>
<p>Assessment: A minimum 6m³ storage area is nominated for each dwelling within either the courtyard area (Dwelling 1) or within the car parking area (Dwellings 2-5).</p>		

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<p>Clause 55.06-1 Design Detail objective</p> <ul style="list-style-type: none"> To encourage design detail that respects the existing or preferred neighbourhood character 	<p>Standard B31 The design of buildings, including:</p> <ul style="list-style-type: none"> Facade articulation and detailing Window and door proportions, Roof form, and Verandahs, eaves and parapets, <p>should respect the existing or preferred neighbourhood character. Garages and carports should be visually compatible with the development and the existing or preferred neighbourhood character.</p>	<p>Complies with standard & meets objective</p>
<p>Assessment: The proposed development is consistent with the evolving residential character associated with existing housing stock located on the periphery of the Mordialloc Activity Centre, including the three-storey high medium density development known as “Casa Del Mar” at No. 76 Albert Street. The design and siting of the proposal ensures that the development should not adversely affect the amenity of the only residential property adjoining the subject site. It is noted that the use of varied external building materials associated with each dwelling would be varied and should minimise any visual bulk generated by the proposed three-storey high development.</p>		
<p>Clause 55.06-2 Front fences objective</p> <ul style="list-style-type: none"> To encourage front fence design that respects the existing or preferred neighbourhood character. 	<p>Standard B32 The design of front fences should complement the design of the dwelling or residential building and any front fences on adjoining properties.</p> <p>Schedule to GRZ3: A front fence within 3m of a street should not exceed:2m for streets in a RDZ1 or 1.2m for other streets</p>	<p>Complies with standard & meets objective</p> <p>Complies with standard & meets objective</p>
<p>Assessment: A 1.0 m high front fence is proposed, which is consistent with the trend of front fencing heights in the immediate area and meets the standard to the Zone.</p>		
<p>Clause 55.06-3 Common property objectives</p> <ul style="list-style-type: none"> To ensure that communal open space, car parking, access areas and site facilities are practical, attractive and easily maintained. To avoid future management difficulties in areas of common ownership. 	<p>Standard B33 Developments should clearly delineate public, communal and private areas.</p> <p>Common property, where provided, should be functional and capable of efficient management.</p>	<p>Complies with standard & meets objective</p>
<p>Assessment: Common property is proposed, it is functional, well-designed and capable of efficient management through an owner’s corporation arrangement. No likely future body corporate management difficulties are envisaged with the proposed development.</p>		

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Clause 55.06-4 Site services objectives <ul style="list-style-type: none">• To ensure that site services can be installed and easily maintained.• To ensure that site facilities are accessible, adequate and attractive.	Standard B34 <p>Dwelling layout and design should provide sufficient space and facilities for services to be installed and maintained efficiently and economically.</p> <p>Bin and recycling enclosures, mailboxes and other site facilities should be adequate in size, durable, waterproof and blend in with the development.</p>	Complies with standard & meets objective
Assessment: Site services such as mailboxes and bin/recycling enclosures have been nominated on the respective plans and located appropriately.		

Appendices

Appendix 1 - KP-2017981 - 1 Park Street Mordialloc - considered plans (Ref 18/610467) [↓](#)

Author/s: Girija Shrestha, Senior Statutory Planner
Reviewed and Approved By: Krystal Blizzard, Team Leader City Development
Ilan Nice, Manager City Development

8.3

KP-2017/981 - 1 PARK STREET MORDIALLOC

1	KP-2017981 - 1 Park Street Mordialloc - considered plans.....	97
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PROPOSED MULTI-DWELLING DEVELOPMENT 1 PARK STREET, MORDIALLOC



DEVELOPMENT SUMMARY	
SITE AREA	816 SQ M
SITE COVERAGE	463 SQ M (56%)
PERMITTED AREA	2762 SQ M (33%)
CARVED AREA	307 SQ M (37%)
NUMBER OF DWELLINGS	5
COVERAGE	5
5 LEVELS SHOW TO ALLOW MAX HEIGHT 15.0 METRES	
SUITE TO LANDSCAPE ARCHITECT'S DRAWINGS FOR ALL LANDSCAPING DETAILS	
DWELLING 1:	
SHED FLOOR (GND. GARAGE)	51M ²
FIRST FLOOR (PUL. BALCONY)	51M ²
SECOND FLOOR	118M ² (250%)
TOTAL	210M ²
PRIVATE OPENSPACE	110M ²
OCCLUDED PRIVATE OPENSPACE	31M ²
DWELLING 2:	
SHED FLOOR (GND. GARAGE)	11M ²
FIRST FLOOR (PUL. BALCONY)	11M ²
TOTAL	22M ² (50%)
PRIVATE OPENSPACE	11M ²
OCCLUDED PRIVATE OPENSPACE	11M ²
DWELLING 3:	
SHED FLOOR (GND. GARAGE)	11M ²
FIRST FLOOR (PUL. BALCONY)	11M ²
TOTAL	22M ² (50%)
PRIVATE OPENSPACE	11M ²
OCCLUDED PRIVATE OPENSPACE	11M ²
DWELLING 4:	
SHED FLOOR (GND. GARAGE)	11M ²
FIRST FLOOR	11M ²
SECOND FLOOR	11M ²
TOTAL	33M ² (75%)
PRIVATE OPENSPACE	11M ²
OCCLUDED PRIVATE OPENSPACE	11M ²
DWELLING 5:	
SHED FLOOR (GND. GARAGE)	5M ²
FIRST FLOOR	5M ²
SECOND FLOOR	5M ²
TOTAL	15M ² (35%)
PRIVATE OPENSPACE	11M ²
OCCLUDED PRIVATE OPENSPACE	11M ²
 DRIVING TEE TO BE CONSIDERED TO THE DISCRETION OF THE RESPONSIBLE AUTHORITY	
 DRIVING TEE TO BE CONSIDERED	
PEDESTRIAN VISIBILITY DISPLAY: ANY STRUCTURES OR OBJECTS WITHIN THE TRIANGLE MUST BE 1.5 METRES HIGH AT ALL TIMES	
ALL SHEET DIMENSIONS REFER TO THE CENTER LINE UNLESS OTHERWISE SPECIFIED	

DATE/REVISION	BY	DATE	REASON
01/10/2018	AS	01/10/2018	A
01/10/2018	AS	01/10/2018	B
01/10/2018	AS	01/10/2018	C

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Note:
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PROJECT NO.	17322
DATE	OCTOBER 2018
REVISION	CV1.0

PROJECT NO.	17322
DATE	OCTOBER 2018
REVISION	CV1.0

COVER SHEET

PROJECT NO.	17322
DATE	OCTOBER 2018
REVISION	CV1.0

PROJECT NO.	17322
DATE	OCTOBER 2018
REVISION	CV1.0



Job No: 22715
 DWG: 2271511M1D
 Date: 28/09/17
 Sheet: 1 OF 1

Client: BURTON OWLSETH Mortdallocc KINGSFORD

Neighbourhood & Site Description Plan For
No.1 (Lots 1& 2) Park Street, Mortdallocc

Scale: 1:250 @ A1

NOTES

LOCATION WELL SERVED BY RETAIL, SCHOOL, PUBLIC TRANSPORT, COMMUNITY OPEN SPACE PARK LAND AND SPORTS FACILITIES RESERVE. RESERVE - BICYCLE PATH PLAY EQUIPMENT OPEN PARKLAND 200m TO MAIN STREET SHOPPING STRIP - FULL RANGE OF RETAIL OUTLETS & SERVICES 200m TO MORTDALLOCC TRANSPORTATION.

NEIGHBOURHOOD SUBSTANTIALLY NORTH SOUTH EAST AND WEST GARDEN WITH SOME DIAGONAL DISTORTION. GENTLE TO FLAT LAND FORM. NO VARIATION IN LEVELS BETWEEN SITE AND ADJACENT PROPERTIES. VARIOUS LOT SIZES AND PROFILES TYPICALLY 70m² MINIMUM DWELLING SETBACK 3.4m.

STREET SCOPE 20.0m ROAD RESERVATION. 1.3m WIDE BRIMMUM STREET SIDEWALK. 1.5m WIDE SIDEWALK. 3.4m WIDE SIDEWALK. STREET SCAPE LAURE STREET THREE PLANTINGS IN BARK STREET. MANUKAM PARK SHOWN ON STREET PLANNING ON A OFF THE STREET.

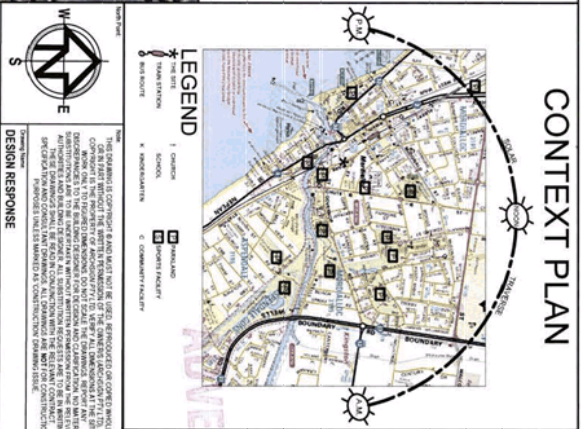
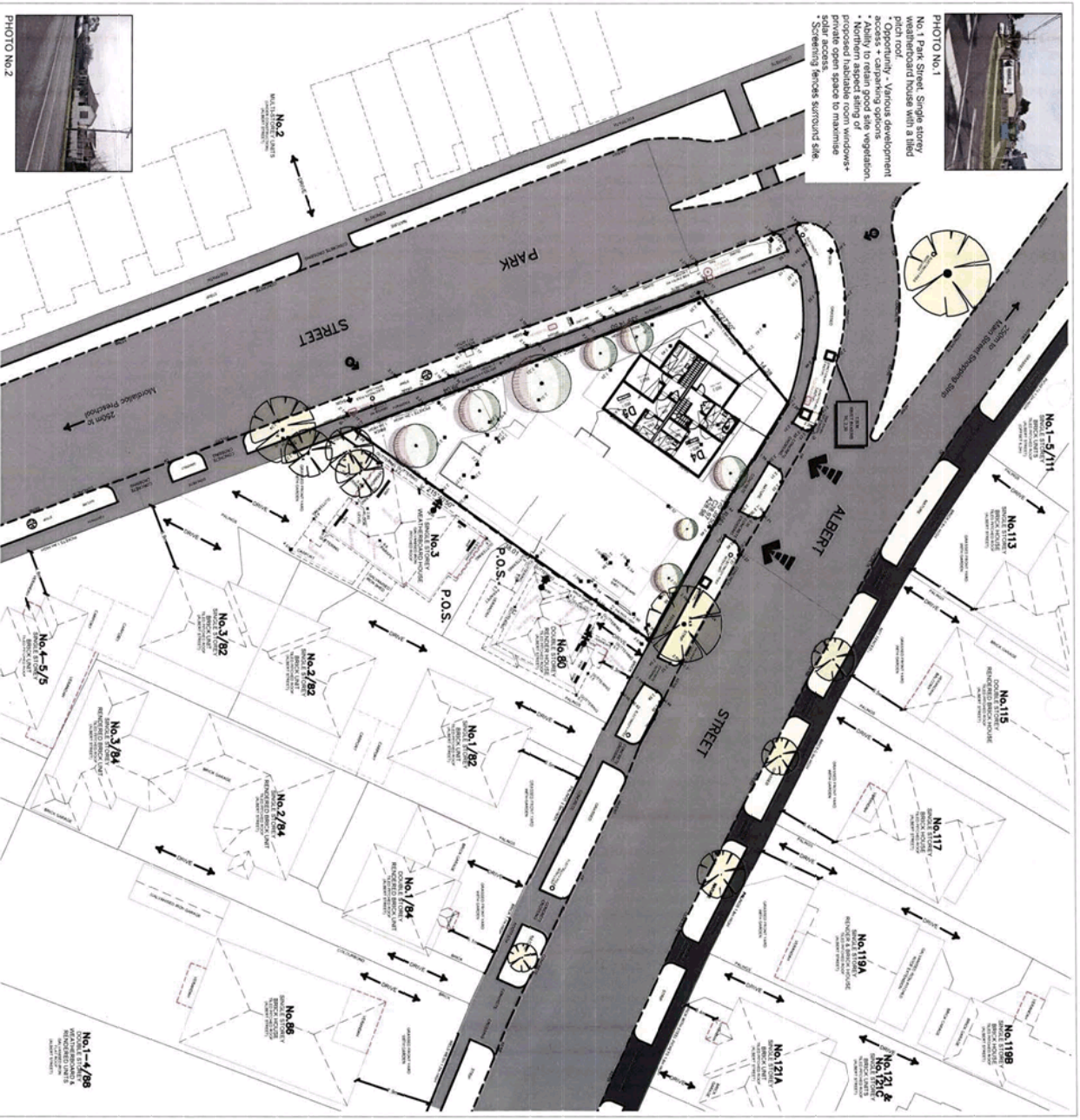
STREET INTERFACE. VARIOUS INCENTIVES TO STREET INCLUDING LOW BRICK POCKETS + LOW KEY LIMITED PLANTING. SOME CLUMP TREES. TYPICALLY BRICK MATERIAL. CONCRETE ROOF TILES OF LOW PITCH. HIP ROOFS. TILES LOW PITCHES - SPARSE PLANTING - HOUSES VISIBLE FROM STREET.

VARIOUS ARCHITECTURAL STYLES. TYPICAL SINGLE STOREY RESIDENTIAL CONSTRUCTION. LIGHT FARTH TONES. GARAGES CAR PORTS TYPICALLY USE. BRICK MATERIAL. CONCRETE ROOF TILES OF LOW PITCH. HIP ROOFS. TILES LOW PITCHES - SPARSE PLANTING - HOUSES VISIBLE FROM STREET.

FIELD MARKS

FEATURES SHOWN WITHIN THE SUBJECT SITE ACCORD WITH SUBJECT PLAN REF: 22715/11/DWG. SURROUNDING FEATURES HAVE BEEN LOCATED FOR PLANNING PURPOSES ONLY AND SHOULD NOT BE USED FOR DESIGN.

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 THE SUBDIVISION SPECIALISTS
 Surveying Engineering Town Planning
 Suite 8, 200 Moorabbin Highway, Ringwood VIC, Australia 3104
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DISCLAIMER
FEATURES SHOWN WITHIN THE SUBJECT SITE ACCORD WITH THE CITY OF MORTIALLOC Z771511F-D0.WG SURROUNDING FEATURES PLANNING PURPOSES ONLY AND SHOULD NOT BE USED FOR DESIGN.

Project No.	17322
Scale	AT 1:250
Issue	NOVEMBER 2017
Author	BN
Checker	AD
Version	DR1.0
Category	A

TOWN PLANNING

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PROPOSED MULTI-DEVELOPMENT AT 1 PARK STREET MORTIALLOC



WATER SENSITIVE URBAN DESIGN (WSUD) NOTES:
 THIS SUD PLAN IS PROVIDED TO COMPLY WITH ALL REQUIREMENTS AS SET OUT, AND IN THE EVENT THAT A 3.5 STAR RATING IS NOT ACHIEVED:

PROJECT NUMBER: 17322
PROJECT NAME: 1 PARK ST

NATHERS STAR RATING:
6.0 STARS

HEATING & COOLING SYSTEMS:
 DWELLING 1: AIR-CONDITIONING SYSTEM: REFRIGERANT: R410A
 DWELLING 2: AIR-CONDITIONING SYSTEM: REFRIGERANT: R410A

HOT WATER SYSTEMS:
 HOT WATER SYSTEMS: ENERGY RATING: ENERGY RATING: ENERGY RATING: ENERGY RATING

WATER FITTINGS/PICURES:
 DWELLING 1: SHOWER HEADS: 2-2.0L PER MINUTE (2) SHOWER HEADS: 2-2.0L PER MINUTE (2) TOILETS: 2-6.0L PER FLUSH (2) TOILETS: 2-6.0L PER FLUSH (2) SINKS: 2-1.5L PER USE (2) SINKS: 2-1.5L PER USE (2) BATHS: 2-1.5L PER USE (2) BATHS: 2-1.5L PER USE (2) WASH BASINS: 2-1.5L PER USE (2) WASH BASINS: 2-1.5L PER USE (2)

ADJUSTABLE SHADING:
 ADJUSTABLE EXTERNAL COVERING DEVICES (DROP DOWN AWNINGS) ARE PROVIDED TO ALL BALCONY AND TERRACE AREAS.

DOUBLE GLAZING:
 DOUBLE GLAZING IS PROVIDED TO ALL WINDOWS AND DOORWAYS.

BICYCLE PARKING:
 BICYCLE PARKING IS PROVIDED TO ALL DWELLING UNITS.

TOTAL HARD SURFACE AREA CONNECTED TO RAIN GARDENS:
 TOTAL HARD SURFACE AREA CONNECTED TO RAIN GARDENS: 150.00 M²

RAINWATER TANK SIZES:

UNIT	BEHIND WALL	AHEAD WALL
UNIT 1	200L	200L
UNIT 2	200L	200L
UNIT 3	200L	200L
UNIT 4	200L	200L

TOTAL NUMBER OF TOILETS CONNECTED BACK TO RAINWATER TANKS:

UNIT	BEHIND WALL	AHEAD WALL
UNIT 1	1	1
UNIT 2	1	1
UNIT 3	1	1
UNIT 4	1	1

TOTAL ROOF CATCHMENT AREA CONNECTED TO COMBINE UNDERGROUND RAINWATER TANK:
 TOTAL ROOF CATCHMENT AREA CONNECTED TO COMBINE UNDERGROUND RAINWATER TANK: 120.00 M²

TOTAL ROOF CATCHMENT AREA CONNECTED TO ABOVE-GROUND RAINWATER TANKS:
 TOTAL ROOF CATCHMENT AREA CONNECTED TO ABOVE-GROUND RAINWATER TANKS: 150.00 M²

DEVELOPMENT SUMMARY

SITE AREA	19 953 M ²
SITE COVERAGE	46.70 M ² (2.34%)
PERMITTED AREA	1700 M ² (8.52%)
GARDEN AREA	331.50 M ² (1.66%)
NUMBER OF DWELLINGS	4
CONTRACTOR	1
DESIGNER	1
DATE	04/10/2018

HEATING & COOLING SYSTEMS:

DWELLING	TYPE	ENERGY RATING
DWELLING 1	RESIDENTIAL	4.5
DWELLING 2	RESIDENTIAL	4.5
DWELLING 3	RESIDENTIAL	4.5
DWELLING 4	RESIDENTIAL	4.5

HOT WATER SYSTEMS:

DWELLING	TYPE	ENERGY RATING
DWELLING 1	RESIDENTIAL	4.5
DWELLING 2	RESIDENTIAL	4.5
DWELLING 3	RESIDENTIAL	4.5
DWELLING 4	RESIDENTIAL	4.5

WATER FITTINGS/PICURES:

DWELLING	TYPE	ENERGY RATING
DWELLING 1	RESIDENTIAL	4.5
DWELLING 2	RESIDENTIAL	4.5
DWELLING 3	RESIDENTIAL	4.5
DWELLING 4	RESIDENTIAL	4.5

ADJUSTABLE SHADING:

DWELLING	TYPE	ENERGY RATING
DWELLING 1	RESIDENTIAL	4.5
DWELLING 2	RESIDENTIAL	4.5
DWELLING 3	RESIDENTIAL	4.5
DWELLING 4	RESIDENTIAL	4.5

DOUBLE GLAZING:

DWELLING	TYPE	ENERGY RATING
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DWELLING 3	RESIDENTIAL	4.5
DWELLING 4	RESIDENTIAL	4.5

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DWELLING 3	RESIDENTIAL	4.5
DWELLING 4	RESIDENTIAL	4.5

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DWELLING	TYPE	ENERGY RATING
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DWELLING 3	RESIDENTIAL	4.5
DWELLING 4	RESIDENTIAL	4.5

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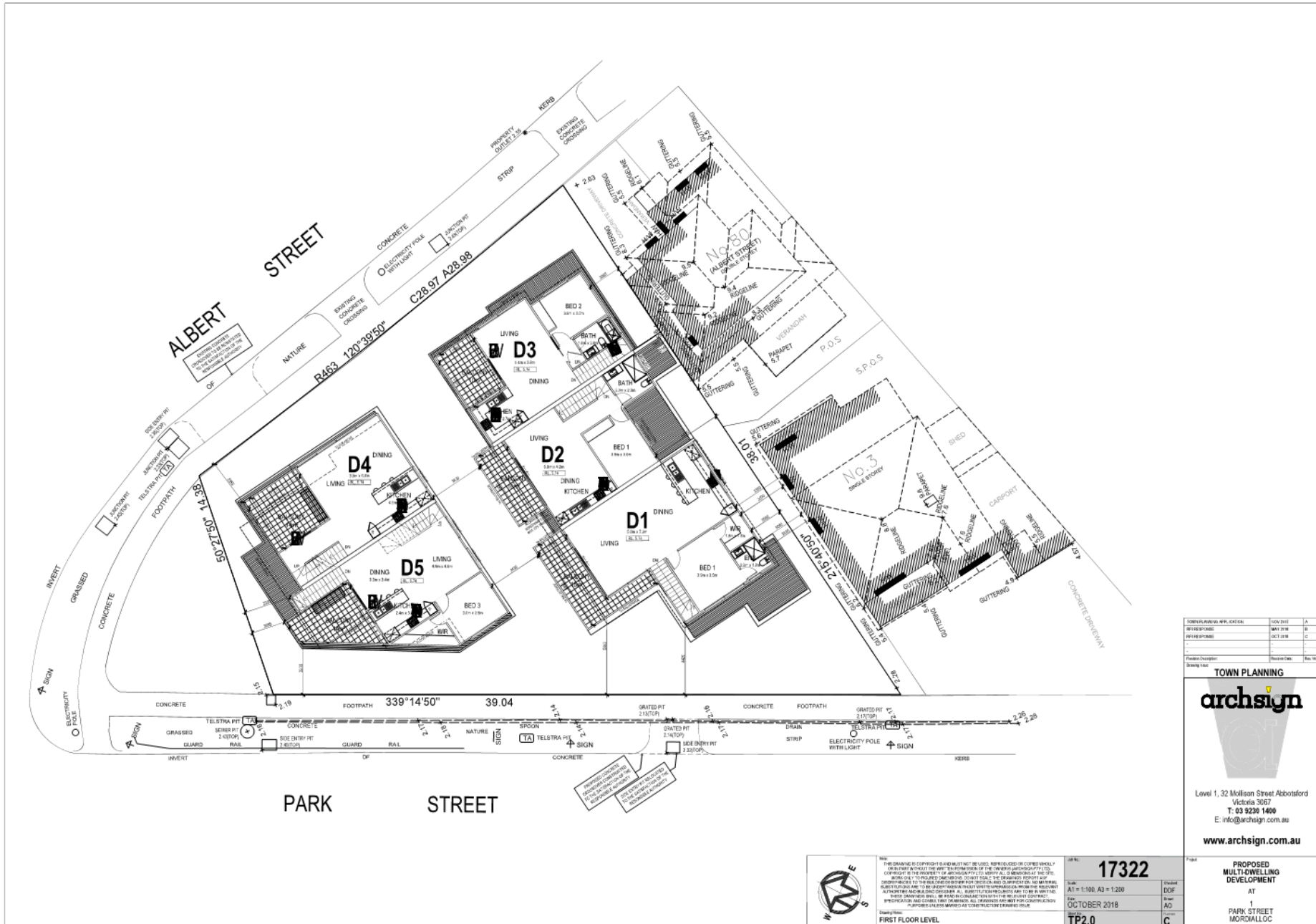
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DWELLING 3	RESIDENTIAL	4.5
DWELLING 4	RESIDENTIAL	4.5

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17322

PROPOSED MULTILEVEL DEVELOPMENT

AT
 1
 PARK STREET
 MORTDALLO



TOWN PLANNING REVIEW DATE:	16/01/2012	A
BY RESPONSE:	MAY 2018	B
BY RESPONSE:	OCT 2018	C
Project Description:		
Drawing Title:		

TOWN PLANNING

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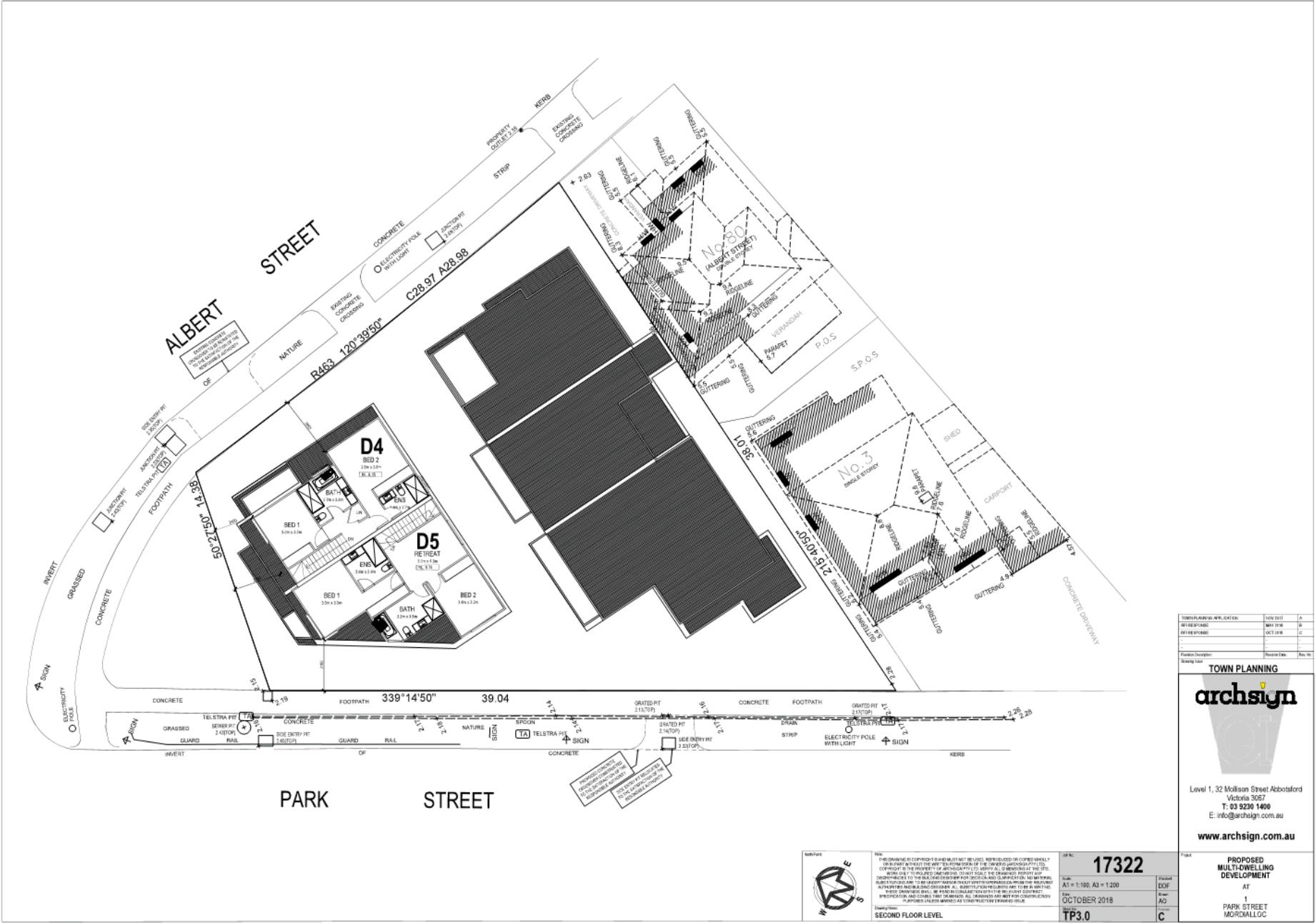
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17322	Date
Scale: A1 = 1:500, A3 = 1:200	Created
DATE: 10 OCT 2018	By: DOF
Client: TP2.0	No: AC
	C

PROPOSED MULTIDWELLING DEVELOPMENT
AT
1
PARK STREET
MORDIALLOC



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REVISED	MAY 2016	B
REVISED	OCT 2018	C
Project description: Reserve Change		
Working code:	Reserve Change	Rev. No.

TOWN PLANNING

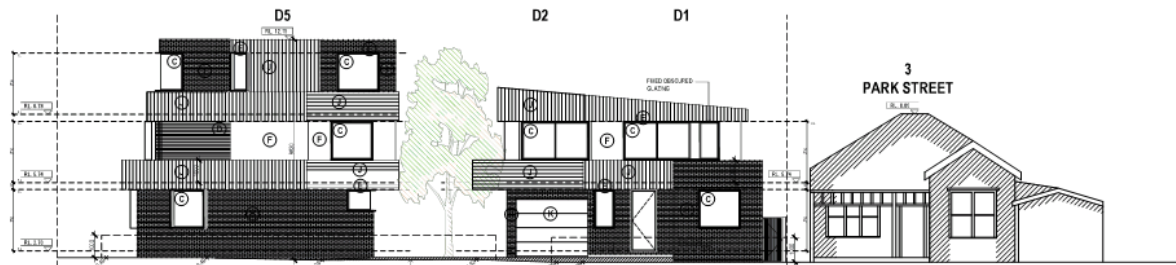
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Project No:	17322	Phase:	PROPOSED MULTI-DWELLING DEVELOPMENT
Scale:	A1 = 1:500, A2 = 1:200	Client:	EQP
Date:	OCTOBER 2018	Drawn:	AD
Group Name:	SECOND FLOOR LEVEL	Check:	C



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SCALE: 1:100

SOUTH-WEST FENCE ELEVATION (PARK STREET)

SCALE: 1:100

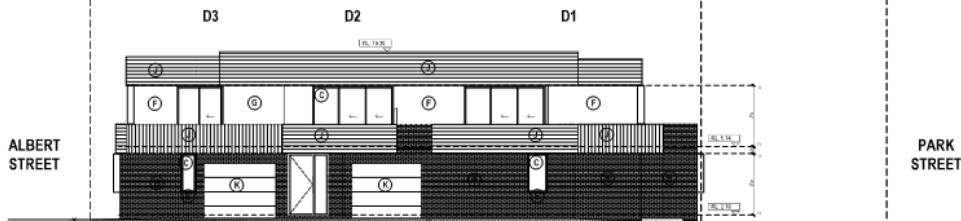


NORTH-WEST ELEVATION

SCALE: 1:100

NORTH-WEST FENCE ELEVATION

SCALE: 1:100



INTERNAL NORTH-WEST ELEVATION

SCALE: 1:100

EXTERIOR COLOUR & FINISHES SCHEDULE

REF.	ITEM	COLOUR	SUPPLIER / FINISHER
A	COPLANED ROOF SHEETING	SURFMENT	COLOURBOND
B	GUTTERS & FLASHING	NONALUMIT	COLOURBOND
C	POWDERCOAT FINISH TO ALUMINIUM WINDOW & SLIDING DOOR FRAMES	029 COLOURBOND MONUMENT	COLOURBOND
D	POWDERCOAT FINISH TO ALUMINIUM SLATS	029 COLOURBOND MONUMENT	COLOURBOND
E	POWDERCOAT FINISH TO ALUMINIUM WINDOW DETAILS	029 COLOURBOND MONUMENT	COLOURBOND
F	APR. 60 THERMOFRONT	029 COLOURBOND MONUMENT	COLOURBOND
G	APR. 60 THERMOFRONT	WALSHBURY SP	SOLID
H	FACE BRICKWORK	MFC 3A PALM LAMANGE	AUSTAL
I	BRICK FINISH	MFC 3A PALM LAMANGE	AUSTAL
J	NATURAL WOOD CLADDING EXPRESSION (CONCRETE FINISH)	PACIFIC WOOD	WOODFORMARCHITECTURAL
K	PAVE LIFT GARAGE DOOR	NONALUMIT	COLOURBOND

TOWN PLANNING APPLICATION	10/19/2017	A
9/11 RESPONSE	MAY 2018	B
9/11 RESPONSE	OCT 2018	C
9/11 RESPONSE		
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17322

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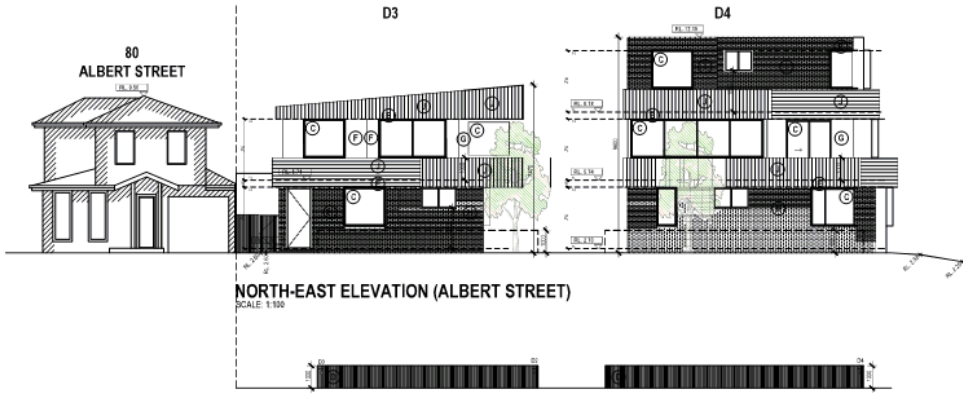
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Drawn: AD

Checked: C

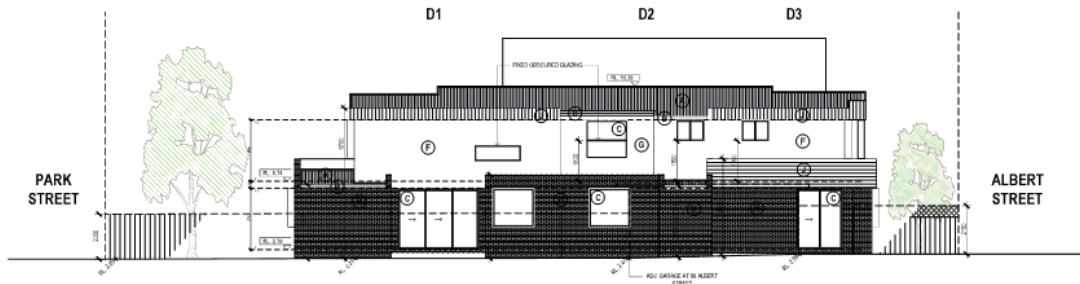
PROPOSED MULTIDWELLING DEVELOPMENT AT 1 PARK STREET MORDALLOC

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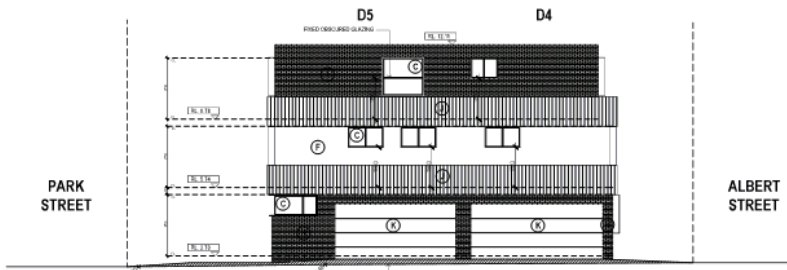


NORTH-EAST ELEVATION (ALBERT STREET)
SCALE: 1:100

NORTH-EAST FENCE ELEVATION (ALBERT STREET)
SCALE: 1:100



SOUTH-EAST ELEVATION
SCALE: 1:100



INTERNAL SOUTH-EAST ELEVATION
SCALE: 1:100

REF.	ITEM	COLOUR	SUPPLIER / FINISHES
A	CORRUGATED ROOF SHEETING	SILVERMET	COLORBOND
B	SUPPLY SIDE FLASH	POWDER COAT FINISH TO RALUMALUMINUM	COLORBOND
C	3.8mm COLOUR FRAME	COLOUR COORDINATED FINISH	COLORBOND
D	POWDER COAT FINISH TO RALUMALUMINUM	COLOUR COORDINATED FINISH	COLORBOND
E	POWDER COAT FINISH TO RALUMALUMINUM	COLOUR COORDINATED FINISH	COLORBOND
F	APPLIED POLYURETHANE FINISH	COLOUR COORDINATED FINISH	COLORBOND
G	APPLIED POLYURETHANE FINISH	COLOUR COORDINATED FINISH	COLORBOND
H	FACE BRICKWORK	WINDLEDALE BRICKWORK	BRICKLAB
I	BRICK FACINGS	WINDLEDALE BRICKWORK	BRICKLAB
J	NATURAL TREATED GUMWOOD EXTERIOR WALLS	FACED BRICK	WOODVIEW ARCHITECTURAL
K	FINISH LIFT GARAGE DOOR	SILVERMET	COLORBOND

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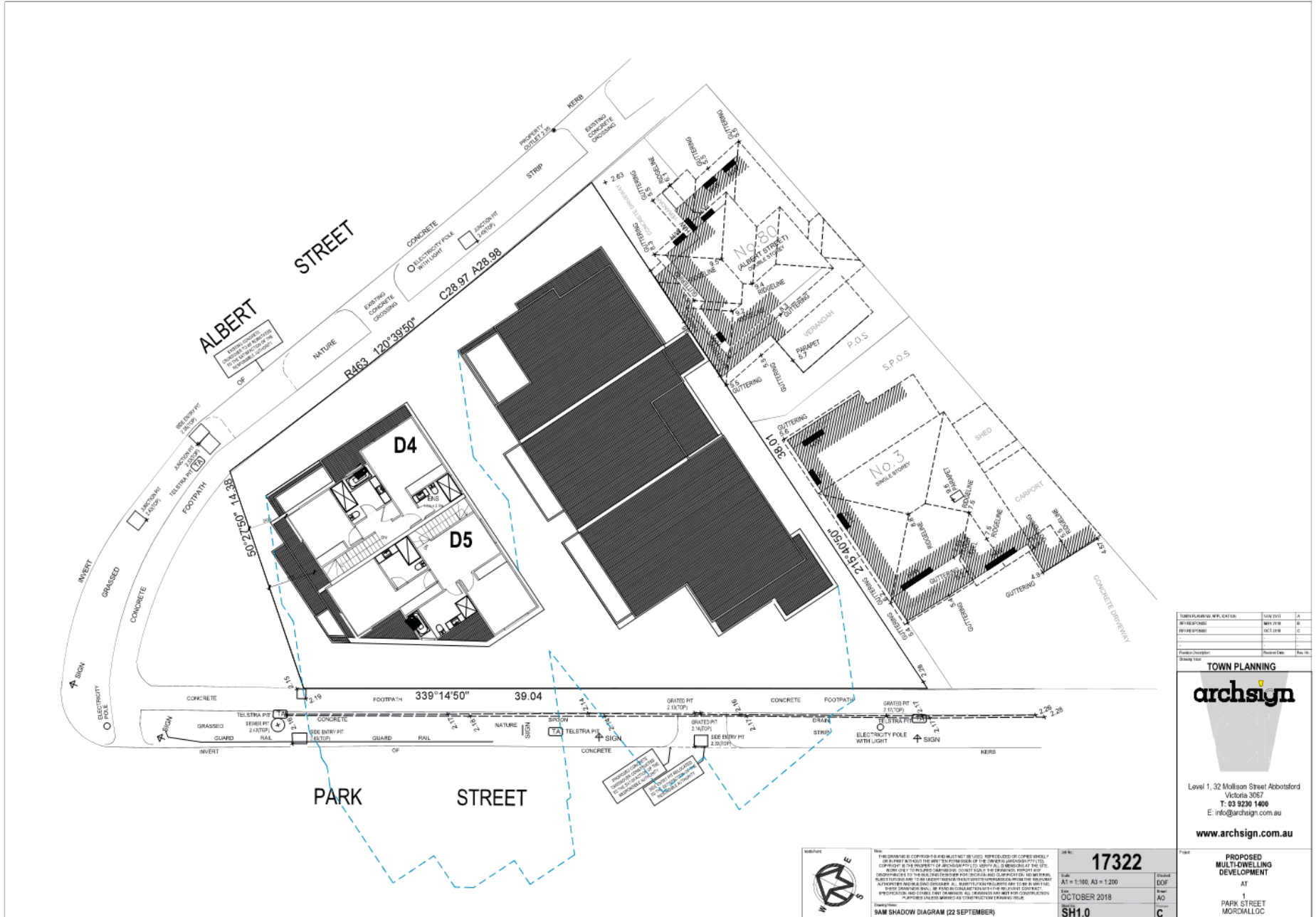
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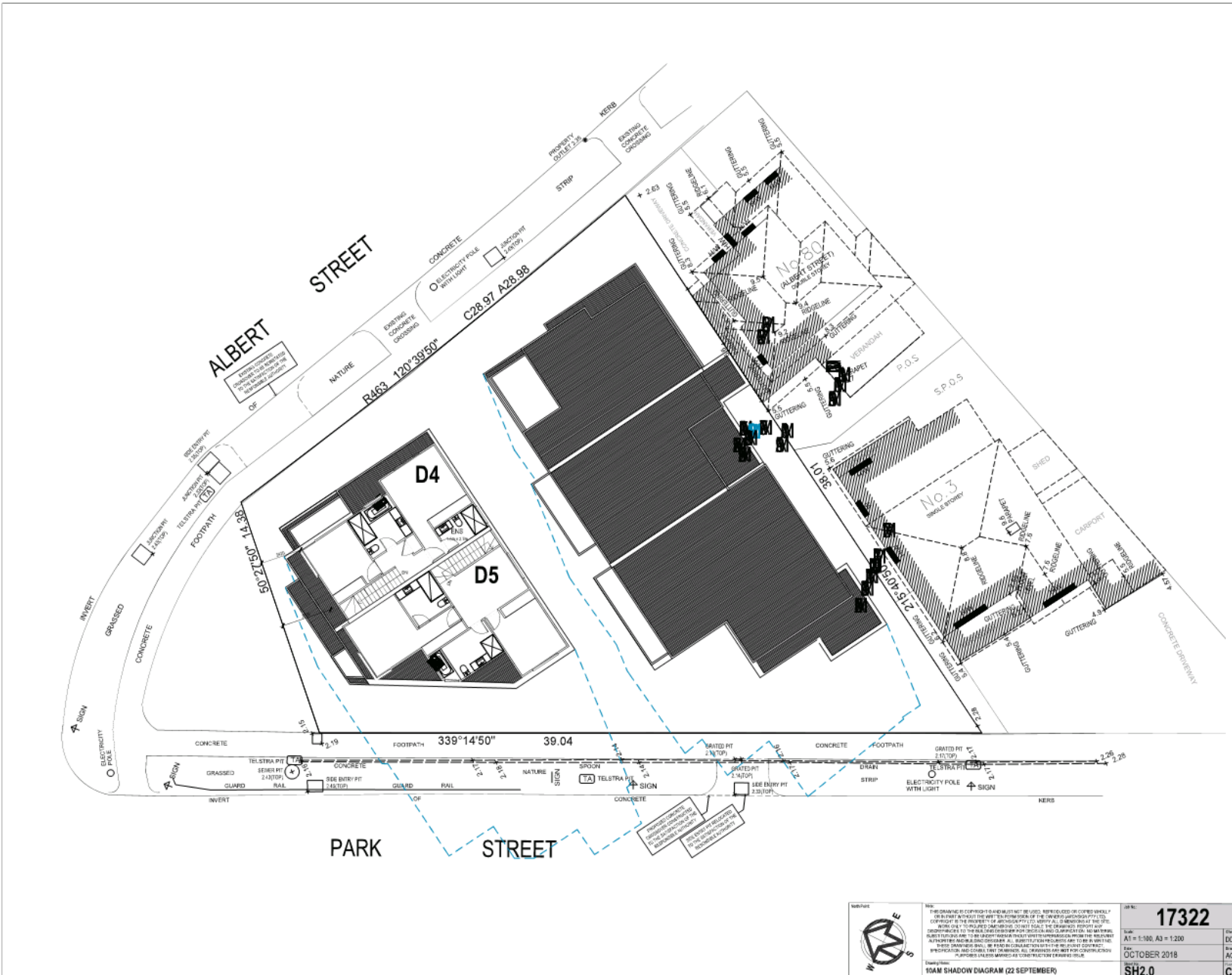
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PROPOSED
MULTI-DWELLING
DEVELOPMENT
AT
1
PARK STREET
MORDALLO





REVISED	MAY 2018	01
PREPARED	OCT 2018	01

Revision description: Revision Date Rev. No.

Working Issue

TOWN PLANNING

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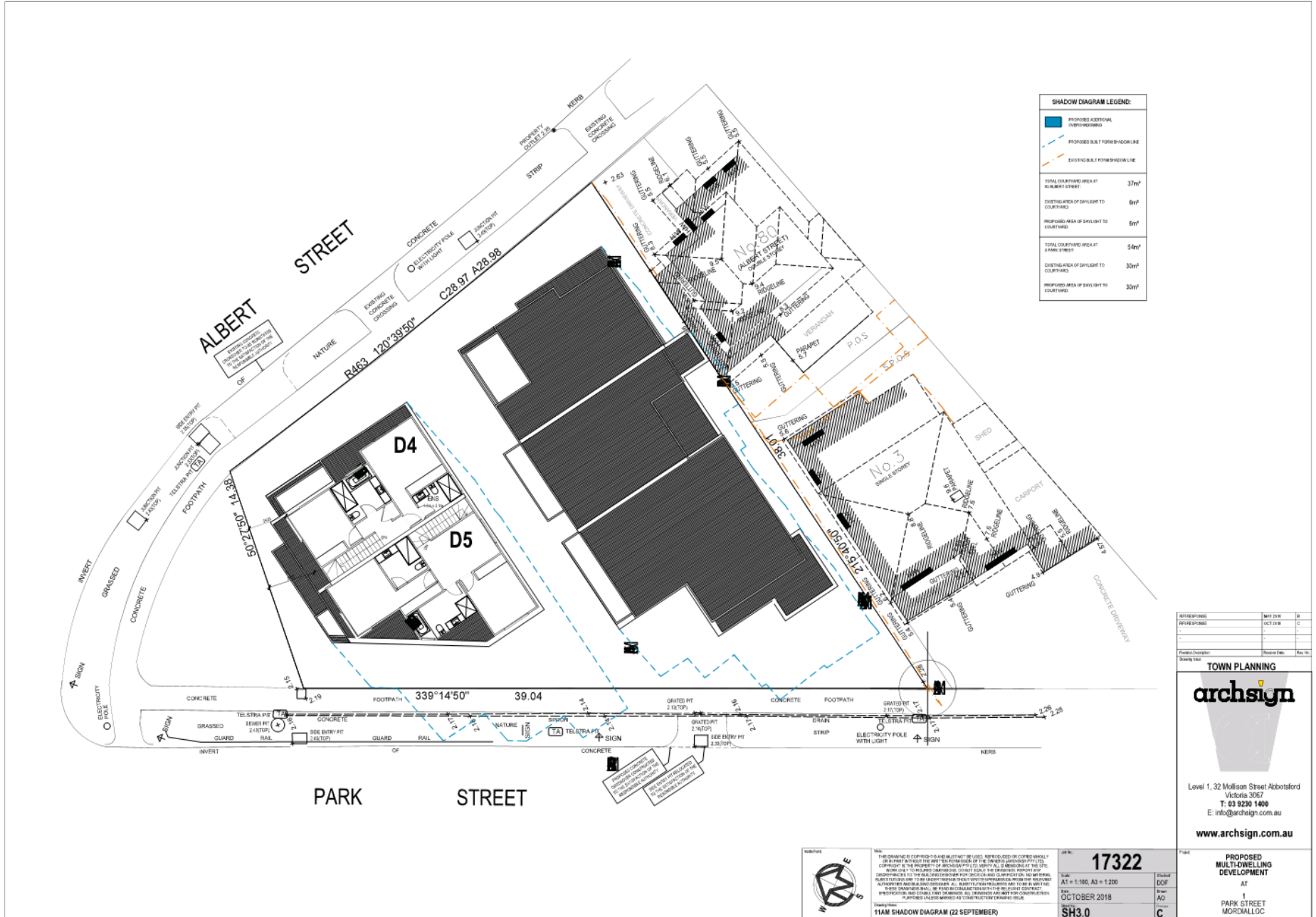
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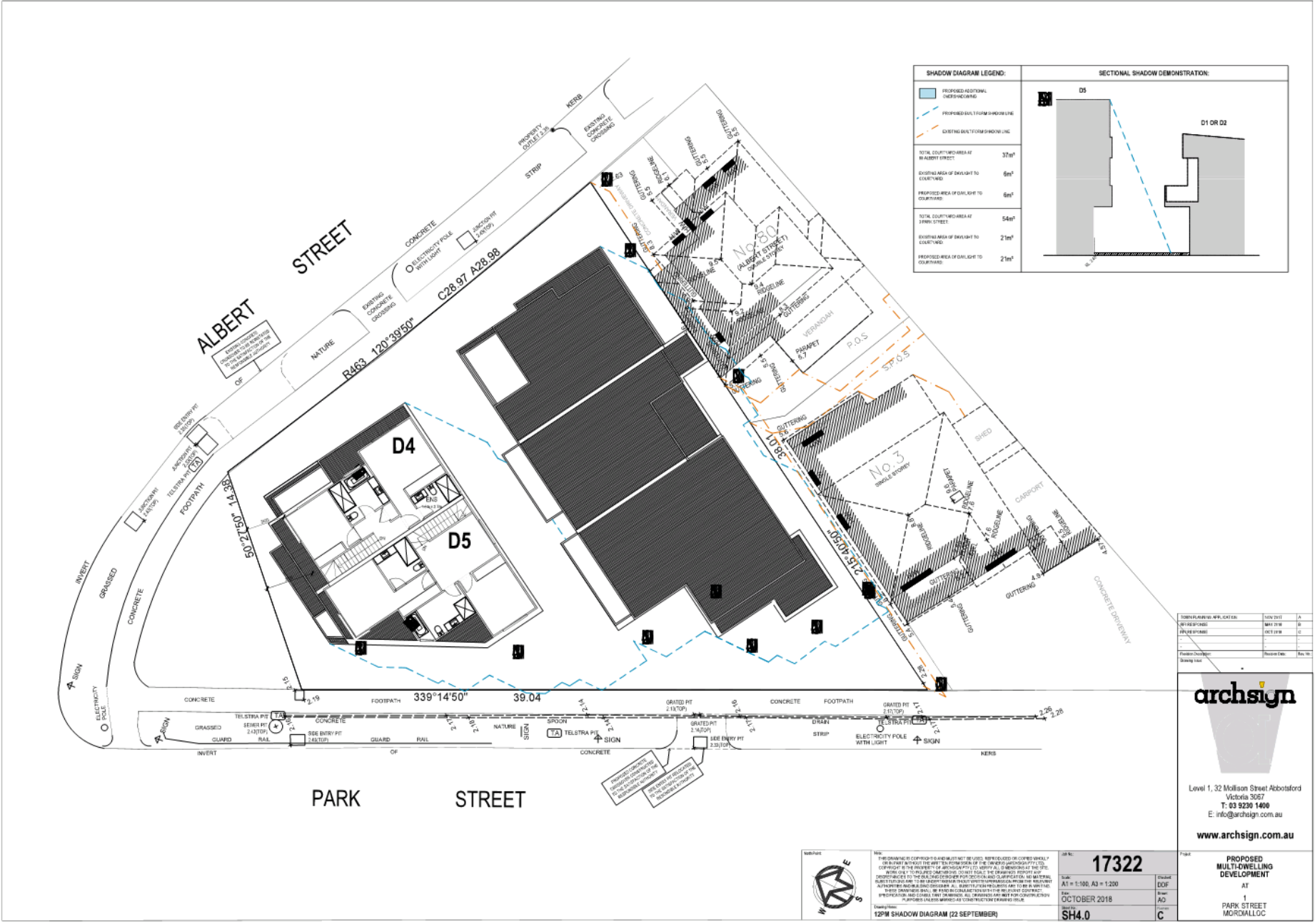
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Date	OCTOBER 2018	Revised	AG
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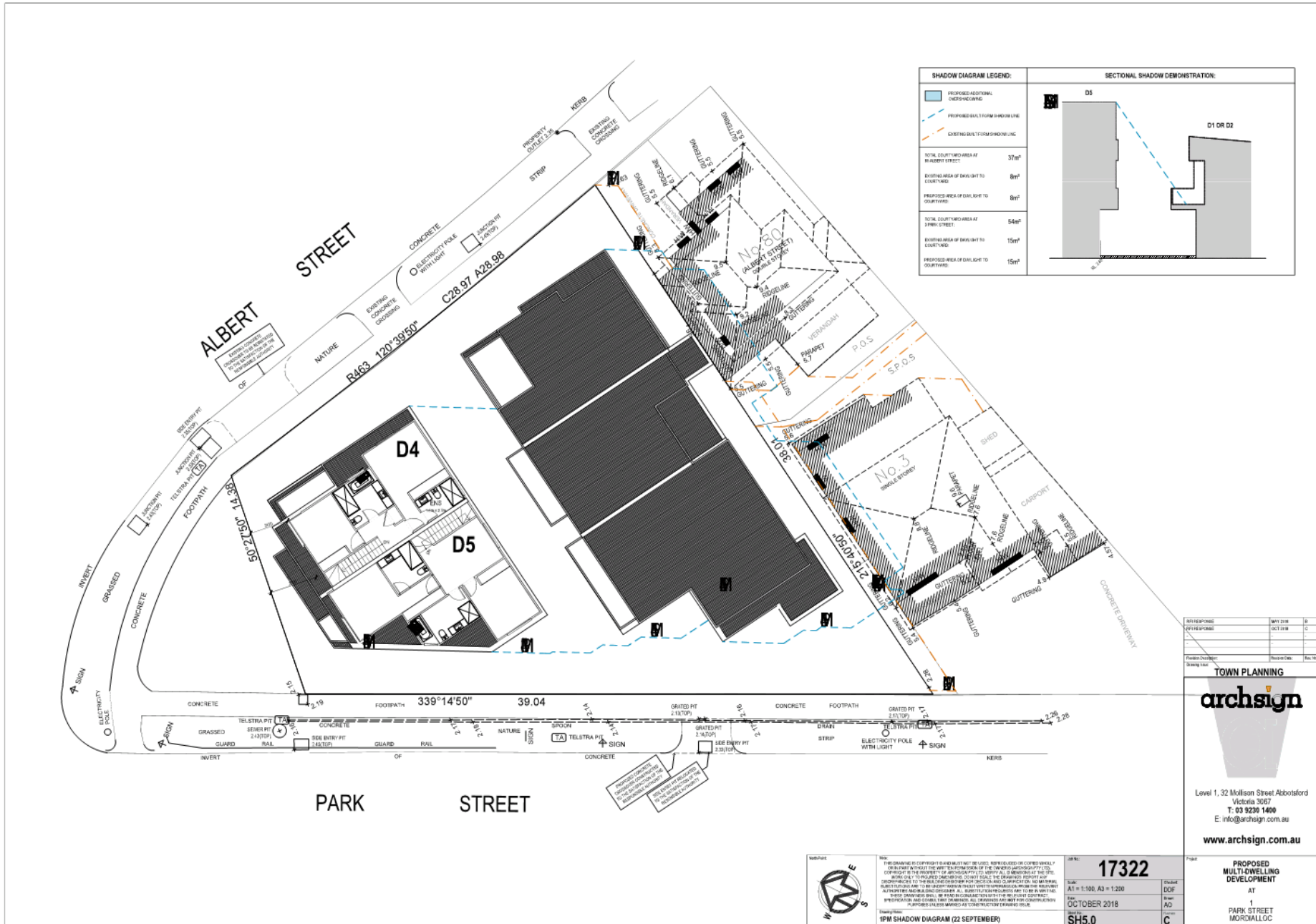
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Drawn by: **YOUNG SHADOW DIAGRAM (22 SEPTEMBER)**







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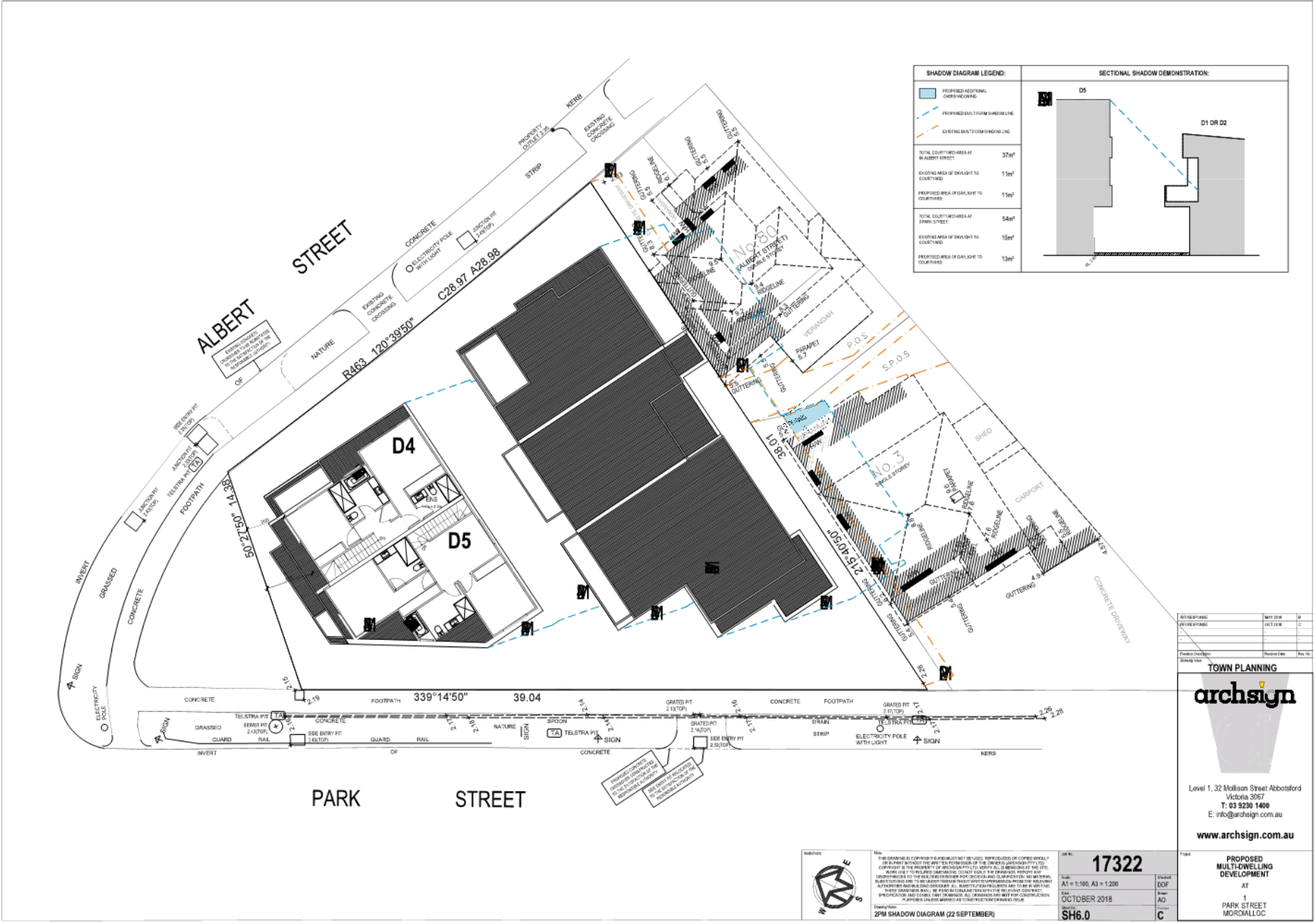
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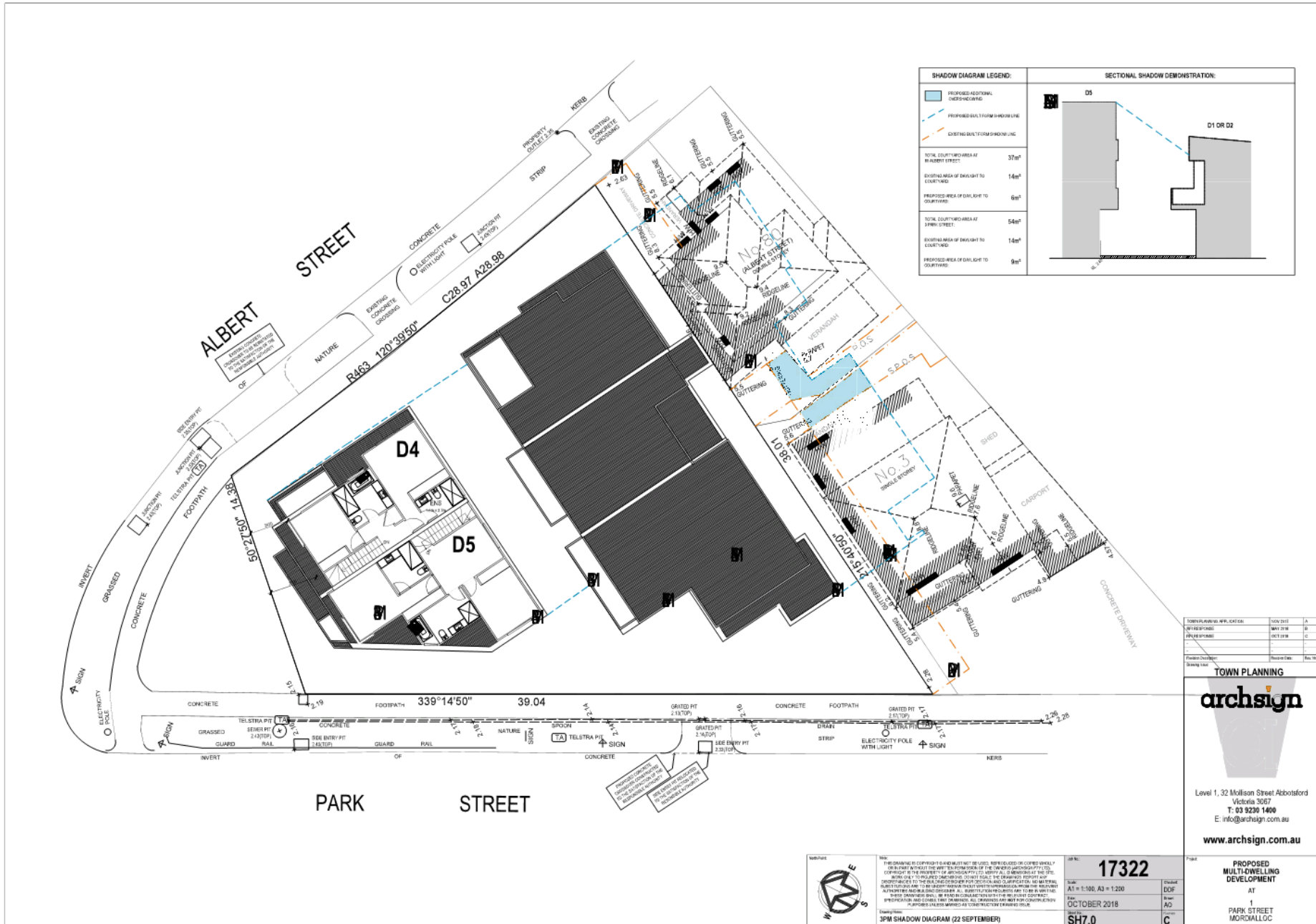
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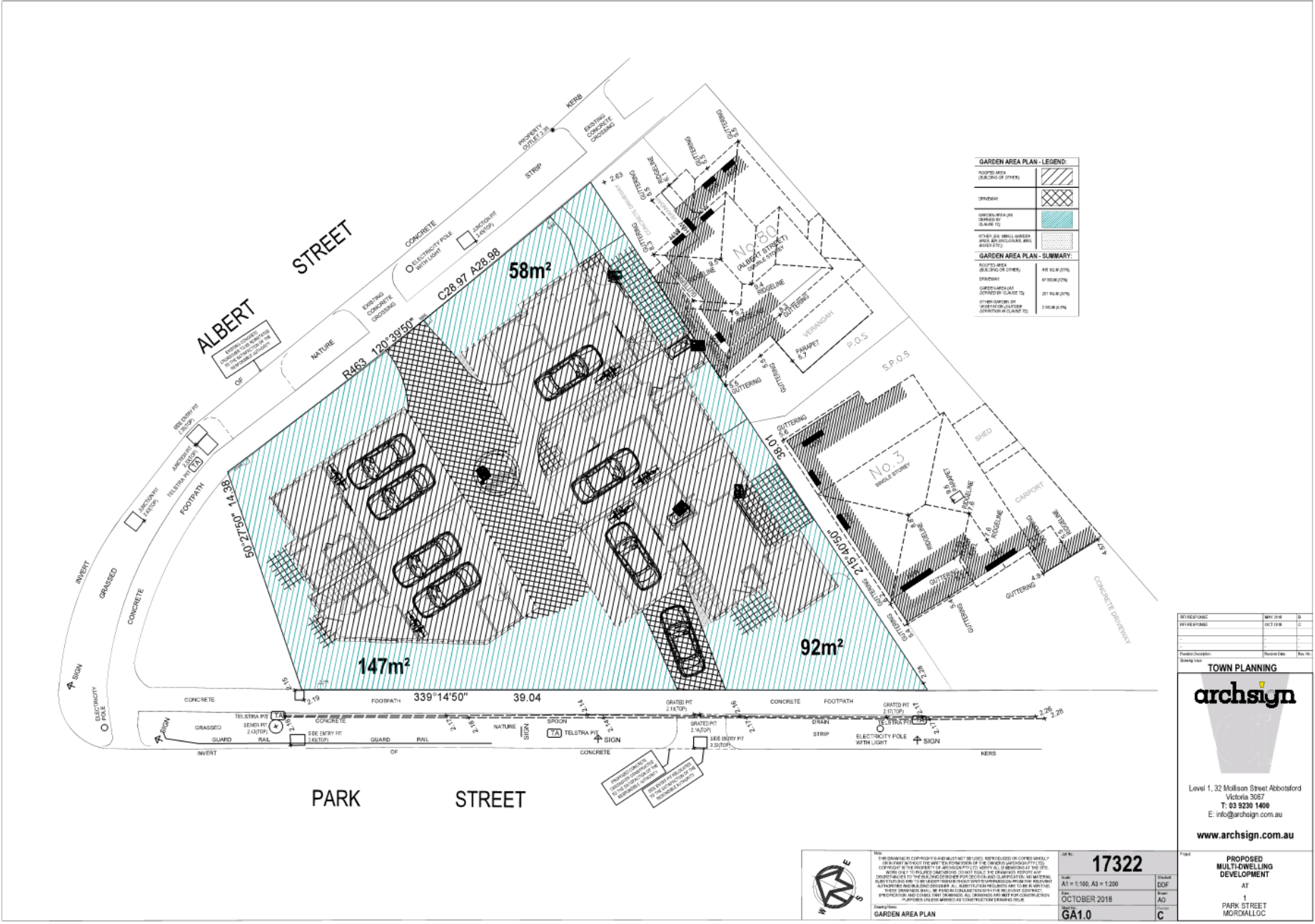
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Scale: A1 = 1:100, A3 = 1:200
 Date: OCTOBER 2018
 Sheet: SH.5.0

Project: PROPOSED MULTIDWELLING DEVELOPMENT
 AT
 1 PARK STREET MORDALLOIC







Job Address: 1 Park Street, Mordialloc
Multi-Unit Development

Job No. 17322

COLOUR SCHEDULE (REV C)

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A. Corrugated Roof Sheet – Surfmist, *Colorbond*



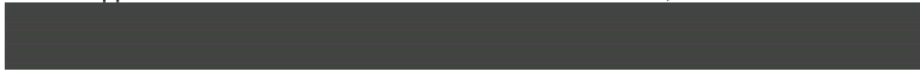
B. Gutters & Fascia – Monument, *Colorbond*

C. Powdercoat finish to Aluminium Window & Sliding Door Frames –

D. Powdercoat finish to Aluminium Slats–

E. Powdercoat finish to Aluminium Window Shrouds–

F. Applied Render Finish 1 – C29 Colorbond Monument, *Dulux*

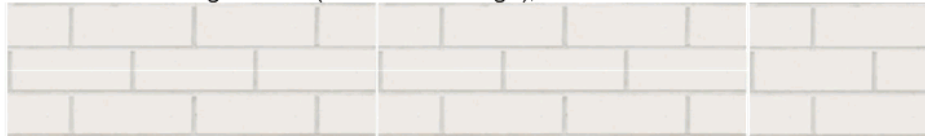


G. Applied Render Finish 2 – Vivid White 1W, *Dulux*



H. Face Brickwork –

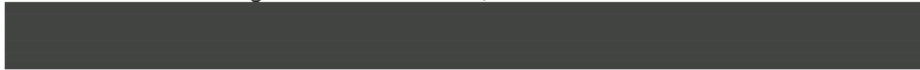
I. Brick Facings – Miro (La Paloma Range), *Austral Bricks*



J. Natural Timber Cladding (Expression Sorrento Profile) – Pacific Teak, *Woodform Architectural*



K. Panel Lift Garage Door – Monument, *Colorbond*



Ordinary Meeting of Council

10 December 2018

Agenda Item No: 8.4

AMENDMENT C149 - ANOMALIES

Contact Officer: Sarah Capenerhurst, Principal Strategic Planner

Purpose of Report

This report provides an update to the Council on the outcomes of the exhibition of Planning Scheme Amendment C149. The report recommends that Council adopt Amendment C149 and submit it to the Minister for Planning for approval.

Disclosure of Officer / Contractor Direct or Indirect Interest

No Council officer/s and/or Contractor/s who have provided advice in relation to this report have declared a Conflict of Interest regarding the matter under consideration.

OFFICER RECOMMENDATION

That Council:

1. Adopt Amendment C149 to the Kingston Planning Scheme with the following changes:
 - 1.1 Removal of 18 Council owned properties that were proposed to be rezoned to Public Use Zone and Public Park and Recreation Zone for further review.
 - 1.2 Removal of 2 privately owned sites at 22-36 Bulli Street, Moorabbin and 96 – 100 Gladesville Boulevard, Patterson Lakes for further review.
 - 1.3 Removal of 2-8 Balcombe Road, Mentone at the request of VicRoads.
 - 1.4 Removal of 5 Long Island Point, Patterson Lakes at the request of Melbourne Water.
2. Submit Amendment C149 to the Minister for Planning for approval.

1. Executive Summary

Amendment C149 proposes to make various corrections to the mapping of zones and overlays to resolve anomalies in the Kingston Planning Scheme which were identified in the adopted Kingston Planning Scheme Review 2012. The anomalies include mapping errors, redundant overlay provisions, land affected by multiple zones and land where the zone is inconsistent with its use or ownership.

The explanatory report provided at Appendix 1 includes a list of all sites affected by the Amendment and a description of proposed changes.

The Amendment was placed on public exhibition for four (4) weeks from 15 June to 16 July 2018. During exhibition of the Amendment one submission was received from VicRoads in relation a site at 2-8 Balcombe Road, Mentone (Doris Stockton Park) and one late submission was received from Melbourne Water in relation a site at 5 Long Island Point, Patterson Lakes.

The submission received from VicRoads in relation land at 2-8 Balcombe Road objected to the site's proposed rezoning to Public Park and Recreation Zone (PPRZ) on the basis the land is still in the ownership of VicRoads. Following consultation with VicRoads, Council had previously resolved to explore acquisition of the site mindful its current use as a park and ongoing maintenance by Council. The acquisition of the site has not yet occurred and, as the land is not in Council ownership, VicRoads have submitted that it would be premature for Council to rezone the site from the current Commercial 1 Zone (C1Z) to the proposed Public Park and Recreation Zone (PPRZ).

A further late submission was received on the 20 August 2018, from Melbourne Water in relation a site at 5 Long Island Point, Patterson Lakes which is nominated to be rezoned from GRZ to PPRZ. Melbourne Water have stated in their submission that this site is used by their operations staff to provide maintenance around the lakes in addition to providing passive access to the lake. Melbourne Water have identified a preference for the site to remain as GRZ (of which the entire area of the lakes reserve is zoned).

On the basis of the submissions received, and subsequent discussions between Council officers, VicRoads and Melbourne Water, it is proposed that the properties at 2-8 Balcombe Road, Mentone and 5 Long Island Point, Patterson Lakes be removed from the Amendment. Both submitters have indicated that they are supportive of this outcome.

Following a final internal review of all properties, Council Officers have removed any Council owned site that was proposed to be rezoned to Public Use Zone and 5 sites proposed for inclusion in the PPRZ. These sites require further investigation to confirm their current and future status as recreation reserves. Subject to the outcomes of this review and further discussions with Council's Manager Property Services, these sites may be included in a future anomalies amendment.

In addition to the above, two privately owned sites were identified for removal from the current Amendment. In both instances it was the view of officers that further investigation is required to determine the appropriate zone to be applied to address the current anomaly. These sites are located at 22-36 Bulli St, Moorabbin and 96-100 Gladesville Boulevard, Patterson Lakes.

It is recommended that Council adopt Amendment C149, subject to the removal of the 22 sites as outlined above and submit Amendment C149 to the Minister for Planning for approval.

The report also proposes to rezone Council owned land at 12-14 Kerr Crescent, Aspendale Gardens and at 77-83 Nurten Parade, Aspendale Gardens that has previously has been used for recreational purposes from part General Residential Zone and part Public Use Zone Schedule 1 (Service and Utility) to a Public Park and Recreation Zone. The proposed zone accurately reflects the established and intended future use of the land. It is important to note that this land is also subject to community consultation regarding the development of soccer fields for the Aspendale Stingrays Soccer Club.

2. Background

At its Planning Committee Meeting of 22 November 2017, Council resolved to seek authorisation from the Minister for Planning to prepare Amendment C149 to the Kingston Planning Scheme.

Amendment C149, as exhibited, consisted of 268 anomalies which require rezoning of land that has been incorrectly zoned or is located in dual zones, and minor amendments to maps to correct errors including updating the Heritage Schedule to correctly identify the exact location of properties included in the Schedule. The identified anomalies are considered administrative in nature which are unlikely to negatively impact upon property owners, developers, current applicants or other parties.

The Amendment applies to sites throughout the City of Kingston. Council officers identified that those sites could be divided into the following categories:

- Council owned reserves not currently zoned Public Park and Recreation Zone (PPRZ) and Council owned land in dual or inconsistent zoning: 226 sites.
- Publically owned sites (Melbourne Water, CFA and MFA): 11 sites.
- Privately owned properties with dual zones or inconsistent zones: 15 sites.
- Sites which require overlay changes: 15 sites.

Detail of sites included in the exhibited Amendment is provided at Appendix 1.

Authorisation to prepare the Amendment was received from the Minister for Planning on 10 April 2018.

The Amendment was placed on public exhibition for a period of four (4) weeks between 14 June 2018 and 16 July 2018. During exhibition of the Amendment one submission was received from VicRoads in relation a site at 2-8 Balcombe Road, Mentone and one late submission was received from Melbourne Water in relation a site at 5 Long Island Point, Patterson Lakes. Both submissions requested that the above sites be removed from the proposed Amendment.

3. Discussion

3.1 Council Plan Alignment

Goal 1 - Our well-planned, liveable city supported by infrastructure to meet future needs

Direction 1.1 - Intergenerational land use planning for a sustainable community

The Amendment will correct a range of zoning and overlay and mapping errors which will improve the efficiency of the Kingston Planning Scheme. Correcting these errors was identified in the Kingston Planning Scheme Review 2012, which was adopted by Council at its Ordinary Council Meeting of 26 March 2012.

3.2 Exhibition

Amendment C149 was placed on public exhibition for a period of four (4) weeks between 14 June 2018 and 16 July 2018. Notice was given as follows:

- Notice of the Amendment was published in the Victorian Government Gazette on 14 June 2018.
- Notice of the Amendment was published in the Moorabbin and Mordialloc/Chelsea Leaders on 14 June 2018.
- Direct notification was sent via ordinary mail to prescribed Ministers and all landowners and occupiers affected by the Amendment on 12 June 2018.
- Amendment documents were made available for viewing at the Cheltenham Customer Service Centre.

- Amendment documentation was made available on Council's website and the Department of Environment, Land, Water and Planning website from 14 June 2018.

3.3 Submissions

During exhibition of the Amendment one submission was received from VicRoads in relation a site at 2-8 Balcombe Road, Mentone and one late submission was received from Melbourne Water in relation a site at 5 Long Island Point, Patterson Lakes.

VicRoads Submission

A report was prepared and went to the Ordinary Council Meeting on 27 March 2017 to seek Council's approval to acquire land at 2-8 Balcombe Road (known as Doris Stockton Park). VicRoads had previously initiated discussions with Council mid 2016 regarding four parcels of land that they have deemed surplus to their needs. It is noted that the land is currently used as a park and is maintained by Council.

The acquisition of the site at 2-8 Balcombe Road, Mentone has not yet occurred and, mindful the land is still in the ownership of VicRoads, they submitted that it would be inappropriate for Council to prematurely rezone the site to the PPRZ from the Commercial 1 Zone (C1Z).

Following discussions with VicRoads and removal of the site from the Amendment, VicRoads agreed to withdraw their submission. Consequential changes to the Amendment were made to remove reference to the site from the exhibited explanatory report (refer to track changes at Appendix 1) and any maps that will be sent to the Minister for approval.

Subject to Council's future purchase of the land at 2-8 Balcombe Road, Mentone, the site would be included in a future anomalies amendment to be progressed by the Strategic Planning Team.

Melbourne Water Submission

One late submission was received on the 20 August 2018, from Melbourne Water. During the course of the Planning Scheme Review 2012, a number of sites including 5 Long Island Point, Patterson Lakes were nominated to be rezoned from GRZ to PPRZ.

Melbourne Water have stated in their late submission that this site is used by their operations staff to provide maintenance around the lakes in addition to providing passive access to the lake. As this site is owned by Melbourne Water and they would prefer it to remain as GRZ (of which the entire area of the lakes reserve is zoned), Council Officers have removed this site from the list of anomalies within the explanatory report (refer to track changes at Appendix 1).

With removal of the site at 2-8 Balcombe Road, Mentone, and 5 Long Island Point Patterson Lakes, the total number of sites to be impacted by the Amendment reduced from 268 to 266.

Internal Review

Following a final internal review of all properties, Council Officers have removed any Council owned site that was proposed to be rezoned to Public Use Zone for further consideration. Officers have also removed 5 sites proposed for inclusion in the PPRZ which require further investigation to confirm their current and future status as recreation reserves. The 18 Council owned properties proposed to be removed from the Amendment are listed below:

- Launching Way Reserve, Carrum
- 14 Baxter Avenue, Chelsea
- 13-15 Chelsea Rd, Chelsea
- 8 Chelsea Rd, Chelsea
- 8-12 Northcliffe Rd, Edithvale
- 1 McDonald Street, Mordialloc
- 13 Centreway, Mordialloc
- 27 Chute Street, Mordialloc
- 94 Parkers Rd, Parkdale
- 52-70 Thompson Rd, Patterson Lakes
- Reserve 1, Riverside West, Patterson Lakes
- Reserve, Riverside West, Patterson Lakes
- 8 Cypress Court, Oakleigh South
- 1A Dissik Street, Cheltenham
- 13 Broadchapel Place, Clarinda
- 47-49 Farm Road, Cheltenham
- 19-21 Jacobs Drive, Clarinda
- 100-116 Howard Road, Dingley

In addition to the above, two privately owned sites were identified for removal from the current Amendment. In both instances it was the view of officers that further investigation is required to determine the appropriate zone to be applied to address the current anomaly. These sites are:

- 22-36 Bulli St, Moorabbin
- 96-100 Gladesville Boulevard, Patterson Lakes

In order to proceed with the amendment in a timely fashion, it was agreed that the 22 sites referenced above would be removed from the current anomalies amendment with a further review to be undertaken to ensure consistency. Should these be required to be rezoned they can be included in the stage two anomalies amendment in 2019. Following these changes, the total number of sites to be corrected has reduced from 258 to 246. The complete list is attached in Appendix 1.

3.4 Options

3.4.1 Option 1

Request that the Minister for Planning approve Amendment C149 in line with the attached explanatory report and updated zoning map. This is the preferred option as the Amendment will correct a range of zoning and mapping errors which will improve the efficiency of the Kingston Planning Scheme.

3.4.2 Option 2

Do not proceed with the Amendment. This is not the preferred option as it would leave Council's Planning Scheme with mapping errors and would not deliver on the adopted outcomes of the 2012 Planning Scheme Review.

4. Conclusion

Amendment C149 seeks to correct 246 anomalies that are in the existing Scheme in line with the adopted Kingston Planning Scheme Review 2012. Following exhibition, two submissions were received and subsequently withdrawn subject to removal of the subject sites from the Amendment. It is recommended that Council adopt the Amendment and that a request be made to the Minister for Planning to approve Amendment C149.

Appendices

Appendix 1 - explanatory report C149 approval (Ref 18/618759)



Author/s:	Sarah Capenerhurst, Principal Strategic Planner
Reviewed and Approved By:	Paul Marsden, Manager City Strategy Jonathan Guttmann, General Manager Planning and Development

8.4

AMENDMENT C149 - ANOMALIES

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Planning and Environment Act 1987

KINGSTON PLANNING SCHEME
AMENDMENT C149
EXPLANATORY REPORT

Who is the planning authority?

This amendment has been prepared by the City of Kingston, which is the planning authority for this amendment.

The Amendment has been made at the request of the City of Kingston.

Land affected by the Amendment

The Amendment applies to 246 sites throughout the City of Kingston. Council has identified that these sites can be divided into the following categories;

- Council owned reserves (currently not zoned PPRZ) and Council owned land in dual or inconsistent zoning: 207
- Publicly owned sites (Melbourne Water, CFA, MFA): 11
- Privately owned properties with dual zones or inconsistent zone: 13 sites

A mapping reference table is attached at Appendix A to this Explanatory Report.

These 246 sites have been individually listed, with a detailed explanation and are listed by suburb below.

There are also 15 sites which require overlay changes which are listed in the below list under their relevant suburb.

These are:

- 1 Station Street, Aspendale, VIC 3195
- Rear of 440-443 Nepean Highway, Chelsea, VIC 3196
- 466 Warrigal Road, Heatherton, 3202
- Lot 1, Warrigal Road, Heatherton, 3203
- 34 Larnook Cres, Aspendale, VIC 3195
- Mordialloc Creek, Browns Reserve, Aspendale, VIC 3195
- 55 Nepean Highway, Aspendale, VIC 3195
- 16A Canberra St, Carrum, VIC 3197
- 3 Swan Walk, Chelsea, VIC 3196
- 1-3 Lynne St, Chelsea Heights, VIC 3196
- 4 Rimmer St, Mentone, VIC 3194
- 18-36 Levanto Street Mentone, VIC 3194
- 944-954 Nepean Highway, Moorabbin, VIC 3189
- 8-12 Lochiel Avenue, Edithvale, VIC 3196
- 1 The Fairway, Bonbeach, VIC 3196

What the amendment does

The amendment proposes to implement various corrections to the mapping of zones and overlays, in order to resolve anomalies in the Kingston Planning Scheme.

The anomalies include mapping errors, redundant overlay provisions, land affected by multiple zones and land where the zone is inconsistent with its use or ownership.

The amendment proposes to implement the following corrections:

Aspendale

- Rezone 89 Ebb Street, Aspendale, from General Residential Zone (Schedule 3) to Public Park and Recreation Zone (PPRZ).
- Rezone 57 Ebb Street, Aspendale, from General Residential Zone (Schedule 3) to Public Park and Recreation Zone (PPRZ).
- Rezone 6A Gnotuk Avenue, Aspendale, from General Residential Zone (Schedule 2) to Public Park and Recreation Zone (PPRZ).
- Update 1 Station Street, Aspendale heritage overlay map as (HO14) to change to Station Street frontage. Incorrectly identified on the current heritage overlay map.
- 34 Larnook Crescent, Aspendale, remove Environmental Significance Overlay (ESO) from the site as the tree has been removed.
- Mordialloc Creek, Browns Reserve, Aspendale, remove Environmental Significance Overlay (ESO) from the site as the tree has been removed.
- 55 Nepean Highway, Aspendale, remove Environmental Significance Overlay (ESO) from the site as the tree has been removed.

Aspendale Gardens

- Rezone 11 Ferntree Grove, Aspendale Gardens from General Residential Zone (Schedule 3) to Public Park and Recreation Zone (PPRZ).
- Rezone 19 Clair Court, Aspendale Gardens, from General Residential Zone (Schedule 3) to Public Park and Recreation Zone (PPRZ).
- Rezone 18 Amott Court, Aspendale Gardens, from General Residential Zone (Schedule 3) to Public Park and Recreation Zone (PPRZ).
- Rezone 56A Kearney Drive, Aspendale Gardens, from General Residential Zone (Schedule 3) and Public Use Zone (Schedule 1) to Public Park and Recreation Zone (PPRZ).
- Rezone 17-25 Winners Circle, Aspendale Gardens, from General Residential Zone (Schedule 3) to Public Park and Recreation Zone (PPRZ).
- Rezone 12-14 Kerr Crescent, Aspendale Gardens, from General Residential Zone (Schedule 3) and Public Use Zone (Schedule 1) to Public Park and Recreation Zone (PPRZ).
- Rezone 25A Branagan Drive, Aspendale Gardens, from General Residential Zone (Schedule 3) to Public Park and Recreation Zone (PPRZ).
- Rezone 12A Jacqueline Drive, Aspendale Gardens, from General Residential Zone (Schedule 3) to Public Park and Recreation Zone (PPRZ).
- Rezone 47 Nurten Parade, Aspendale Gardens, from General Residential Zone (Schedule 3) and Public Use Zone (Schedule 1) to Public Park and Recreation Zone (PPRZ).
- Rezone 44A Branagan Drive, Aspendale Gardens, from General Residential Zone (Schedule 3) to Public Park and Recreation Zone (PPRZ).

- Rezone 18 Atherton Close, Aspendale Gardens, from General Residential Zone (Schedule 3) to Public Park and Recreation Zone (PPRZ).
- Rezone 77-83 Nurten Parade Aspendale Gardens, from General Residential Zone (Schedule 3) and Public Use Zone (schedule 1) to Public Park and Recreation Zone (PPRZ).
- Rezone 10-20 Palm Grove Boulevard, Aspendale Gardens, from General Residential Zone (Schedule 3) to Public Park and Recreation Zone (PPRZ).
- Rezone 82-92 Branagan Drive, Aspendale Gardens, from General Residential Zone (Schedule 3) to Public Park and Recreation Zone (PPRZ).
- Rezone 27A Lance Close, Aspendale Gardens, from General Residential Zone (Schedule 3) to Public Park and Recreation Zone (PPRZ).
- Rezone 208 Wells Road, Aspendale Gardens, from General Residential Zone (Schedule 3) to Public Park and Recreation Zone (PPRZ).
- Rezone 35-39 Pacific Drive, Aspendale Gardens, from General Residential Zone (Schedule 3) to Public Park and Recreation Zone (PPRZ).
- Rezone 14-16 Jackie Court, Aspendale Gardens, from General Residential Zone (Schedule 3) to Public Park and Recreation Zone (PPRZ).
- Rezone 21 Denahy Court, Aspendale Gardens, from General Residential Zone (Schedule 3) to Public Park and Recreation Zone (PPRZ).
- Rezone 68-72 Pacific Drive, Aspendale Gardens, from General Residential Zone (Schedule 3) to Public Park and Recreation Zone (PPRZ).
- Rezone 25 Nurten Parade, Aspendale Gardens, from General Residential Zone (Schedule 3) to Public Park and Recreation Zone (PPRZ).
- Rezone 15 Wildoer Drive, Aspendale Gardens, from General Residential Zone (Schedule 3) to Public Park and Recreation Zone (PPRZ).

Bonbeach

- Rezone 43-45 Fowler Street Bonbeach, from General Residential Zone (Schedule 3) to Public Park and Recreation Zone (PPRZ).
- Rezone 86 Mascot Avenue Bonbeach, from General Residential Zone (Schedule 3) to Public Park and Recreation Zone (PPRZ).
- Rezone 10 Coast Banksia Drive, Bonbeach, from General Residential Zone (Schedule 2) to Public Park and Recreation Zone (PPRZ).
- Rezone Rear 12 Mascot Avenue, Bonbeach, from General Residential Zone (Schedule 3) to Public Park and Recreation Zone (PPRZ).
- Rezone 26 Coast Banksia Drive, Bonbeach, from General Residential Zone (Schedule 2) to Public Park and Recreation Zone (PPRZ).
- Rezone 70 Coast Banksia Drive, Bonbeach, from General Residential Zone (Schedule 2) to Public Park and Recreation Zone (PPRZ).
- Rezone 2-4 Williams Grove Bonbeach as the site is located in two zones (Commercial 1 Zone and General Residential Zone Schedule 2). The site is a four lot development. This address is the common driveway. The site should be located within the General Residential Zone Schedule 2.
- Rezone 1-2/2-4 Williams Grove, Bonbeach- the site is located within two zones. Incorrectly zoned as part of the commercial precinct to the east. Proposed to remove Commercial 1 Zone and replace entirely within the General Residential Zone Schedule 2.

- Rezone 538-539 Nepean Highway, Bonbeach- a small strip of land associated with this site is incorrectly located within the General Residential Zone (Schedule 2). The site is predominately located within the Commercial 1 Zone and should be rezoned accordingly.
- 1 The Fairway, Bonbeach, update the Heritage Overlay Map (HO18) as it is described incorrectly at the Patterson River Country Club.

Carrum

- Rezone 5 Dyson Road, Carrum, from General Residential Zone (Schedule 3 and Schedule 2) to Public Park and Recreation Zone (PPRZ).
- 16A Canberra Street, Carrum, remove Environmental Significance Overlay (ESO) from the site as the tree has been removed

Chelsea

- Rezone 87 Argyle Avenue, Chelsea, from General Residential Zone (Schedule 2) to General Residential Zone (Schedule 3).
- Rezone 3-5 Showers Avenue, Chelsea, from General Residential Zone (Schedule 2) to Public Park and Recreation Zone (PPRZ).
- Rezone 2 Thames Promenade, Chelsea. Site is the RSL Chelsea, a small portion of the building is located in the General Residential Zone (Schedule 2) along the western boundary. The site should be correctly rezoned into the Commercial 1 Zone.
- 3 Swan Walk, Chelsea, remove Environmental Significance Overlay (ESO) from the site as the tree has been removed
- Rear of 440-443 Nepean Highway, Chelsea, remove the Public Acquisition Overlay (Schedule 3) as the site has already been acquired by Council.

Chelsea Heights

- Rezone 1A Nimrod Rise, Chelsea Heights, from General Residential Zone (Schedule 3) to Public Park and Recreation Zone (PPRZ).
- Rezone 21 Amaroo Drive, Chelsea Heights, from General Residential Zone (Schedule 3) to Public Park and Recreation Zone (PPRZ).
- Rezone 50-58 Enterprize Avenue, Chelsea Heights, from General Residential Zone (Schedule 3) to Public Park and Recreation Zone (PPRZ).
- Rezone 30-38 Sixth Avenue, Chelsea Heights, from General Residential Zone (Schedule 3) to Public Park and Recreation Zone (PPRZ).
- Rezone 31-33 Talab Court, Chelsea Heights, from General Residential Zone (Schedule 3) to Public Park and Recreation Zone (PPRZ). 7-9 Endeavour Drive Chelsea Heights, VIC 3196
- Rezone 11A Puffin Close, Chelsea Heights, from General Residential Zone (Schedule 3) to Public Park and Recreation Zone (PPRZ).
- Rezone 22-24 Linton Close, Chelsea Heights, from General Residential Zone (Schedule 3) to Public Park and Recreation Zone (PPRZ).
- Rezone 73 Jabiru Drive, Chelsea Heights, from General Residential Zone (Schedule 3) to Public Park and Recreation Zone (PPRZ).
- Rezone 43-49 Fielding Drive Chelsea Heights, from General Residential Zone (Schedule 3) to Public Park and Recreation Zone (PPRZ).
- Rezone 9 Zeus Court, Chelsea Heights, from General Residential Zone (Schedule 3) to Public Park and Recreation Zone (PPRZ).

- Rezone Lot A (Prop No. 445153) Fielding Drive, Chelsea Heights, from General Residential Zone (Schedule 3) to Public Park and Recreation Zone (PPRZ).
- 1-3 Lynne Street, Chelsea Height, remove Environmental Significance Overlay (ESO) from the site as the tree has been removed

Cheltenham

- Rezone 25 Evergreen Circuit, Cheltenham, from General Residential Zone (Schedule 3) to Public Park and Recreation Zone (PPRZ).
- Rezone 10 Tintern Grove, Cheltenham, from General Residential Zone (Schedule 3) to Public Park and Recreation Zone (PPRZ).
- Rezone 131 Wilson Street, Cheltenham, from General Residential Zone (Schedule 2) to Public Park and Recreation Zone (PPRZ).
- Rezone 1A Dissik Street, Cheltenham, from General Residential Zone (Schedule 3) to Public Park and Recreation Zone (PPRZ).
- Rezone 27-29 Loma Street, Cheltenham, from General Residential Zone (Schedule 3) to Public Park and Recreation Zone (PPRZ).
- Rezone 15-17 Bettina Court, Cheltenham, from General Residential Zone (Schedule 3) to Public Park and Recreation Zone (PPRZ).
- Rezone Prop #481453 Evergreen Circuit, Cheltenham, from General Residential Zone (Schedule 3) to Public Park and Recreation Zone (PPRZ).
- Rezone 30-32 Blackburn Drive, Cheltenham, from General Residential Zone (Schedule 3) to Public Park and Recreation Zone (PPRZ).
- Rezone 1 Evergreen Circuit, Cheltenham, from General Residential Zone (Schedule 3) to Public Park and Recreation Zone (PPRZ).
- Rezone 4A Farm Road, Cheltenham, from General Residential Zone (Schedule 3) to Public Park and Recreation Zone (PPRZ).
- Rezone 50-52 Lincoln Drive, Cheltenham, from General Residential Zone (Schedule 3) to Public Park and Recreation Zone (PPRZ).
- Rezone 22A Hibiscus Avenue, Cheltenham, from General Residential Zone (Schedule 3) to Public Park and Recreation Zone (PPRZ).
- Rezone 5A Snowdon Drive, Cheltenham, from General Residential Zone (Schedule 3) to Public Park and Recreation Zone (PPRZ).
- Rezone 41 Nancy Street, Cheltenham, from General Residential Zone (Schedule 3) to Public Park and Recreation Zone (PPRZ).
- Rezone 1 Derring Lane, Cheltenham, from General Residential Zone (Schedule 3) to Public Park and Recreation Zone (PPRZ).
- Rezone 2 Booker Street, Cheltenham, from Public Use Zone (Schedule 4) to Public Park and Recreation Zone (PPRZ).
- Rezone 8-12 Judd Parade, Cheltenham, from General Residential Zone (Schedule 3) to Public Park and Recreation Zone (PPRZ).
- Rezone 7-9 Elliott Street, Cheltenham, from General Residential Zone (Schedule 3) to Public Park and Recreation Zone (PPRZ).
- Rezone 6-18 Stanley Avenue, Cheltenham, from General Residential Zone (Schedule 2) to Activity Centre Zone (Schedule 1)- this applies to only the library and carpark within the northern portion of the site. The playground known as 'Stanley Reserve' is to remain zoned as Public Park and Recreation Zone.
- Rezone 1A Goulbourn Street, Cheltenham, from Commercial 1 Zone to Activity Centre Zone (Schedule 1).

- Rezone 100 Farm Road, Cheltenham from Public Park and Recreation Zone to Green Wedge Zone (Schedule 2) as identified by the Green Wedge Plan 2012.

Clarinda

- Rezone 1-2 Shannon Court, Clarinda, from General Residential Zone (Schedule 3) to Public Park and Recreation Zone (PPRZ).
- Rezone 11-13 Debbie Close, Clarinda, from General Residential Zone (Schedule 3) to Public Park and Recreation Zone (PPRZ).
- Rezone 26-30 Dalbeattie Drive, Clarinda, from General Residential Zone (Schedule 3) to Public Park and Recreation Zone (PPRZ).
- Rezone 9 Hendon Court, Clarinda, from General Residential Zone (Schedule 3) to Public Park and Recreation Zone (PPRZ).
- Rezone 8 Tatra Close, Clarinda, from General Residential Zone (Schedule 3) to Public Park and Recreation Zone (PPRZ).
- Rezone 37-39 Leonard Close, Clarinda, from General Residential Zone (Schedule 3) to Public Park and Recreation Zone (PPRZ).
- Rezone 53A Schneider Crescent, Clarinda, from General Residential Zone (Schedule 3) to Public Park and Recreation Zone (PPRZ).
- Rezone 5-9 Drushi Court, Clarinda, from General Residential Zone (Schedule 3) to Public Park and Recreation Zone (PPRZ).
- Rezone 10 Rosewall Place, Clarinda, from General Residential Zone (Schedule 3) to Public Park and Recreation Zone (PPRZ).

Clayton South

- Rezone 525-529 Clayton Road, Clayton South, from General Residential Zone (Schedule 3) to Public Use Zone (Schedule 1).
- Rezone 13-15 Bemboka Avenue, Clayton South, from General Residential Zone (Schedule 3) to Public Park and Recreation Zone (PPRZ).
- Rezone 69-79 Springs Road, Clayton South, from General Residential Zone (Schedule 3) to Public Park and Recreation Zone (PPRZ).
- Rezone 54 Glenelg Drive, Clayton South, from General Residential Zone (Schedule 3) to Public Park and Recreation Zone (PPRZ).
- Rezone 7-9 Meppel Drive, Clayton South, from General Residential Zone (Schedule 3) to Public Park and Recreation Zone (PPRZ).
- Rezone 65 Botanic Drive, Clayton South, from General Residential Zone (Schedule 2) and the Public Use Zone (Schedule 4) to Public Park and Recreation Zone (PPRZ).
- Rezone Reserve (471649) Botanic Drive, Clayton South, from General Residential Zone (Schedule 3) to Public Park and Recreation Zone (PPRZ).
- Rezone 12-14 Sheldon Place, Clayton South, from General Residential Zone (Schedule 3) to Public Park and Recreation Zone (PPRZ).
- Rezone 15 Evergreen Boulevard, Clayton South, from General Residential Zone (Schedule 2) to Public Park and Recreation Zone (PPRZ).
- Rezone 3-4 Ricki Court, Clayton South, from General Residential Zone (Schedule 2) to Public Park and Recreation Zone (PPRZ).
- Rezone 123 Rosebank Avenue, Clayton South, from General Residential Zone (Schedule 3) to Public Park and Recreation Zone (PPRZ).
- Rezone 9-13 Scott Avenue, Clayton South, from General Residential Zone (Schedule 3) to Public Park and Recreation Zone (PPRZ).

- Rezone 515-521 Clayton Road, Clayton South, from General Residential Zone (Schedule 3) to Public Park and Recreation Zone (PPRZ).
- Rezone 5-7 Melosa Avenue, Clayton South, from General Residential Zone (Schedule 3) to Public Park and Recreation Zone (PPRZ).
- Rezone 1A Kitson Road, Clayton South, from General Residential Zone (Schedule 3) to Public Park and Recreation Zone (PPRZ).
- Rezone 64-72 Main Road, Clayton South, from General Residential Zone (Schedule 3) to Public Park and Recreation Zone (PPRZ).
- Rezone Lot 1 (458553) Main Road, Clayton South, from General Residential Zone (Schedule 3) to Public Park and Recreation Zone (PPRZ).
- Rezone 136-176 Osborne Avenue, Clayton South, from General Residential Zone (Schedule 3) to Public Park and Recreation Zone (PPRZ).
- Rezone 5 Larado Place, Clayton South, from General Residential Zone (Schedule 3) to Public Park and Recreation Zone (PPRZ).
- Rezone 26A Audsley Street, Clayton South from Commercial 1 Zone to General Residential Zone (Schedule 3).

Dingley

- Rezone 38-40 Wyndarra Crescent, Dingley Village, from General Residential Zone (Schedule 3) to Public Park and Recreation Zone (PPRZ).
- Rezone 26-28 Aisha Crescent, Dingley Village, from General Residential Zone (Schedule 3) to Public Park and Recreation Zone (PPRZ).
- Rezone 8 Newbridge Close, Dingley Village, from General Residential Zone (Schedule 3) to Public Park and Recreation Zone (PPRZ).
- Rezone (454295) Fernhill Drive, Dingley Village, from General Residential Zone (Schedule 3) to Public Park and Recreation Zone (PPRZ).
- Rezone 1A Caruana Drive, Dingley Village, from General Residential Zone (Schedule 3) to Public Park and Recreation Zone (PPRZ).
- Rezone 2A Wheatland Crescent, Dingley Village, from General Residential Zone (Schedule 3) to Public Park and Recreation Zone (PPRZ).
- Rezone 13-15 Golfwood Close, Dingley Village, from General Residential Zone (Schedule 3) to Public Park and Recreation Zone (PPRZ).
- Rezone 8 Village Drive, Dingley Village, from General Residential Zone (Schedule 3) to Public Park and Recreation Zone (PPRZ).
- Rezone 18-22 Christina Terrace, Dingley Village, from General Residential Zone (Schedule 3) to Public Park and Recreation Zone (PPRZ).
- Rezone 22 Golfwood Close, Dingley Village, from General Residential Zone (Schedule 3) to Public Park and Recreation Zone (PPRZ).
- Rezone 31-33 Heathland Way, Dingley Village, from General Residential Zone (Schedule 3) to Public Park and Recreation Zone (PPRZ).
- Rezone 22A Holland Avenue, Dingley Village, from General Residential Zone (Schedule 3) to Public Park and Recreation Zone (PPRZ).
- Rezone 41 Kingston Drive, Dingley Village, from General Residential Zone (Schedule 3) to Public Park and Recreation Zone (PPRZ).
- Rezone 10 Heathland Way, Dingley Village, from General Residential Zone (Schedule 3) to Public Park and Recreation Zone (PPRZ).
- Rezone 14 Seaton Drive, Dingley Village, from General Residential Zone (Schedule 3) to Public Park and Recreation Zone (PPRZ).

- Rezone 100-116 Howard Road, Dingley Village, from General Residential Zone (Schedule 3) to Public Park and Recreation Zone (PPRZ).
- Rezone 12-14 Greenwoods Close, Dingley Village, from General Residential Zone (Schedule 3) to Public Park and Recreation Zone (PPRZ).
- Rezone 18 Cannery Place, Dingley Village, from General Residential Zone (Schedule 3) to Public Park and Recreation Zone (PPRZ).
- Rezone 619-623 Lower Dandenong Road, Dingley Village, from General Residential Zone (Schedule 3) to Public Park and Recreation Zone (PPRZ).
- Rezone 359 Spring Road, Dingley Village, from General Residential Zone (Schedule 3) to Public Park and Recreation Zone (PPRZ).
- Rezone 26 Golf Links Crescent, Dingley Village, from General Residential Zone (Schedule 3) to Public Park and Recreation Zone (PPRZ).
- Rezone 13A Teralba Close, Dingley Village, from General Residential Zone (Schedule 3) to Public Park and Recreation Zone (PPRZ).
- Rezone 8 Williams Close, Dingley Village, from General Residential Zone (Schedule 3) to Public Park and Recreation Zone (PPRZ).
- Rezone Tree Reserve (451696) Howard Road, Dingley Village, from General Residential Zone (Schedule 3) to Public Park and Recreation Zone (PPRZ).
- Rezone 15 Balcombe Place, Dingley Village, from General Residential Zone (Schedule 3) to Public Park and Recreation Zone (PPRZ).

Edithvale

- Rezone 206 Station Street, Edithvale, from General Residential Zone (Schedule 2) to Public Use Zone (Schedule 1).
- Rezone Reserve Randall Avenue, Edithvale, from General Residential Zone (Schedule 3) to Public Park and Recreation Zone (PPRZ).
- Rezone 47A Lochiel Avenue, Edithvale, from General Residential Zone (Schedule 3) to Public Park and Recreation Zone (PPRZ).
- Rezone Reserve 51 Hughes Avenue, Edithvale, from General Residential Zone (Schedule 3) to Public Park and Recreation Zone (PPRZ).
- Rezone 45A Randall Avenue, Edithvale, from General Residential Zone (Schedule 3) to Public Park and Recreation Zone (PPRZ).
- Rezone 3-4 Waters Edge Place from Commercial 1 Zone to General Residential Zone (Schedule 2). Both sites are residential units and have their rear (eastern boundary) secluded private open space incorrectly located within the Commercial 1 Zone.
- Rezone 256-260 Station Street, Edithvale which is a privately run Aged Care Facility. Currently located within the Public Use Zone (Schedule 1) and General Residential Zone (Schedule 3). The PUZ1 should be removed.
- 8-12 Lochiel Avenue, Edithvale- site is affected by both HO47 and ESO3. It is described and mapped incorrectly. Site is St. Columbus Church. Under HO47, it states the wrong address.

Heatherton

- Rezone Reserve at rear of 49-59 Sunmore Close/Barkers Street, Heatherton, from Commercial 2 Zone Zone (Schedule 3) to Public Park and Recreation Zone (PPRZ).
- Rezone 10 Henry Street, Heatherton, from General Residential Zone (Schedule 3) to Public Park and Recreation Zone (PPRZ).

- Rezone 105 St Andrews Drive, Heatherton, from General Residential Zone (Schedule 3) to Public Park and Recreation Zone (PPRZ).
- Rezone 42 San Lorenzo Court, Heatherton, from General Residential Zone (Schedule 3) to Public Park and Recreation Zone (PPRZ).
- Rezone 74 St Andrews Drive, Heatherton, from General Residential Zone (Schedule 3) to Public Park and Recreation Zone (PPRZ).
- Rezone 88-90 St Andrews Drive Heatherton, from Road Zone Category 1 Zone and General Residential Zone (Schedule 3) to Public Park and Recreation Zone (PPRZ).
- Rezone 23 Birkdale Avenue, Heatherton from General Residential Zone (Schedule 3) to Public Park and Recreation Zone (PPRZ).
- Rezone 16 Augusta Square, Heatherton, from General Residential Zone (Schedule 3) to Public Park and Recreation Zone (PPRZ).
- Rezone 26 St Andrews Drive Heatherton, from General Residential Zone (Schedule 3) to Public Use Zone (Schedule 6).
- Rezone 68 St Andrews Drive Heatherton, from General Residential Zone (Schedule 3) to Public Park and Recreation Zone (PPRZ).
- Rezone 26 Muirfield Close, Heatherton, from General Residential Zone (Schedule 3) to Public Park and Recreation Zone (PPRZ).
- Rezone 15 Augusta Square, Heatherton, from General Residential Zone (Schedule 3) to Public Park and Recreation Zone (PPRZ).
- Rezone 15 St Andrews Drive, Heatherton, from General Residential Zone (Schedule 3) to Public Park and Recreation Zone (PPRZ).
- Rezone Reserve (471409) Augusta Square, Heatherton, from General Residential Zone (Schedule 3) to Public Park and Recreation Zone (PPRZ).
- Rezone 28 Oakmont Crescent, Heatherton, from General Residential Zone (Schedule 3) to Public Park and Recreation Zone (PPRZ).
- Rezone 24-30 St Georges Crescent, Heatherton, from General Residential Zone (Schedule 3) to Public Park and Recreation Zone (PPRZ).
- Rezone 17-23 Cypress Point Parade Heatherton, from General Residential Zone (Schedule 3) to Public Park and Recreation Zone (PPRZ).
- Rezone 2-6 Sunningdale Way Heatherton, from General Residential Zone (Schedule 3) to Public Park and Recreation Zone (PPRZ).
- Rezone Tree Reserve (471285) Pinehurst Way, Heatherton, from General Residential Zone (Schedule 3) to Public Park and Recreation Zone (PPRZ).
- Rezone 8-10 Oakmont Crescent, Heatherton, from General Residential Zone (Schedule 3) to Public Park and Recreation Zone (PPRZ).
- Rezone 2-4 Arco Lane, Heatherton, from Commercial 2 Zone to Public Use Zone (Schedule 1).
- 466 Warrigal Road, Heatherton, remove the Public Acquisition Overlay (Schedule 1) as the site has already been acquired by Council.
- Lot 1 Warrigal Road, Heatherton, remove the Public Acquisition Overlay (Schedule 2) as the site has already been acquired by Council.

Highett

- Rezone 150 Wickham Road, Highett, from General Residential Zone (Schedule 3) to Public Use Zone (Schedule 1).
- Rezone 152 Wickham Road, Highett, a small triangular piece of land located in the south west corner of the site is incorrectly located in the Public Use Zone (Schedule 1). The site should only be located within the General Residential Zone (Schedule 3).

- Rezone 1 McFarlane Court, Highett, from General Residential Zone (Schedule 3) to Public Park and Recreation Zone (PPRZ).

Mentone

- Rezone 200-204 Balcombe Road, Mentone, from General Residential Zone (Schedule 3) to Public Park and Recreation Zone (PPRZ).
- Rezone 1A Phillip Street, Mentone, from General Residential Zone (Schedule 2) to Public Park and Recreation Zone (PPRZ).
- Rezone 22-24 Mentone Parade Mentone, from General Residential Zone (Schedule 3) to Public Park and Recreation Zone (PPRZ).
- 4 Rimmer Street, Mentone, remove Environmental Significance Overlay (ESO) as tree has been removed.
- 18-36 Levanto Street, Mentone, remove Development Plan Overlay (Schedule 3) as this is a redundant control.

Moorabbin

- Rezone 462678 (Property Number) Cooma Street, Moorabbin, from General Residential Zone (Schedule 3) to Public Park and Recreation Zone (PPRZ).
Rezone 4 Kingsway, Moorabbin, a small section of the north east corner of the site is incorrectly located within the Industrial 3 Zone. The site is used for industry purposes with a number of warehouses on the site and should be located in the Industrial 1 Zone only.
- 944-954 Nepean Highway, Moorabbin, update Heritage Overlay (HO119) as the site is incorrectly map (Moorabbin Tenpin Bowls site).

Mordialloc

- Rezone 31 Albert Street, Mordialloc, from General Residential Zone (Schedule 2) to Public Use Zone (Schedule 1).
- Rezone Lot 1 (493241) Ashmore Avenue, Mordialloc, from General Residential Zone (Schedule 3) to Public Park and Recreation Zone (PPRZ).
- Rezone 2 Duggan Street, Mordialloc, from General Residential Zone (Schedule 3) to Public Park and Recreation Zone (PPRZ).
- Rezone 31 Waratah Avenue Mordialloc, from General Residential Zone (Schedule 3) to Public Park and Recreation Zone (PPRZ).
- Rezone 59A Brownfield Street Mordialloc, from General Residential Zone (Schedule 3) to Public Park and Recreation Zone (PPRZ).
- Rezone 26 Purtell Close, Mordialloc, from General Residential Zone (Schedule 3) to Public Park and Recreation Zone (PPRZ).
- Rezone 34A Purtell Close, Mordialloc, from General Residential Zone (Schedule 3) to Public Park and Recreation Zone (PPRZ).
- Rezone 7-9 Hall Mark Road, Mordialloc, from General Residential Zone (Schedule 3) to Public Park and Recreation Zone (PPRZ).
- Rezone 15 Reckless Lane, Mordialloc, from General Residential Zone (Schedule 3) to Public Park and Recreation Zone (PPRZ).
- Rezone 8-10 Hall Mark Road, Mordialloc, from General Residential Zone (Schedule 3) to Public Park and Recreation Zone (PPRZ).
- Rezone 72-76 Jack Holt Way Mordialloc, from General Residential Zone (Schedule 3) to Public Park and Recreation Zone (PPRZ).

- Rezone 65-85 Governor Road, Mordialloc, from General Residential Zone (Schedule 3) to Public Park and Recreation Zone (PPRZ).

Oakleigh South

- Rezone 28 Dowling Road, Oakleigh South, from General Residential Zone (Schedule 3) to Public Park and Recreation Zone (PPRZ).
- Rezone 1A Washington Drive, Oakleigh South, from General Residential Zone (Schedule 3) to Public Park and Recreation Zone (PPRZ).
- Rezone 61-65 Dowling Road Oakleigh South, from General Residential Zone (Schedule 3) to Public Park and Recreation Zone (PPRZ).
- Rezone 97 Clarinda Road, Oakleigh South, from General Residential Zone (Schedule 3) to Public Park and Recreation Zone (PPRZ).
- Rezone 43A Dealing Drive, Oakleigh South, from General Residential Zone (Schedule 3) to Public Park and Recreation Zone (PPRZ).
- Rezone 40A Sherbrooke Avenue Oakleigh South, from General Residential Zone (Schedule 3) to Public Park and Recreation Zone (PPRZ).
- Rezone 548-558 Warrigal Road, Oakleigh South, from General Residential Zone (Schedule 3) to Road Zone (Category 1).
- Rezone 10 Vanessa Court, Oakleigh South, from General Residential Zone (Schedule 3) to Public Park and Recreation Zone (PPRZ).
- Rezone 17 Vanessa Court, Oakleigh South, from General Residential Zone (Schedule 3) to Public Park and Recreation Zone (PPRZ).
- Rezone 57-59 Dowling Road Oakleigh South, from General Residential Zone (Schedule 3) to public Use Zone (Schedule 1).
- Rezone 1-17 Carroll Road, Oakleigh South, from Special Use Zone (Schedule 2) and Road Zone (Category 1) to Public Park and Recreation Zone (PPRZ).

Parkdale

- Rezone 4A-4B Meribah Court, Parkdale, from General Residential Zone (Schedule 3) to Public Park and Recreation Zone (PPRZ).
- Rezone 275-295, Nepean Highway, Parkdale, from General Residential Zone (Schedule 3) to Public Park and Recreation Zone (PPRZ).
- Rezone 9 Grey Street, Parkdale, from General Residential Zone (Schedule 3) to Public Park and Recreation Zone (PPRZ).
- Rezone 58A Robert Street, Parkdale, from General Residential Zone (Schedule 3) to Public Park and Recreation Zone (PPRZ).

Patterson Lakes

- Rezone 15-17 Harbour Drive Patterson Lakes, from General Residential Zone (Schedule 3) to Public Park and Recreation Zone (PPRZ).
- Rezone 2A Gladesville Boulevard Patterson Lakes, from General Residential Zone (Schedule 3) to Public Park and Recreation Zone (PPRZ).
- Rezone 11 William Salthouse Way Patterson Lakes, from General Residential Zone (Schedule 3) to Public Park and Recreation Zone (PPRZ).
- Rezone 48 Wetland Drive Patterson Lakes, from General Residential Zone (Schedule 3) to Public Park and Recreation Zone (PPRZ).
- Rezone 14-16 Portal Place Patterson Lakes, from General Residential Zone (Schedule 3) to Public Park and Recreation Zone (PPRZ).

- Rezone 56 Wetland Drive Patterson Lakes, from General Residential Zone (Schedule 3) to Public Park and Recreation Zone (PPRZ).
- Rezone Reserve Schooner Bay Drive, Patterson Lakes, from General Residential Zone (Schedule 3) to Public Park and Recreation Zone (PPRZ).
- Rezone 21 Boloka Court Patterson Lakes, from General Residential Zone (Schedule 3) to Public Park and Recreation Zone (PPRZ).
- Rezone 30A Snapper Point Drive Patterson Lakes, from General Residential Zone (Schedule 3) to Public Park and Recreation Zone (PPRZ).
- Rezone 36 Myola Street Patterson Lakes, from General Residential Zone (Schedule 3) to Public Park and Recreation Zone (PPRZ).
- Rezone 31 Arrunga Court Patterson Lakes, from General Residential Zone (Schedule 3) to Public Park and Recreation Zone (PPRZ).
- Rezone 16 Manhattan Court Patterson Lakes, from General Residential Zone (Schedule 3) to Public Park and Recreation Zone (PPRZ).
- Rezone 2A Lagoon Place Patterson Lakes, from General Residential Zone (Schedule 3) to Public Park and Recreation Zone (PPRZ).
- Rezone 22 Kalang Court Patterson Lakes, from General Residential Zone (Schedule 3) to Public Park and Recreation Zone (PPRZ).
- Rezone 3A Acacia Crescent Patterson Lakes, from General Residential Zone (Schedule 3) to Public Park and Recreation Zone (PPRZ).
- Rezone 11 Balarang Court Patterson Lakes, from General Residential Zone (Schedule 3) to Public Park and Recreation Zone (PPRZ). Rezone 52 Palm Beach Drive Patterson Lakes, from General Residential Zone (Schedule 3) to Public Park and Recreation Zone (PPRZ).
- Reserve 14 Illawong Court Patterson Lakes, from General Residential Zone (Schedule 3) to Public Park and Recreation Zone (PPRZ).
- Rezone 9A Erinka Crescent Patterson Lakes, from General Residential Zone (Schedule 3) to Public Park and Recreation Zone (PPRZ).
- Rezone 27 Colac Court Patterson Lakes, from General Residential Zone (Schedule 3) to Public Park and Recreation Zone (PPRZ).
- Rezone 165 Gladesville Boulevard, Patterson Lakes, from General Residential Zone (Schedule 3) to Public Park and Recreation Zone (PPRZ).
- Rezone 131 Gladesville Boulevard Patterson Lakes, from General Residential Zone (Schedule 3) to Public Park and Recreation Zone (PPRZ).
- Rezone Reserve 3 Wetland Drive, Patterson Lakes, from General Residential Zone (Schedule 3) to Public Use Zone (Schedule 1).
- Rezone Reserve 4 Wetland Drive Patterson Lakes, from General Residential Zone (Schedule 3) to Public Use Zone (Schedule 1).
- Rezone 42-44 Palm Beach Drive Patterson Lakes, from General Residential Zone (Schedule 3) to Public Park and Recreation Zone (PPRZ).
- Rezone 2A Shell Court, Patterson Lakes, from General Residential Zone (Schedule 3) to Public Park and Recreation Zone (PPRZ).
- Rezone 14A/102-114 Gladesville Boulevard, Patterson Lakes. This is the car park associated with the shopping centre at Patterson Lakes. Used solely for commercial purposes yet has a dual zone. The General Residential Zone (Schedule 2) should be removed and replaced with the Commercial 1 Zone.

Waterways

- Rezone 28-36 Macquarie Circle Waterways, from General Residential Zone (Schedule 3) to Public Park and Recreation Zone (PPRZ).
- Rezone Reserve (496004) Lake King Circle, Waterways, from General Residential Zone (Schedule 3) to Public Park and Recreation Zone (PPRZ).
- Rezone 22 Macquarie Circle, Waterways, from Public Use Zone (Schedule 6) to Public Park and Recreation Zone (PPRZ).
- Rezone 12-16 Princetown Drive, Waterways, from Public Use Zone (Schedule 6) to Public Park and Recreation Zone (PPRZ).
- Rezone 5-7 Broadwater Drive, Waterways, from Public Use Zone (Schedule 6) to Public Park and Recreation Zone (PPRZ).
- Rezone 5-7 Shoalwater Drive Waterways, from Public Use Zone (Schedule 6) to Public Park and Recreation Zone (PPRZ).
- Rezone 2 Paragon Way, Waterways, from General Residential Zone (Schedule 3) to Public Park and Recreation Zone (PPRZ).
- Rezone 1-3 Shoalwater Drive, Waterways, from General Residential Zone (Schedule 3) to Public Park and Recreation Zone (PPRZ).
- Rezone 31 Coorong Circle, Waterways, from General Residential Zone (Schedule 3) to Public Park and Recreation Zone (PPRZ).
- Rezone 7 Coorong Circle, Waterways, from General Residential Zone (Schedule 3) to Public Park and Recreation Zone (PPRZ).
- Rezone 2-36 Tyrrell Terrace Waterways, from General Residential Zone (Schedule 3) to Public Park and Recreation Zone (PPRZ).
- Rezone 3 Mitta Avenue, Waterways, from General Residential Zone (Schedule 3) to Public Park and Recreation Zone (PPRZ).
- Rezone 26 Portland Place Waterways, from General Residential Zone (Schedule 3) to Public Park and Recreation Zone (PPRZ).
- Rezone Reserve South (495992) Lake King Circle, Waterways, from Public Use Zone (Schedule 6) to Public Park and Recreation Zone (PPRZ).

Strategic assessment of the Amendment

Why is the Amendment required?

The Amendment is required to correct zoning and overlay anomalies and errors that exist within the maps of the current Kingston Planning Scheme.

In order to provide continuous improvement of the Kingston Planning Scheme, practical resolutions of the identified zoning inconsistencies at each of the 246 sites are necessary. Additionally, the corrections will allow for greater consistency with State Policies. The Strategic Planning Unit keeps a record of errors and anomalies that have been identified in the Scheme. All of the anomalies and errors are minor in nature and do not affect the intent of policy or controls.

How does the Amendment implement the objectives of planning in Victoria?

The amendment gives effect to the following objectives of planning in Victoria identified in Section 4(1) of the *Planning and Environment Act 1987*:

- To provide for the fair, orderly, economic and sustainable use, and development of land;
- To secure a pleasant, efficient and safe working, living and recreational environment for all Victorians and visitors to Victoria;
- To protect public utilities and other facilities for the benefit of the community;
- To facilitate development in accordance with the above objectives;
- To balance the present and future interests of all Victorians.

How does the Amendment address any environmental, social and economic effects?

The amendment will not have any significant environmental, social or economic effects, as the amendment is correctional in nature. It will have a slight positive net community impact by enhancing the effectiveness and efficiency of the Kingston Planning Scheme

Does the Amendment address relevant bushfire risk?

There is no bushfire risk identified for the subject land and it is considered that these provisions are not relevant to this amendment.

Does the Amendment comply with the requirements of any Minister's Direction applicable to the amendment?

The Amendment is consistent with *Ministerial Direction 11 – Strategic Assessment of Amendments* under section 7(5) of the *Planning and Environment Act 1987*.

The amendment is consistent with Ministerial Direction: The Form and Content of Planning Schemes.

The amendment complies with *Ministerial Direction No. 9: Metropolitan Planning Strategy* which requires amendments to have regard to *Plan Melbourne: Metropolitan Planning Strategy*. The amendment also addresses the requirements of *Ministerial Direction No. 11: Strategic Assessment of Amendments*.

Specifically, the amendment will assist in the implementation of the objectives of *Plan Melbourne: Metropolitan Planning Strategy* and the objectives of planning in Victoria, as it will resolve inconsistencies and errors in the Kingston Planning Scheme.

The requirements of *Ministerial Direction No. 1: Potentially Contaminated Land* have also been considered and addressed.

How does the Amendment support or implement the Planning Policy Framework and any adopted State policy?

The amendment supports the Planning Policy Framework (PPF) by implementing corrections to anomalies in the Kingston Planning Scheme. The amendment is corrective in its nature and does not result in any significant changes to the Kingston Planning Scheme, it is therefore considered

that the amendment has no significant impact on the implementation of the Planning Policy Framework.

The amendment responds to the Planning Policy Framework as follows:

Clause 11 Settlement- The amendment responds to the need for and as far as practicable, towards the protection of environmentally sensitive areas and natural resources.

Clause 11.02-1S Supply of Urban Land- the amendment responds to the need to ensure the ongoing provision of land supporting infrastructure to support sustainable urban development, by ensuring existing parks and reserves are located in the appropriate zone which gives reassurance to the broader community that these places are to remain as parks/reserves.

Clause 12- Environmental and Landscape Values- The amendment seeks to conserve areas with identified environmental and landscape values.

Clause 15.01- Built Environment- the amendment builds upon the objective to create urban environments that are safe, healthy, functional and enjoyable by protecting public open spaces to support the amenity and attractiveness of Kingston's public realm.

Clause 19.02-6S- Open Space 'The amendment responds to the strategies in this clause by facilitating improvements to the quality of open space and rezoning existing recreational areas into the correct zone (PPRZ), contributing to the long term protections of areas of public open spaces and facilitates to the efficient, long term management and ongoing use of recreational areas and public open spaces.

The amendment is consistent with these objectives as it ensures that the planning controls which apply to land within the municipality are accurate, appropriate and consistent with the SPPF

How does the Amendment support or implement the Local Planning Policy Framework, and specifically the Municipal Strategic Statement?

The amendment supports and will help to implement the following objectives and strategies of the *Local Planning Policy Framework*:

Clause 21.14 – Performance Monitoring and Review by ensuring the Kingston Planning Scheme is responsive to changing local circumstances and new land use opportunities and issues. This requires the Kingston Planning Scheme is to be reviewed at least once every three years. The proposed corrections have been identified through an on-going review of the Kingston Planning Scheme.

Does the Amendment make proper use of the Victoria Planning Provisions?

The amendment is correctional in nature and seeks to resolve anomalies in the Kingston Planning Scheme. All proposed changes to the Kingston Planning Scheme are consistent with the Victorian Planning Provisions. Sites that are nominated to be rezoned have being nominated to the appropriate zone to ensure that the recreational, open space and environmental values of the land are recognised and protected. The application of the PPRZ will accurately reflect the public ownership, current use and conditions of the land.

How does the Amendment address the views of any relevant agency?

Internal consultation occurred between the relevant departments with regards to all Council-owned land and any inconsistencies that should necessitate a zone change. The views of relevant agencies were considered through preliminary consultation and the exhibition as part of the Planning Scheme Review.

Does the Amendment address relevant requirements of the Transport Integration Act 2010?

The amendment will not have a significant impact on the transport system, as defined by section 3 of the *Transport Integration Act 2010*.

Resource and administrative costs**• What impact will the new planning provisions have on the resource and administrative costs of the responsible authority?**

It is not anticipated that the amendment will add additional resource costs for Council. The proposed corrections should result in a slight reduction in officer time required for the assessment of planning permit application through removing unnecessary or inconsistent planning controls. Overall this is not expected to result in a significant impact on the resource

Where you may inspect this Amendment

The Amendment is available for public inspection, free of charge, during office hours at the following places:

City of Kingston
Level 1
1230 Nepean Hwy, Cheltenham
www.kingston.vic.gov.au

The Amendment can also be inspected free of charge at the Department of Environment, Land, Water and Planning website at www.delwp.vic.gov.au/public-inspection.

For further information about amendment C149, please contact Sarah Capenerhurst, Principal Strategic Planner, at Kingston City Council on 95814364.

Submissions

Any person who may be affected by the Amendment may make a submission to the planning authority. Submissions about the Amendment must be received by 16 July 2018. A submission must be sent to:

City of Kingston
PO Box 1000
Mentone 3194

Panel hearing dates

In accordance with clause 4(2) of Ministerial Direction No.15 the following panel hearing dates have been set for this amendment:

- directions hearing: 27 August 2018
- panel hearing: 18 September 2018

ATTACHMENT A - Mapping reference table

Location	Land /Area Affected	Mapping Reference
Highett	150 Wickham Road "Highett, VIC 3190"	Kingston C149 001znMap01 Exhibition
Highett	1 McFarlane Court "Highett, VIC 3190"	Kingston C149 001znMap01 Exhibition
Moorabbin	462678 (Property Number) Cooma Street "Moorabbin, VIC 3189"	Kingston C149 001znMap01 Exhibition
Heatherton	24-30 St Georges Crescent "Heatherton, VIC 3202"	Kingston C149 Map 02ZN and 05ZN Exhibition
Clarinda	1-2 Shannon Court "Clarinda, VIC 3168"	Kingston C149 002znMap02 Exhibition
Clarinda	11-13 Debbie Close "Clarinda, VIC 3169"	Kingston C149 002znMap02 Exhibition
Clarinda	26-30 Dalbeattie Drive "Clarinda, VIC 3169"	Kingston C149 002znMap02 Exhibition
Clarinda	9 Hendon Court "Clarinda, VIC 3169"	Kingston C149 002znMap02 Exhibition
Clarinda	8 Tatra Close "Clarinda, VIC 3169"	Kingston C149 002znMap02 Exhibition
Clarinda	37-39 Leonard Close "Clarinda, VIC 3169"	Kingston C149 002znMap02 Exhibition
Clarinda	53A Schneider Crescent "Clarinda, VIC 3169"	Kingston C149 002znMap02 Exhibition
Clarinda	10 Rosewall Place "Clarinda, VIC 3196"	Kingston C149 002znMap02 Exhibition
Heatherton	Reserve at rear of 49-59 Sunmore Close/Barkers Street "Heatherton, VIC 3202"	Kingston C149 002znMap02 Exhibition
Heatherton	10 Henry Street "Heatherton, VIC 3202"	Kingston C149 002znMap02 Exhibition
Heatherton	105 St Andrews Drive "Heatherton, VIC 3202"	Kingston C149 002znMap02 Exhibition
Heatherton	42 San Lorenzo Court "Heatherton, VIC 3202"	Kingston C149 002znMap02 Exhibition

Location	Land /Area Affected	Mapping Reference
Heatherton	74 St Andrews Drive "Heatherton, VIC 3202"	Kingston C149 002znMap02 Exhibition
Heatherton	88-90 St Andrews Drive "Heatherton, VIC 3202"	Kingston C149 002znMap02 Exhibition
Heatherton	23 Birkdale Avenue "Heatherton, VIC 3202"	Kingston C149 002znMap02 Exhibition
Heatherton	16 Augusta Square "Heatherton, VIC 3202"	Kingston C149 002znMap02 Exhibition
Heatherton	26 St Andrews Drive "Heatherton, VIC 3202"	Kingston C149 002znMap02 Exhibition
Heatherton	68 St Andrews Drive "Heatherton, VIC 3202"	Kingston C149 002znMap02 Exhibition
Heatherton	26 Muirfield Close "Heatherton, VIC 3202"	Kingston C149 002znMap02 Exhibition
Heatherton	15 Augusta Square "Heatherton, VIC 3202"	Kingston C149 002znMap02 Exhibition
Heatherton	15 St Andrews Drive "Heatherton, VIC 3202"	Kingston C149 002znMap02 Exhibition
Heatherton	Reserve (471409) Augusta Square "Heatherton, VIC 3202"	Kingston C149 002znMap02 Exhibition
Heatherton	28 Oakmont Crescent "Heatherton, VIC 3202"	Kingston C149 002znMap02 Exhibition
Heatherton	17-23 Cypress Point Parade "Heatherton, VIC 3202"	Kingston C149 002znMap02 Exhibition
Heatherton	2-6 Sunningdale Way "Heatherton, VIC 3202"	Kingston C149 002znMap02 Exhibition
Heatherton	Tree Reserve (471285) Pinehurst Way "Heatherton, VIC 3202"	Kingston C149 002znMap02 Exhibition
Heatherton	2-4 Arco Lane "Heatherton, VIC 3202"	Kingston C149 002znMap02 Exhibition
Oakleigh South	28 Dowling Road "Oakleigh South, VIC 3167"	Kingston C149 002znMap02 Exhibition
Oakleigh South	1A Washington Drive "Oakleigh South, VIC 3167"	Kingston C149 002znMap02 Exhibition
Oakleigh South	61-65 Dowling Road "Oakleigh South, VIC 3167"	Kingston C149 002znMap02 Exhibition
Oakleigh South	97 Clarinda Road "Oakleigh South, VIC 3167"	Kingston C149 002znMap02 Exhibition
Oakleigh South	43A Dealing Drive "Oakleigh South, VIC 3167"	Kingston C149 002znMap02 Exhibition
Oakleigh South	40A Sherbrooke Avenue "Oakleigh South, VIC 3167"	Kingston C149 002znMap02 Exhibition

Location	Land /Area Affected	Mapping Reference
Oakleigh South	548-558 Warrigal Road "Oakleigh South, VIC 3167"	Kingston C149 002znMap02 Exhibition
Oakleigh South	10 Vanessa Court "Oakleigh South, VIC 3167"	Kingston C149 002znMap02 Exhibition
Oakleigh South	17 Vanessa Court "Oakleigh South, VIC 3167"	Kingston C149 002znMap02 Exhibition
Oakleigh South, VIC	57-59 Dowling Road "Oakleigh South, VIC, 3167"	Kingston C149 002znMap02 Exhibition
Oakleigh South, VIC	1-17 Carroll Road "Oakleigh South, VIC, 3167"	Kingston C149 002znMap02 Exhibition
Clarinda	5-9 Drushi Court "Clarinda, VIC 3169"	Kingston C149 Map 03ZN Exhibition
Clayton South	525-529 Clayton Road "Clayton South, VIC 3169"	Kingston C149 Map 03ZN Exhibition
Clayton South	13-15 Bemboka Avenue "Clayton South, VIC 3169"	Kingston C149 Map 03ZN Exhibition
Clayton South	69-79 Springs Road "Clayton South, VIC 3169"	Kingston C149 Map 03ZN Exhibition
Clayton South	54 Glenelg Drive "Clayton South, VIC 3169"	Kingston C149 Map 03ZN Exhibition
Clayton South	7-9 Meppel Drive "Clayton South, VIC 3169"	Kingston C149 Map 03ZN Exhibition
Clayton South	65 Botanic Drive "Clayton South, VIC 3169"	Kingston C149 Map 03ZN Exhibition
Clayton South	Reserve (471649) Botanic Drive "Clayton South, VIC 3169"	Kingston C149 Map 03ZN Exhibition
Clayton South	12-14 Sheldon Place "Clayton South, VIC 3169"	Kingston C149 Map 03ZN Exhibition
Clayton South	15 Evergreen Boulevard "Clayton South, VIC 3169"	Kingston C149 Map 03ZN Exhibition
Clayton South	3-4 Ricki Court "Clayton South, VIC 3169"	Kingston C149 Map 03ZN Exhibition
Clayton South	123 Rosebank Avenue "Clayton South, VIC 3169"	Kingston C149 Map 03ZN Exhibition
Clayton South	9-13 Scott Avenue "Clayton South, VIC 3169"	Kingston C149 Map 03ZN Exhibition
Clayton South	515-521 Clayton Road "Clayton South, VIC 3169"	Kingston C149 Map 03ZN Exhibition
Clayton South	1A Kitson Road "Clayton South, VIC 3169"	Kingston C149 Map 03ZN Exhibition
Clayton South	64-72 Main Road "Clayton South, VIC 3169"	Kingston C149 Map 03ZN Exhibition

Location	Land /Area Affected	Mapping Reference
Clayton South	Lot 1 (458553) Main Road "Clayton South, VIC 3169"	Kingston C149 Map 03ZN Exhibition
Clayton South	136-176 Osborne Avenue "Clayton South, VIC 3169"	Kingston C149 Map 03ZN Exhibition
Clayton South	5 Larado Place "Clayton South, VIC 3169"	Kingston C149 Map 03ZN Exhibition
Cheltenham	2 Booker Street "Cheltenham, VIC 3192"	Kingston C149 Map 04ZN Exhibition
Cheltenham	6-18 Stanley Avenue "Cheltenham, VIC 3192"	Kingston C149 Map 04ZN Exhibition
Cheltenham	1A Goulbourn Street "Cheltenham, VIC 3192"	Kingston C149 Map 04ZN Exhibition
Mentone	200-204 Balcombe Road "Mentone, VIC 3194"	Kingston C149 Map 04ZN Exhibition
Mentone	1A Phillip Street "Mentone, VIC 3194"	Kingston C149 Map 04ZN Exhibition
Mordialloc	72-76 Jack Holt Way "Mordialloc, VIC 3195"	Kingston C149 Map 05ZN and 07ZN Exhibition
Cheltenham	25 Evergreen Circuit "Cheltenham, VIC 3192"	Kingston C149 Map 05ZN Exhibition
Cheltenham	10 Tintern Grove "Cheltenham, VIC 3192"	Kingston C149 Map 05ZN Exhibition
Cheltenham	131 Wilson Street "Cheltenham, VIC 3192"	Kingston C149 Map 05ZN Exhibition
Cheltenham	1A Dissik Street "Cheltenham, VIC 3192"	Kingston C149 Map 05ZN Exhibition
Cheltenham	27-29 Lorna Street "Cheltenham, VIC 3192"	Kingston C149 Map 05ZN Exhibition
Cheltenham	15-17 Bettina Court "Cheltenham, VIC 3192"	Kingston C149 Map 05ZN Exhibition
Cheltenham	Prop #481453 Evergreen Circuit "Cheltenham, VIC 3192"	Kingston C149 Map 05ZN Exhibition
Cheltenham	30-32 Blackburn Drive "Cheltenham, VIC 3192"	Kingston C149 Map 05ZN Exhibition
Cheltenham	1 Evergreen Circuit "Cheltenham, VIC 3192"	Kingston C149 Map 05ZN Exhibition
Cheltenham	4A Farm Road "Cheltenham, VIC 3192"	Kingston C149 Map 05ZN Exhibition
Cheltenham	50-52 Lincoln Drive "Cheltenham, VIC 3192"	Kingston C149 Map 05ZN Exhibition
Cheltenham	22A Hibiscus Avenue "Cheltenham, VIC 3192"	Kingston C149 Map 05ZN Exhibition

Location	Land /Area Affected	Mapping Reference
Cheltenham	5A Snowdon Drive "Cheltenham, VIC 3192"	Kingston C149 Map 05ZN Exhibition
Cheltenham	41 Nancy Street "Cheltenham, VIC 3192"	Kingston C149 Map 05ZN Exhibition
Cheltenham	1 Derring Lane "Cheltenham, VIC 3192"	Kingston C149 Map 05ZN Exhibition
Cheltenham	8-12 Judd Parade "Cheltenham, VIC 3192"	Kingston C149 Map 05ZN Exhibition
Cheltenham	100 Farm Road "Cheltenham, VIC 3192"	Kingston C149 Map 05ZN Exhibition
Cheltenham	7-9 Elliott Street "Cheltenham, VIC 3192"	Kingston C149 Map 05ZN Exhibition
Heatherton	8-10 Oakmont Crescent "Heatherton, VIC 3202"	Kingston C149 Map 05ZN Exhibition
Mentone	22-24 Mentone Parade "Mentone, VIC 3194"	Kingston C149 Map 05ZN Exhibition
Mordialloc	Lot 1 (493241) Ashmore Avenue "Mordialloc, VIC 3195"	Kingston C149 Map 05ZN Exhibition
Mordialloc	2 Duggan Street "Mordialloc, VIC 3195"	Kingston C149 Map 05ZN Exhibition
Mordialloc	31 Waratah Avenue "Mordialloc, VIC 3195"	Kingston C149 Map 05ZN Exhibition
Mordialloc	59A Brownfield Street "Mordialloc, VIC 3195"	Kingston C149 Map 05ZN Exhibition
Mordialloc	26 Purtell Close "Mordialloc, VIC 3195"	Kingston C149 Map 05ZN Exhibition
Mordialloc	34A Purtell Close "Mordialloc, VIC 3195"	Kingston C149 Map 05ZN Exhibition
Mordialloc	7-9 Hall Mark Road "Mordialloc, VIC 3195"	Kingston C149 Map 05ZN Exhibition
Mordialloc	15 Reckless Lane "Mordialloc, VIC 3195"	Kingston C149 Map 05ZN Exhibition
Mordialloc	8-10 Hall Mark Road "Mordialloc, VIC 3195"	Kingston C149 Map 05ZN Exhibition
Parkdale	4A-4B Meribah Court "Parkdale, VIC 3165"	Kingston C149 Map 05ZN Exhibition
Parkdale	275-295 Nepean Highway "Parkdale, VIC 3195"	Kingston C149 Map 05ZN Exhibition
Parkdale	9 Grey Street "Parkdale, VIC 3195"	Kingston C149 Map 05ZN Exhibition
Parkdale	58A Robert Street "Parkdale, VIC 3195"	Kingston C149 Map 05ZN Exhibition

Location	Land /Area Affected	Mapping Reference
Dingley Village	38-40 Wyndarra Crescent "Dingley Village, VIC 3172"	Kingston C149 Map 06ZN Exhibition
Dingley Village	26-28 Aisha Crescent "Dingley Village, VIC 3172"	Kingston C149 Map 06ZN Exhibition
Dingley Village	8 Newbridge Close "Dingley Village, VIC 3172"	Kingston C149 Map 06ZN Exhibition
Dingley Village	(454295) Fernhill Drive "Dingley Village, VIC 3172"	Kingston C149 Map 06ZN Exhibition
Dingley Village	1A Caruana Drive "Dingley Village, VIC 3172"	Kingston C149 Map 06ZN Exhibition
Dingley Village	2A Wheatland Crescent "Dingley Village, VIC 3172"	Kingston C149 Map 06ZN Exhibition
Dingley Village	13-15 Golfwood Close "Dingley Village, VIC 3172"	Kingston C149 Map 06ZN Exhibition
Dingley Village	8 Village Drive "Dingley Village, VIC 3172"	Kingston C149 Map 06ZN Exhibition
Dingley Village	18-22 Christina Terrace "Dingley Village, VIC 3172"	Kingston C149 Map 06ZN Exhibition
Dingley Village	22 Golfwood Close "Dingley Village, VIC 3172"	Kingston C149 Map 06ZN Exhibition
Dingley Village	31-33 Heathland Way "Dingley Village, VIC 3172"	Kingston C149 Map 06ZN Exhibition
Dingley Village	22A Holland Avenue "Dingley Village, VIC 3172"	Kingston C149 Map 06ZN Exhibition
Dingley Village	41 Kingston Drive "Dingley Village, VIC 3172"	Kingston C149 Map 06ZN Exhibition
Dingley Village	10 Heathland Way "Dingley Village, VIC 3172"	Kingston C149 Map 06ZN Exhibition
Dingley Village	Rear 110-116 Howard Road "Dingley Village, VIC 3172"	Kingston C149 Map 06ZN Exhibition
Dingley Village	14 Seaton Drive "Dingley Village, VIC 3172"	Kingston C149 Map 06ZN Exhibition
Dingley Village	12-14 Greenwoods Close "Dingley Village, VIC 3172"	Kingston C149 Map 06ZN Exhibition
Dingley Village	18 Cannery Place "Dingley Village, VIC 3172"	Kingston C149 Map 06ZN Exhibition
Dingley Village	619-623 Lower Dandenong Road "Dingley Village, VIC 3172"	Kingston C149 Map 06ZN Exhibition
Dingley Village	359 Spring Road "Dingley Village, VIC 3172"	Kingston C149 Map 06ZN Exhibition
Dingley Village	26 Golf Links Crescent "Dingley Village, VIC 3172"	Kingston C149 Map 06ZN Exhibition

Location	Land /Area Affected	Mapping Reference
Dingley Village	13A Teralba Close "Dingley Village, VIC 3172"	Kingston C149 Map 06ZN Exhibition
Dingley Village	8 Williams Close "Dingley Village, VIC 3172"	Kingston C149 Map 06ZN Exhibition
Dingley Village	Tree Reserve (451696) Howard Road "Dingley Village, VIC 3172"	Kingston C149 Map 06ZN Exhibition
Dingley Village	15 Balcombe Place "Dingley Village, VIC 3172"	Kingston C149 Map 06ZN Exhibition
Aspendale	89 Ebb Street "Aspendale, VIC 3195"	Kingston C149 Map 07ZN Exhibition
Aspendale	57 Ebb Street "Aspendale, VIC 3195"	Kingston C149 Map 07ZN Exhibition
Aspendale	6A Gnotuk Avenue "Aspendale, VIC 3195"	Kingston C149 Map 07ZN Exhibition
Edithvale	206 Station Street "Edithvale, VIC 3196"	Kingston C149 Map 07ZN Exhibition
Mordialloc	31 Albert Street "Mordialloc, VIC 3195"	Kingston C149 Map 07ZN Exhibition
Mordialloc	65-85 Governor Road "Mordialloc, VIC 3195"	Kingston C149 Map 07ZN Exhibition
Aspendale Gardens	17-25 Winners Circle "Aspendale Gardens, VIC 3195"	Kingston C149 Map 07ZN Exhibition
Aspendale Gardens	19 Clair Court "Aspendale Gardens, VIC 3195"	Kingston C149 Map 07ZN Exhibition
Chelsea Heights	1A Nimrod Rise "Chelsea Heights, VIC 3196"	Kingston C149 Map 08ZN Exhibition
Chelsea Heights	21 Amaroo Drive "Chelsea Heights, VIC 3196"	Kingston C149 Map 08ZN Exhibition
Chelsea Heights	50-58 Enterprize Avenue "Chelsea Heights, VIC 3196"	Kingston C149 Map 08ZN Exhibition
Chelsea Heights	30-38 Sixth Avenue "Chelsea Heights, VIC 3196"	Kingston C149 Map 08ZN Exhibition
Chelsea Heights	31-33 Talab Court "Chelsea Heights, VIC 3196"	Kingston C149 Map 08ZN Exhibition
Chelsea Heights	7-9 Endeavour Drive "Chelsea Heights, VIC 3196"	Kingston C149 Map 08ZN Exhibition
Chelsea Heights	22-24 Linton Close "Chelsea Heights, VIC 3196"	Kingston C149 Map 08ZN Exhibition
Chelsea Heights	73 Jabiru Drive "Chelsea Heights, VIC 3196"	Kingston C149 Map 08ZN Exhibition
Chelsea	3-5 Showers Avenue "Chelsea, VIC 3196"	Kingston C149 Map 08ZN Exhibition

Location	Land /Area Affected	Mapping Reference
Edithvale	Reserve Randall Avenue "Edithvale, VIC 3196"	Kingston C149 Map 08ZN Exhibition
Edithvale	47A Lochiel Avenue "Edithvale, VIC 3196"	Kingston C149 Map 08ZN Exhibition
Edithvale	Reserve 51 Hughes Avenue "Edithvale, VIC 3196"	Kingston C149 Map 08ZN Exhibition
Edithvale	45A Randall Avenue "Edithvale, VIC 3196"	Kingston C149 Map 08ZN Exhibition
Waterways	28-36 Macquarie Circle "Waterways, VIC 3195"	Kingston C149 Map 08ZN Exhibition
Waterways	Reserve (496004) Lake King Circle "Waterways, VIC 3195"	Kingston C149 Map 08ZN Exhibition
Waterways	22 Macquarie Circle "Waterways, VIC 3195"	Kingston C149 Map 08ZN Exhibition
Waterways	12-16 Princetown Drive "Waterways, VIC 3195"	Kingston C149 Map 08ZN Exhibition
Waterways	5-7 Broadwater Drive "Waterways, VIC 3195"	Kingston C149 Map 08ZN Exhibition
Waterways	5-7 Shoalwater Drive "Waterways, VIC 3195"	Kingston C149 Map 08ZN Exhibition
Waterways	2 Paragon Way "Waterways, VIC 3195"	Kingston C149 Map 08ZN Exhibition
Waterways	1-3 Shoalwater Drive "Waterways, VIC 3195"	Kingston C149 Map 08ZN Exhibition
Waterways	31 Coorong Circle "Waterways, VIC 3195"	Kingston C149 Map 08ZN Exhibition
Waterways	7 Coorong Circle "Waterways, VIC 3195"	Kingston C149 Map 08ZN Exhibition
Waterways	2-36 Tyrrell Terrace "Waterways, VIC 3195"	Kingston C149 Map 08ZN Exhibition
Waterways	3 Mitta Avenue "Waterways, VIC 3195"	Kingston C149 Map 08ZN Exhibition
Waterways	26 Portland Place "Waterways, VIC 3195"	Kingston C149 Map 08ZN Exhibition
Waterways	Reserve South (495992) Lake King Circle "Waterways, VIC 3195"	Kingston C149 Map 08ZN Exhibition
Aspendale Gardens	11 Ferntree Grove "Aspendale Gardens, VIC 3196"	Kingston C149 Map 08ZN Exhibition
Aspendale Gardens	18 Amott Court "Aspendale Gardens, VIC 3195"	Kingston C149 Map 08ZN Exhibition
Aspendale Gardens	56A Kearney Drive "Aspendale Gardens, VIC 3195"	Kingston C149 Map 08ZN Exhibition

Location	Land /Area Affected	Mapping Reference
Aspendale Gardens	12-14 Kerr Crescent "Aspendale Gardens, VIC 3195"	Kingston C149 Map 08ZN Exhibition
Aspendale Gardens	25A Branagan Drive "Aspendale Gardens, VIC 3195"	Kingston C149 Map 08ZN Exhibition
Aspendale Gardens	12A Jacqueline Drive "Aspendale Gardens, VIC 3195"	Kingston C149 Map 08ZN Exhibition
Aspendale Gardens	47 Nurten Parade "Aspendale Gardens, VIC 3195"	Kingston C149 Map 08ZN Exhibition
Aspendale Gardens	44A Branagan Drive "Aspendale Gardens, VIC 3195"	Kingston C149 Map 08ZN Exhibition
Aspendale Gardens	18 Atherton Close "Aspendale Gardens, VIC 3195"	Kingston C149 Map 08ZN Exhibition
Aspendale Gardens	77-83 Nurten Parade "Aspendale Gardens, VIC 3195"	Kingston C149 Map 08ZN Exhibition
Aspendale Gardens	10-20 Palm Grove Boulevard "Aspendale Gardens, VIC 3195"	Kingston C149 Map 08ZN Exhibition
Aspendale Gardens	82-92 Branagan Drive "Aspendale Gardens, VIC 3195"	Kingston C149 Map 08ZN Exhibition
Aspendale Gardens	27A Lance Close "Aspendale Gardens, VIC 3195"	Kingston C149 Map 08ZN Exhibition
Aspendale Gardens	208 Wells Road "Aspendale Gardens, VIC 3195"	Kingston C149 Map 08ZN Exhibition
Aspendale Gardens	35-39 Pacific Drive "Aspendale Gardens, VIC 3195"	Kingston C149 Map 08ZN Exhibition
Aspendale Gardens	14-16 Jackie Court "Aspendale Gardens, VIC 3195"	Kingston C149 Map 08ZN Exhibition
Aspendale Gardens	21 Denahy Court "Aspendale Gardens, VIC 3195"	Kingston C149 Map 08ZN Exhibition
Aspendale Gardens	68-72 Pacific Drive "Aspendale Gardens, VIC 3195"	Kingston C149 Map 08ZN Exhibition
Aspendale Gardens	25 Nurten Parade "Aspendale Gardens, VIC 3195"	Kingston C149 Map 08ZN Exhibition
Aspendale Gardens	15 Wildoer Drive "Aspendale Gardens, VIC 3195"	Kingston C149 Map 08ZN Exhibition
Carrum	5 Dyson Road "Carrum, VIC 3197"	Kingston C149 Map 09ZN and 10ZN Exhibition
Bonbeach	43-45 Fowler Street "Bonbeach, VIC 3196"	Kingston C149 Map 09ZN Exhibition
Bonbeach	86 Mascot Avenue "Bonbeach, VIC 3196"	Kingston C149 Map 09ZN Exhibition
Bonbeach	10 Coast Banksia Drive "Bonbeach, VIC 3196"	Kingston C149 Map 09ZN Exhibition

Location	Land /Area Affected	Mapping Reference
Bonbeach	Rear 12 Mascot Avenue "Bonbeach, VIC 3196"	Kingston C149 Map 09ZN Exhibition
Bonbeach	26 Coast Banksia Drive "Bonbeach, VIC 3197"	Kingston C149 Map 09ZN Exhibition
Bonbeach	70 Coast Banksia Drive "Bonbeach, VIC 3197"	Kingston C149 Map 09ZN Exhibition
Chelsea	87 Argyle Avenue "Chelsea, VIC 3196"	Kingston C149 Map 09ZN Exhibition
Chelsea Heights	11A Puffin Close "Chelsea Heights, VIC 3196"	Kingston C149 Map 10ZN Exhibition
Chelsea Heights	43-49 Fielding Drive "Chelsea Heights, VIC 3196"	Kingston C149 Map 10ZN Exhibition
Chelsea Heights	9 Zeus Court "Chelsea Heights, VIC 3196"	Kingston C149 Map 10ZN Exhibition
Chelsea Heights	Lot A (Prop No. 445153) Fielding Drive "Chelsea Heights, VIC 3196"	Kingston C149 Map 10ZN Exhibition
Patterson Lakes	15-17 Harbour Drive "Patterson Lakes, VIC 3197"	Kingston C149 Map 10ZN Exhibition
Patterson Lakes	2A Gladesville Boulevard "Patterson Lakes, VIC 3197"	Kingston C149 Map 10ZN Exhibition
Patterson Lakes	11 William Salthouse Way "Patterson Lakes, VIC 3197"	Kingston C149 Map 10ZN Exhibition
Patterson Lakes	48 Wetland Drive "Patterson Lakes, VIC 3197"	Kingston C149 Map 10ZN Exhibition
Patterson Lakes	14-16 Portal Place "Patterson Lakes, VIC 3197"	Kingston C149 Map 10ZN Exhibition
Patterson Lakes	56 Wetland Drive "Patterson Lakes, VIC 3197"	Kingston C149 Map 10ZN Exhibition
Patterson Lakes	Reserve Schooner Bay Drive "Patterson Lakes, VIC 3197"	Kingston C149 Map 10ZN Exhibition
Patterson Lakes	21 Boloka Court "Patterson Lakes, VIC 3197"	Kingston C149 Map 10ZN Exhibition
Patterson Lakes	30A Snapper Point Drive "Patterson Lakes, VIC 3197"	Kingston C149 Map 10ZN Exhibition
Patterson Lakes	36 Myola Street "Patterson Lakes, VIC 3197"	Kingston C149 Map 10ZN Exhibition
Patterson Lakes	31 Arrunga Court "Patterson Lakes, VIC 3197"	Kingston C149 Map 10ZN Exhibition
Patterson Lakes	16 Manhattan Court "Patterson Lakes, VIC 3197"	Kingston C149 Map 10ZN Exhibition
Patterson Lakes	2A Lagoon Place "Patterson Lakes, VIC 3197"	Kingston C149 Map 10ZN Exhibition

Location	Land /Area Affected	Mapping Reference
Patterson Lakes	22 Kalang Court "Patterson Lakes, VIC 3197"	Kingston C149 Map 10ZN Exhibition
Patterson Lakes	3A Acacia Crescent "Patterson Lakes, VIC 3197"	Kingston C149 Map 10ZN Exhibition
Patterson Lakes	11 Balarang Court "Patterson Lakes, VIC 3197"	Kingston C149 Map 10ZN Exhibition
Patterson Lakes	52 Palm Beach Drive "Patterson Lakes, VIC 3197"	Kingston C149 Map 10ZN Exhibition
Patterson Lakes	Reserve 14 Illawong Court "Patterson Lakes, VIC 3197"	Kingston C149 Map 10ZN Exhibition
Patterson Lakes	9A Erinka Crescent "Patterson Lakes, VIC 3197"	Kingston C149 Map 10ZN Exhibition
Patterson Lakes	27 Colac Court "Patterson Lakes, VIC 3197"	Kingston C149 Map 10ZN Exhibition
Patterson Lakes	165 Gladesville Boulevard "Patterson Lakes, Vic 3197"	Kingston C149 Map 10ZN Exhibition
Patterson Lakes	131 Gladesville Boulevard "Patterson Lakes, VIC 3197"	Kingston C149 Map 10ZN Exhibition
Patterson Lakes	Reserve 3 Wetland Drive "Patterson Lakes, VIC 3197"	Kingston C149 Map 10ZN Exhibition
Patterson Lakes	Reserve 4 Wetland Drive "Patterson Lakes, VIC 3197"	Kingston C149 Map 10ZN Exhibition
Patterson Lakes	42-44 Palm Beach Drive "Patterson Lakes, VIC 3197"	Kingston C149 Map 10ZN Exhibition
Patterson Lakes	2A Shell Court "Patterson Lakes, VIC 3197"	Kingston C149 Map 10ZN Exhibition
Bonbeach	2-4 Williams Grove "Bonbeach, VIC, 3195"	Kingston C149 009zn Map09 Exhibition
Chelsea	2 Thames Promenade "Chelsea, VIC 3196"	Kingston C149 008zn Map08 Exhibition
Bonbeach	1/2-4 Williams Grove "Bonbeach, VIC 3195"	Kingston C149 009zn Map09 Exhibition
Bonbeach	2/2-4 Williams Grove "Bonbeach, VIC 3195"	Kingston C149 009zn Map09 Exhibition
Bonbeach	538-539 Nepean Highway "Bonbeach, VIC 3196"	Kingston C149 009zn Map09 Exhibition
Edithvale	256-260 Station Street "Edithvale, VIC 3196"	Kingston C149 008zn Map08 Exhibition
Highbett	152 Wickham Road "Highbett, VIC 3190"	Kingston C149 001zn Map01 Exhibition
Moorabbin	4 Kingsway "Moorabbin, VIC 3189"	Kingston C149 001zn Map01 Exhibition

Location	Land /Area Affected	Mapping Reference
Patterson Lakes	100 Gladesville Boulevard "Patterson Lakes, VIC 3197"	Kingston C149 010zn Map10 Exhibition
Edithvale	3 & 4 Waters Edge Place "Edithvale, VIC 3196"	Kingston C149 007zn Map07 Exhibition
Clayton South	26A Audsley Street "Clayton South, VIC 3169"	Kingston C149 003zn Map03 Exhibition
Patterson Lakes	14A 102-114 Gladesville Boulevard "Patterson Lakes, VIC 3197"	Kingston C149 010zn Map10 Exhibition
Aspendale	1 Station Street, Aspendale Vic 3195	Kingston C149 011d-hoMap07 Exhibition and Kingston C149 012hoMap07 Exhibition
Chelsea	Rear of 440-443 Nepean Highway, Chelsea VIC 3196	Kingston C149 023d-paoMap09 Exhibition
Heatherton	466 Warrigal Road, Heatherton VIC 3203	Kingston C149 024d-paoMap02 Exhibition
Heatherton	Lot 1 Warrigal Road, Heatherton VIC 3203	Kingston C149 024d-paoMap02 Exhibition
Aspendale	34 Larnook Crescent, Aspendale VIC 3195	Kingston C149 016d-esoMap07 Exhibition
Browns Reserve Aspendale	Mordialloc Creek, Browns Reserve Aspendale VIC 3195	Kingston C149 019d-esoMap07 Exhibition
Aspendale	55 Nepean Highway, Aspendale VIC 3195	Kingston C149 019d-esoMap07 Exhibition
Carrum	16A Canberra Street, Carrum VIC 3197	Kingston C149 020d-esoMap10 Exhibition
Chelsea	3 Swan Walk, Chelsea VIC 3196	Kingston C149 021d-esoMap08 Exhibition
VIC	1-3 Lynne Street, Chelsea Heights, VIC 3196	Kingston C149 022d-esoMap08 Exhibition
Mentone	4 Rimmer Street, Mentone VIC 3194	Kingston C149 018d-esoMap04 Exhibition
Mentone	18-36 Levanto Street, Mentone VIC 3194	Kingston C149 015d-dpoMap05 Exhibition
Moorabbin	944-954 Nepean Highway, Moorabbin VIC 3189	Kingston C149 013hoMap01 Exhibition
Edithvale	8-12 Lochiel Avenue, Edithvale VIC 3196	Kingston C149 017d-esoMap07 Exhibition and Kingston C149 014ho Map07 Exhibition
Bonbeach	1 The Fairway, Bonbeach VIC 3196	Update to the Schedule to Heritage Overlay

Ordinary Meeting of Council

10 December 2018

Agenda Item No: 8.5

MORDIALLOC FREEWAY ENVIRONMENTAL EFFECTS STATEMENT - COUNCIL SUBMISSION

Contact Officer: Bianca Coughlan, Principal Strategic Planner

Purpose of Report

To provide an update in relation to the Environmental Effects Statement (EES) process for the Mordialloc Bypass (Freeway) project and to seek Council endorsement of a submission prepared by officers.

Disclosure of Officer / Contractor Direct or Indirect Interest

No Council officer/s and/or Contractor/s who have provided advice in relation to this report have declared a Conflict of Interest regarding the matter under consideration.

OFFICER RECOMMENDATION

That Council:

1. Endorse the submission at Appendix 3 in relation the Environmental Effects Statement for the Mordialloc Bypass (Freeway) project.
2. Request to be heard at the Advisory Committee Hearing currently scheduled for early 2019.

1. Executive Summary

On 13 September 2017, the Minister for Planning requested that VicRoads prepare an Environmental Effects Statement (EES) under the *Environment Effects Act 1978* to assess the potential effects of the project. The EES is a detailed study into the potential impacts of the project. It describes potential effects of the Mordialloc Bypass (Freeway) project on the environment and recommends ways to avoid, minimise or manage these impacts.

Since late 2017 Council officers have been involved in the development of the EES for the Mordialloc Freeway project through representation on the Technical Reference Group, which guides the technical investigations and development of the EES.

The EES is currently on public exhibition between 26 October and 14 December 2018. Council received a briefing from the Major Roads Project Authority in relation the EES at CIS on 12 November 2018.

Having now reviewed the EES, it is the view of officers that while the project satisfactorily addresses some of the risks outlined in the scoping requirements set by the Minister for Planning and the Commonwealth there are still a number of matters which are not adequately resolved in the EES documentation. These include:

- The lack of a clear and long term commitment to ongoing monitoring of groundwater levels

- Management of potential contamination issues associated with past land uses and fill material within the project area.
- Negative impacts on local flora and fauna including bird flight paths post construction.
- Groundwater impacts and the potential for any change to groundwater levels to negatively impact on the Ramsar listed Edithvale and Seaford Wetlands.
- Flood impacts including changes to floodplain capacity and stormwater flows post construction.
- Potential negative human health impacts associated with a deterioration in air quality post construction.
- Negative visual and landscape impacts associated with elements of the proposed design including the bridged structure over the Waterways wetland.
- The lack of acoustic barriers adjacent Braeside Park.
- The absence of key strategic connections across, and linking into, the Freeway including the need for a separated crossing of the Dingley Bypass to provide a connection to the proposed shared user path.

It is recommended that a submission be made by Council in relation to a number of outstanding issues that require resolution prior to the commencement of construction.

Concurrent to Council's consideration of the draft submission, a final version of the Council commissioned Visual and Landscape Design – Priority Project Outcomes Report (Spiire) is to be considered by Council. It is proposed that subject to adoption of this report, a copy will be included as part of Council's submission to the EES.

It is noted that Council has passed two Notices of Motion (NOM) on 22 May 2017 (Appendix 1) and 25 September 2017 (Appendix 2) (22/2017 and 44/2017) outlining a range of issues to be resolved through the Mordialloc Freeway project. This adopted position of Council has informed officer's subsequent engagement with VicRoads and the MRPA as well as the review of the released EES documentation as described in this report.

2. Background

On 13 September 2017, the Minister for Planning requested that VicRoads prepare an Environmental Effects Statement (EES) under the *Environment Effects Act 1978* to assess the potential effects of the project. The EES is a detailed study into the potential impacts of the project. It describes potential effects of the Mordialloc Bypass (Freeway) project on the environment and recommends ways to avoid, minimise or manage these impacts.

The EES is on public exhibition between 26 October and 14 December 2018. Following exhibition, an independent Inquiry and Advisory Committee (IAC) will consider the EES and submissions received and provide a report to the Minister for Planning. The inquiry will also consider the proposed planning scheme amendment to insert an Incorporated Document into the Kingston Planning Scheme to facilitate planning approvals for the project. The Minister will then consider the report and issue a written assessment of the project which will provide a statutory framework to guide decision makers responsible for issuing approvals for the project.

The detailed design and construction of the project will be authorised and regulated through Incorporated Documents that will be inserted into the Kingston Planning Scheme. These Incorporated Documents set the requirements for an Environmental Management Framework (EMF) which will be approved by the Minister for Planning. The EMF includes a comprehensive list of Environmental Performance Requirements (EPRs) which have been determined through the development of the EES and can be found in chapter 23 of the EES.

Concurrent to the EES process, officers have engaged Spiire (Landscape Architects) to undertake a critical review of the landscape and urban design effects and the visual impact of the Mordialloc Freeway. The outcomes of this work and the report prepared by Spiire are addressed in the submission (Appendix 3) and broadly described in Section 3 below. It is noted that this landscape and visual submission is being considered concurrently to this report and the final version will be attached to the submission when lodged, subject to Council approval.

Council Notices of Motion

Council has passed two Notices of Motion (NOM) on 22 May 2017 and 25 September 2017 (22/2017 and 44/2017) outlining a range of issues to be resolved collaboratively with VicRoads.

Council officer's review of the EES documentation has been informed by the content of these NOM's and consideration as to the extent to which matters raised therein have been satisfactorily resolved. The landscape and visual impact assessment commissioned by officers has also sought to address and respond to a range of matters identified within the NOM's as resolved by Council.

3. Discussion

3.1 Proposed Council Submission

It is recommended that a submission be made by Council in relation to a number of outstanding issues and concerns that need to be resolved before construction begins. These matters are summarised below and are further expanded on within the draft letter and submission (Appendix 3).

Concurrent to Council's consideration of this submission Council will be asked to consider the final version of the Council commissioned Visual and Landscape Design – Priority Project Outcomes Report (Spiire) through the December Meeting cycle. It is proposed that subject to adoption of this report, a copy will be included as part of Council's submission to the EES.

Contamination

A number of known potential contamination issues exist within or surrounding the project work area. It is Council's view that the following matters require further consideration:

- Landfill:
The EPA has identified significant landfill gas generation at the Dinsan landfill, and issues with the adequacy of the landfill cap. The landfill is not lined, which has implications for landfill gas migration. Any rehabilitation or maintenance works associated with the landfill will be complicated by the presence of a freeway in proximity of that landfill. Further the freeway, and associated services, such as drainage lines and pits, will offer potential migration pathways for landfill gas. This will raise challenges in relation to containment and control of pollutants and consequential risks associated with access to in ground services from maintenance workers on the freeway and surrounding environment. Similar issues are likely to exist in relation other filled sites along the corridor and further consideration of the long term risk of gas migration is required.

- PFAs (Per and poly-fluoralkyl substances):

The EES has identified PFAS in sediments and groundwater in and around the environs of the project. The level of knowledge, monitoring requirements and government response in relation PFAS is evolving.

PFAS also has potential consequences for surrounding land uses in relation to impacts upon people and the environment. It is considered that EPR CL6 should be amended to ensure that Council is not responsible for any clean up resulting from this issue.

- Asbestos

Asbestos is a potential material of concern that has been identified during sampling and is likely to be further identified across the Project. There is potential for asbestos to be located within the project construction corridor which, if found, will require removal and disposal. Council notes there is no specific EPR in relation to asbestos, and considers that this should be addressed accordingly.

- Soils and Contaminated Land

Contaminated soil and acid sulphate soil has been identified within the Project area. There are potential implications for Council in relation to storage of these materials in both the short and long term. In the short term there will be temporary storage requirements with the associated risk of mishap and discharge associated with both stormwater runoff and/or windblown dust. If contaminated soil is reused there will be ongoing management requirements to ensure integrity of containment. Council seeks greater clarity in relation to planned mitigation measures and confirmation as to who will bear ongoing responsibility.

Flooding

The EES has identified that stormwater ponding and flow will be impacted by the project. It is considered that both the hydrogeology and hydrology impacts on the project have not been fully explored.

- Flood Plain capacity: The presence of the elevated freeway within a designated flood plain, will reduce the storage capacity and result in slightly higher 1 in 100 modelled flood levels. This has implications for inundation of properties in and around the project.
- Stormwater flows: The Project footprint will impact upon localised directional flows. Increased hard surface from the roadways will place extra load upon the stormwater system, and the ability for other premises downstream to discharge stormwater in the event the stormwater system is approaching capacity. Again this may have implications for localised inundation downstream of the project.

The EES has identified the above and recommends liaising with responsible authorities including Council, Council considers that this matter should be addressed further prior to construction commencing.

Council infrastructure

It is possible Council will have assets potentially impacted by changes to pollutant or other environmental elements arising from the project (eg stormwater or changed landfill gas flows). These impacts are yet to be fully explored and no measures are in place to ensure that Council is not burdened with significant infrastructure renewal costs post construction. Council requires further information in relation to any impact on Council infrastructure prior to construction commencing.

Project Area Impact Zone

The EES has examined an area extending 150m beyond the Project footprint for land contamination impacts. The basis for this 150m impact area has not been documented.

EPA Publication 788 *Best Practice Environmental Management Siting, Design, Operation and Rehabilitation of Landfills* (the BPEM) recommends a minimum buffer from buildings and structures of 500m for putrescible landfills and 200m for solid inert landfills. The Project is known to sit upon a solid inert landfill so 200m would be the anticipated impact zone rather than the 150m applied.

The project area is within a sand belt area and as such the ability of pollutants and groundwater to migrate across the sand belt is large compared with clay and other type media, suggesting the use of more conservative estimates.

Fill

The project is identified as requiring 1,500,000m³ of fill material during the construction phase. The demand for fill may have implications upon the ability of other projects (private sector, State and Local Government) within the municipality to meet their needs with associated delays for completion of works and increased project costs. Possible examples include the rehabilitation and capping of former landfills and former extractive industry sites. Council would be interested to understand if this potential issue has been considered as part of the EES process and would welcome the opportunity to further discuss with the MRPA.

Construction Impacts - Wetlands

Construction over or in close proximity to wetlands will result in increased hazards during construction.

1. The management of spoil material bearing acid sulphate soils, and potential contamination of near surface sediments, arising from past industrial practices in upper reaches of Mordialloc creek during piling and other operations.
2. The management of and response to accidents and spills and the ability of responders to contain and manage any clean up in a wetland environment.

These issues have potential in the short term to impact upon significant environmental assets in the municipality including the Ramsar listed Edithvale and Seaford Wetlands. Council seeks greater clarity in relation planned mitigation measures and roles and responsibilities during construction.

Groundwater

Section 17 of the EES describes potential groundwater quality and flow impacts associated with the project. The EES acknowledges that the project '*...could affect groundwater availability through changes to groundwater levels, flows and recharge resulting from excavations and compressions of soils*' and could impact groundwater quality through '*...spills, stormwater runoff or changes to groundwater flows.*'

Given the international environmental significance of the Ramsar listed Edithvale Wetlands, it is Council's view that the work undertaken to date has not adequately considered or responded to the risks presented by potential groundwater impacts.

It is further noted that the groundwater in the vicinity of the project may be impacted from off-site pollution. The review of the EES failed to identify the author having undertaken an examination of whether the project area abuts intersects declared groundwater quality restricted use zones or known contaminated groundwater plumes. This work is considered necessary in the event that groundwater changes occur as a result of the project.

Noise and Vibration

The EES has identified noise as an issue through both construction and operational periods. The EES proposes to deal with noise through engineering controls (sound barriers) and good practices (hours of operation), being codified within works plans. Notwithstanding the fact that the risk has been assessed as low, it is considered that this could be strengthened in relation to both human health and fauna impacts. For example, it is the view of both Council and Parks Victoria that the provision of acoustic barriers is required along the length of Braeside Park to protect this sensitive natural environment.

Air emissions

The EES identifies that dust emissions from the construction project is modelled to be within 60 metres of source and up to 100 metres. It is anticipated that there will be localised impacts, with associated community concerns. It is not clear how these concerns will be managed, and what mitigation measures can be utilised. It is considered that the relevant EPR should be strengthened to include this as well as addressing any impact on human health.

Traffic and Transport

Council has a number of concerns relating to the overall impact on the traffic network, including the known impacts on South Road. Council notes that EPR T2 requires the preparation of a Traffic Management Plan and considers that consultation on this plan should begin well in advance of construction commencing.

Biodiversity (Flora and Fauna Impacts)

While a number of EPR's address the impacts on flora and fauna, and talk to a number of mitigation measures including fauna barriers and fauna sensitive lighting design, it is considered that further matters including flight paths, migration patterns and vegetation loss (including within the Waterways wetlands, Mordialloc Creek bridge and Edithvale and Seaford Wetlands) have not been fully considered or appropriately addressed.

A particular concern of Council is in relation the flight paths of migratory birds that exist currently within the Freeway reserve. It is noted that many of the bird species that inhabit the Edithvale and Seaford Wetlands are regularly observed within the wetlands in and surrounding the Freeway reserve.

Landscape and Visual Impact

Officers have met regularly with VicRoads and the MRPA since early 2017 to provide feedback and input on the landscape and visual outcomes of the project. It is considered that a number of matters relating to Landscape and Visual outcomes could be further strengthened to ensure that the project results in an exemplar outcome that achieves sensitive integration with the region. A number of the EPRs relating to Landscape and Visual are of a very general nature and would benefit from the inclusion of specific outcomes/deliverables.

Advocacy

It is also considered that the project presents an opportunity to advocate for key outcomes of local and regional significance. In addition to the work being undertaken by Spiire, Council officers consider it appropriate to request consideration of the following matters:

- The addition of a “purple pipe” to ensure future access to recycled water and appropriate IWM measures.
- Provision of a wetland near Chadwick reserve to clean stormwater and provide treated water for sportsground irrigation. It is also considered that a number of enhancements to this reserve could be provided as part of the project including the potential provision of an expanded area of open space.
- The opportunity for a large scale solar installation on land abutting the freeway alignment.

3.2 Council Plan Alignment

Goal 1 - Our well-planned, liveable city supported by infrastructure to meet future needs

Direction 1.3 - Infrastructure and property investment for a functional city now and into the future

The project will improve traffic movements and connect the Mornington Peninsula Freeway to the Dingley Bypass.

3.3 Consultation/Internal Review

Council officers have had involvement in the development of the EES. Primarily, this has occurred through representation on the Technical Reference Group, which guides the investigations and development of the EES. The Technical Reference Group met eight times between 30 November 2017 and 14 June 2018. Council officers have also been directly engaged on specific issues.

Councillors were provided with briefings from the MRPA on the 4 June 2018 and 12 November 2018.

During the EES exhibition period, the MRPA has undertaken consultation with key stakeholders including community members, community groups, industry groups, government representatives, local government and utilities. A full outline of the consultation undertaken is provided in chapter 7 of the EES.

The EES is on public exhibition between 26 October and 14 December 2018. The EES documentation is available to view on the MRPA website, as well as in person at the State Library of Victoria, Kingston City Council’s Municipal Office, and Chelsea Library. A number of drop in information sessions are being held at the Mordialloc Freeway Info Hub in Dingley Village. The public is invited to make written submissions to DELWP via their website and hardcopy.

3.4 Resources

It is proposed that Council submit a request to be heard through the Advisory Committee process. It is noted that this would involve Council appointing legal representation to present its submission through the Advisory Committee Hearing including the appointment of a number of experts to ensure Council is adequately represented and an effective voice on behalf of its community.

It is anticipated that the costs associated with this hearing will be in the vicinity of \$140,000 - \$150,000 comprising:

- Preparation by Legal representation: \$25,000
- Legal representation at the Advisory Committee Hearing for 10 days (noting that this hearing may extend to three weeks) at \$3,000 per day
- Engagement of a range of expert witnesses at a cost of approximately \$80,000 - \$90,000 comprising the fees for report research and writing as well as presentation through the hearing process.

The EES process provides for important scrutiny of a highly technical project and it is important that Council engages appropriate advocates and experts to critique and provide input into the projects delivery and legacy which once completed it leaves.

4. Conclusion

At this stage, Council officers, while broadly satisfied with the general process and rigour of the EES, consider that a number of significant matters are required to be further explored through the hearing process. It is recommended that the submission at Appendix 3 be lodged and that Council requests to be heard at the hearing.

Appendices

Appendix 1 - Notice of Motion 22 May 2017 (Ref 18/616424) [↓](#)

Appendix 2 - Notice of Motion 25 September 2017 (Ref 18/616426) [↓](#)

Appendix 3 - Mordialloc Bypass Council submission with attachments (Ref 18/623564) [↓](#)

Author/s: Bianca Coughlan, Principal Strategic Planner
Reviewed and Approved By: Paul Marsden, Manager City Strategy
Jonathan Guttman, General Manager Planning and Development

8.5

MORDIALLOC FREEWAY ENVIRONMENTAL EFFECTS STATEMENT - COUNCIL SUBMISSION

1	Notice of Motion 22 May 2017.....	161
2	Notice of Motion 25 September 2017	163
3	Mordialloc Bypass Council submission with attachments	165

**City of Kingston
Ordinary Meeting of Council**

Minutes**22 May 2017****Procedural Motion****Moved: Cr Bearsley****Seconded: Cr Hua**

That Cr Bearsley be granted an extension of time to speak on the matter

CARRIED**Procedural Motion****Moved: Cr Oxley****Seconded: Cr West**

That Cr Barth be granted an extension of time to speak on the matter

CARRIED**The Substantive motion was put and CARRIED****A Division was Called:****DIVISION:****FOR:** Crs Brownlees, Staikos, West, Eden, Barth and Oxley (6)**AGAINST:** Nil (0)**ABSTAINED:** Crs Bearsley, Gledhill and Hua (3)**CARRIED****12.6 Notice of Motion No. 22/2017 - Cr West - Mornington Peninsula Freeway Extension Announcement****Moved: Cr West****Seconded: Cr Staikos**

That Council:

1. Ask officers to provide a report on the feasibility study and business plan, when they are released, and their key findings.
2. Council respectfully request the State Government to consider and provide a report to the Council and community on the potential impact of the MPFE on:
 - a) Braeside Park and the wetlands and heritage buildings on the park,
 - b) the Kingston Green Wedge,
 - c) environmental, horticultural and Aboriginal Cultural Heritage issues, and
 - d) the amenity and road access of the residents of Aspendale Gardens (east of Wells Road), Richfield Retirement Village, Waterways, Dingley Village and the Green Wedge.
 - e) Whether the freeway will need to be constructed as an elevated freeway (Skyroad) through the Waterways and Braeside Park wetlands or any other part of the route
3. Council asks State Government to:
 - a) undertake an Environmental Effects Statement investigation into any potential impact that may be caused by the MPFE, especially to the Braeside Park and

**City of Kingston
Ordinary Meeting of Council**

Minutes**22 May 2017**

Waterways Wetlands and ensure that any negative impact is minimised;

- b) Give greater priority to the extension of Westall Road to improve motorists' access from the Dandenong Bypass to the Monash Freeway, before commencing the Mornington Peninsula Freeway extension.
- c) ensure that an overpass is constructed at or close to the intersection of the MPFE with the Dingley Bypass to allow continuity for passage of cyclists, pedestrians and equestrians along the Chain of Parks trail, whose route is currently proposed to end in a T-junction at the Dingley Bypass.
- d) ensure that the impact on Kingston's market gardens is minimised.
- e) work with Council to plan for and fund the construction of wetlands on land adjoining the Settlement Creek and Dunlops Creek and on the Green Wedge triangle abutting Springvale Road.
- f) consider the formation of a community reference group to guide decisions about reviewing and implementing plans for the MPFE.

Cr Oxley left the meeting at 10.09pm

Cr Brownlees left the meeting at 10.09pm

Cr Oxley returned to the meeting at 10.10pm

Cr Hua left the meeting at 10.11pm

Cr Gledhill left the meeting at 10.12pm

Cr Brownlees returned to the meeting at 10.13pm

Cr Gledhill returned to the meeting at 10.14pm

Cr Hua returned to the meeting at 10.15pm

CARRIED

13. Urgent Business

There were no items of urgent business.

**City of Kingston
Ordinary Meeting of Council**

Minutes**25 September 2017****12.3 Notice of Motion No. 40/2017 - Cr West - Mentone and Cheltenham Plans**

The item was withdrawn by Cr West.

12.4 Notice of Motion No. 41/2017 - Cr Staikos - Mordialloc Bypass**Moved: Cr Staikos****Seconded: Cr West**

1. Council officers brief Councillors on the matters raised in the Notice of Motion 12.6 that was carried at the May 2017 ordinary Council Meeting,
2. Council works collaboratively with VicRoads to resolve the following issues in relation to the Mordialloc Bypass:
 - a. Council supports the establishment of acoustic fence treatments on both sides of the Mordialloc bypass, with proper vegetation screening to protect the amenity of, Aspendale Gardens, Dingley Village and Waterways residents as well as the amenity of Braeside Park and the Waterways and Woodlands wetlands.
 - b. Council requests that the acoustic fence be lined with a screen of tall indigenous trees to encourage birds to fly across the freeway at a good height, to reduce the risk of being struck by traffic.
 - c. Council does not support the closure of Old Dandenong Road, between Boundary Road and Tootal Road.
 - d. As part of this project Council requests VicRoads improve the traffic management of Centre Dandenong Road, especially between Boundary Road and Tootal Road, and improve the flow of traffic at the roundabout.
 - e. Council reiterates its position to ensure that an overpass/underpass is constructed at or close to the intersection of the Mordialloc Bypass with the Dingley Bypass to allow continuity for passage of cyclists, pedestrians and equestrians along the Chain of Parks trail.
 - f. Council works with VicRoads and Melbourne Water to ensure the establishment of wetlands along the route of the Mordialloc Bypass.
 - g. That the road be located as far to the west as possible on the reservation where it passes Braeside Park and Dingley Village to make room for and the Chain of Parks shared trail to be on the eastern side of the freeway and to minimise any disruption to the Braeside Park historic buildings and rangers' facilities.
 - h. To relocate any of the Braeside Park historic buildings and rangers' facilities that would still be impacted despite (g).
 - i. That VicRoads offers public information sessions to the residents of Dingley Village, Waterways and Aspendale Gardens (east of Wells Road).

Amendment**Moved: Cr Bearsley****Seconded: Cr Hua**

1. Council officers brief Councillors on the matters raised in the Notice of Motion

**City of Kingston
Ordinary Meeting of Council**

Minutes**25 September 2017**

12.6 that was carried at the May 2017 ordinary Council Meeting,

2. Council works collaboratively with VicRoads to resolve the following issues in relation to the Mordialloc Bypass:
 - a. Council supports the establishment of acoustic fence treatments on both sides of the Mordialloc bypass, with proper vegetation screening to protect the amenity of, Aspendale Gardens, Dingley Village and Waterways residents as well as the amenity of Braeside Park and the Waterways and Woodlands wetlands.
 - b. Council requests that the acoustic fence be lined with a screen of tall indigenous trees to encourage birds to fly across the freeway at a good height, to reduce the risk of being struck by traffic.
 - c. Council does not support the closure of Old Dandenong Road, between Boundary Road and Tootal Road.
 - d. As part of this project Council requests VicRoads improve the traffic management of Centre Dandenong Road, especially between Boundary Road and Tootal Road, and improve the flow of traffic at the roundabout.
 - e. Council reiterates its position to ensure that an overpass/underpass is constructed at or close to the intersection of the Mordialloc Bypass with the Dingley Bypass to allow continuity for passage of cyclists, pedestrians and equestrians along the Chain of Parks trail.
 - f. Council works with VicRoads and Melbourne Water to ensure the establishment of wetlands along the route of the Mordialloc Bypass.
 - g. That the road be located as far to the west as possible on the reservation where it passes Braeside Park and Dingley Village to make room for and the Chain of Parks shared trail to be on the eastern side of the freeway and to minimise any disruption to the Braeside Park historic buildings and rangers' facilities.
 - h. To relocate any of the Braeside Park historic buildings and rangers' facilities that would still be impacted despite (g).
 - i. That VicRoads offers public information sessions to the residents of Dingley Village, Waterways and Aspendale Gardens (east of Wells Road)
 - j. Council requests a fly over at Governor Road

**The Amendment was put and CARRIED
The Amendment became the Motion**

Procedural Motion

Moved: Cr Brownlees Seconded: Cr Gledhill

That the motion now be put.

**CARRIED
The Motion was put and CARRIED**

<insert date>



Mordialloc Bypass (Freeway) Environmental Effects Statement Submissions
Planning Panels Victoria

Via online submission: <https://engage.vic.gov.au/mordialloc-bypass-ees-inquiry>

Dear Sir/Madam

Re: Mordialloc Freeway - Submission on Environmental Effects Statement

Thank you for the opportunity to provide feedback on the Environmental Effects Statement (EES) prepared for the Mordialloc Freeway project (the project). Council officers have previously provided comment on the draft documents and understand that the detailed design and construction of the projects will be authorised and regulated through an Incorporated Document that will be inserted into the Kingston Planning Scheme.

Council officers have reviewed the EES and it is the view of Council that the project generally addresses many of the risks outlined in the scoping requirements set by the Minister for Planning and the Commonwealth. However Council considers that a number of significant matters require further consideration including:

- Negative visual, landscape and urban design impacts associated with elements of the proposed design including the bridge structure over the Waterways wetland.
- The lack of acoustic barriers adjacent Braeside Park.
- The absence or inadequate design of key strategic connections across the freeway and linking into the surrounding path network and destinations, in particular the need for a separated pedestrian crossing over the Dingley Bypass.
- The lack of a clear and long term commitment to ongoing monitoring of groundwater levels.
- Management of potential contamination issues associated with past land uses and fill material within the project area.
- Negative impacts on local flora and fauna including bird flight paths post construction do not appear to have been considered and require further investigation to ensure mitigation measures will be effective.
- Groundwater impacts and the potential for any change to groundwater levels to negatively impact on the Ramsar listed Edithvale and Seaford Wetlands.
- Flood impacts including changes to floodplain capacity and stormwater flows post construction and the implications this will have for existing Council infrastructure.
- Potential negative human health impacts associated with a deterioration in air quality post construction.

In the attached table (Attachment 1) Council has provided relatively detailed (albeit summary style) comment and, where applicable, recommendations as to any changes sought. In respect of these comments and recommendations Council wishes to note the following:

- 1 The significant scope, detail and volume of the material, combined with the relatively short turnaround time between exhibition and submission, has meant that this submission (and Table) are somewhat general in nature and will require refinement leading up to and in preparation for, the EES Hearing;
- 2 Kingston Council intends to secure various expert assistance, with the task of those experts including, to assist Council in refining Councils position on various matters;
- 3 A recurrent issue with a number of the areas for comment is the lack of definitive positions in respect of relevant impacts and design and operational solutions. Rather the intended adoption of various "plans" through the EPR's. Council recognises this issue is not uncommon in projects of

community inspired leadership

TRIM XX/XXXX PAGE 1

kingston.vic.gov.au

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1300 653 356 131 450 03 9581 4500 PO Box 1000, Mentone 3194 info@kingston.vic.gov.au cityofkingston kingstoncc

such scale and a number of Councils current and likely future recommendations go to suggested ways to provide additional surety during design, construction and operational phases.

In addition to the comments within the attached Table, Council notes that the construction of the project provides a unique opportunity for a number of important matters to be considered and incorporated into the project accordingly. These matters are to be considered part of Council's submission and will be elaborated on further through Council's substantive submission to the Independent Advisory Committee. These matters include:

- The addition of a "purple pipe" along the Freeway alignment to provide for future access to recycled water;
- The creation of wetlands near Chadwick Reserve to clean stormwater and provide treated water for sportsground irrigation and associated enhancements to this Reserve; and
- The opportunity for the project to facilitate provision of a large scale solar installation on land adjoining the freeway alignment in accordance with the vision presented in Council's adopted Green Wedge Management Plan (2012).

It is further noted that on 22 May 2017 and 25 September 2017, Council endorsed two Notices of Motion in relation to the project which have also been attached for your reference (Attachment 2). The resolved position of Council in relation the project has informed Council's review of the EES documentation and the commissioning of a review of the landscape, urban design and visual impact of the Mordialloc Freeway. The outcomes of this work are included as Attachment 3 to this letter and are to be considered as part of Council's submission to the EES.

Council would welcome the opportunity to discuss these matters in detail prior to (and during) the hearing. It is considered imperative to build in these initiatives at an early project planning stage rather than attempting to retrofit once the project is constructed.

Please contact Bianca Coughlan; Principal Strategic Planner on 9581 4663 or email bianca.coughlan@kingston.vic.gov.au should you require any further information or clarification of the matters raised.

Regards

Cr Georgina Oxley
MAYOR, CITY OF KINGSTON

Encl.

1. Table – full submission
2. Notices of Motion – 22 May 2017 and 25 September 2017
3. City of Kingston Landscape and Visual Assessment – Priority Project Outcomes (Spiire)

Attachment 1 -Table of issues identified in EES with proposed measures.**Table 1 Summary of issues identified with the EES and proposed measures.**

Commentary on aspect of EES	Potential measure to address concerns
1 PRIORITY PROJECT OUTCOMES FOR THE MUNICIPALITY	
<ul style="list-style-type: none"> • Council has examined the project, and potential environmental effects detailed within the EES, against its strategic plans and aspirations for the municipality. Six priority outcomes, that if not met will have a negative impact upon the municipality and its environs. These are documented within <i>City of Kingston, Mordialloc Bypass, Priority Project Outcomes, November 2018 (The SPIIRE report)</i>. The priority outcomes are: <ol style="list-style-type: none"> 1. The project clearly communicates the story of place. The municipality of Kingston is a region invested in an environmentally progressive future. 2. Links and journeys are well used, integrated, direct, accessible, legible, attractive and safe. 3. Important community connections are maintained and future strategic connections allowed for. 4. Provides and maintains effective short and long term visual screening – noise walls and vegetation. 5. Provides and maintains high quality visually recessive bridge structures. 6. Manages the sensitive interfaces of Braeside Park, Dingley Village, Aspendale Gardens and Waterways communities from 	<ul style="list-style-type: none"> • Adoption through changes to or amendment of Environmental Performance Requirements (EPR) of recommendations of the SPIIRE report include: <ul style="list-style-type: none"> • Outcome 1 - Adoption of the proposed design criteria¹ to strengthen the identity of the region. • Outcome 2 - Adoption of the proposed design criteria² to enhance the performance and usability of planned connections. • Outcome 2 Adoption of the proposed design criteria³ to enhance underpass design and utility. • Outcome 2 - Inclusion in the project of the following infrastructure⁴: <ol style="list-style-type: none"> (i) A separated crossing at Dingley Bypass; (ii) Widen the proposed underpass to a minimum of 6 metres; (iii) Create a more direct off road connection at Bowen Parkway/Waterways Estate. • Outcome 3 – Adoption of the proposed allowances for priority future strategic connections⁵. • Outcome 4 - Adoption of the proposed design criteria⁶ for the noise wall and

¹ SPIIRE report, design criteria, page 14

² SPIIRE report, design criteria, page 18

³ SPIIRE report, design criteria, page 22

⁴ SPIIRE report, , page 19

⁵ SPIIRE report, Section identifying physical connections, to ensure compatibility with Council's strategic vision, page 23

⁶ SPIIRE report, design criteria, page 28

Commentary on aspect of EES	Potential measure to address concerns
<p>design through to operational stages.</p> <ul style="list-style-type: none"> • In addition to priority outcomes Council have identified effects to be avoided as a result of the project: <ul style="list-style-type: none"> • Divide the community; • Impede the realisation of strategic objectives for the region; • Impact negatively on the liveability of the surrounding residential areas and workplaces; or • Have a detrimental impact on the environmental values of the region. 	<p>screening vegetation treatments to maximise aesthetics appeal.</p> <ul style="list-style-type: none"> • Outcome 5 - Adoption of the proposed design criteria⁷ for the minimum design standard for low visual impacts for bridge structures. • Outcome 6 - Adoption of the proposed design criteria⁸ for the minimum design standard for sensitive interfaces. • Outcome 6 - Inclusion in the project of the following infrastructure <ul style="list-style-type: none"> (i) A transparent noise wall at the interface between Braeside Park and the freeway to protect the asset; (ii) Advanced tree planting close to Waterways residents to interrupt views of the bridge.
2 LANDSCAPE AND VISUAL EFFECTS (CHAP 11)	
<ul style="list-style-type: none"> • A number of matters relating to Landscape and Visual outcomes could be further strengthened to ensure that the project results in an exemplar outcome that achieves sensitive integration with the region. • A numbers of the EPRs relating to Landscape and Visual impacts are of a very general nature and would benefit from the inclusion of specific outcomes/deliverables. • Mitigation controls for some risks have the potential to compound or worsen other impacts. For example large sound walls to mitigate noise impact, have potential to generate localised overshadowing and impact upon flora and fauna within wetlands. A process is required documenting how competing requirements have been prioritised and incompatibilities resolved. 	<ul style="list-style-type: none"> • Adoption (including as appropriate through the Environmental Performance Requirements) of the recommendations of the SPIIRE report.
3 TRAFFIC AND TRANSPORT (CHAP 8)	
<ul style="list-style-type: none"> • The Project will post construction, reduce local congestion and associated delays, as 	<ul style="list-style-type: none"> • Clarification of interim (construction phase) impacts and measures.

⁷ SPIIRE report, design criteria, page 30

⁸ SPIIRE report, design criteria, page 320

Commentary on aspect of EES	Potential measure to address concerns
<p>well removing a significant “dog leg” within the existing road network for through traffic⁹.</p> <ul style="list-style-type: none"> • The EES identifies increased traffic load on connecting roads to the Moorabbin Airport associated with the Airport Master Plan and the Project. Duplication of Centre Dandenong Road is proposed to manage congestion issues. This will involve its temporary closure¹⁰ • Road users, residents and businesses in the municipality in the short term (the construction phase) will experience localised delays and localised access issues arising from the project. • Parts of the existing road network within the municipality are currently at capacity. The White Street Grosvenor Road- Greens Road Corridor already experiences congestion impacts of up to 20 minute delay. Similarly there is a large difference between fastest (11 mins) and slowest (31 min) travel times along Springvale Road Northbound. Construction works will further exacerbate this congestion¹¹. • Construction activities will cause disruption for commuters utilising Dingley bypass, Springvale Road and Boundary Road, during activities linking those roads to the freeway¹². • The duplication of Governor Road will cause disruption for commuters. Increase in traffic flows once completed has the potential to have impacts upon local residents. • The EES has identified the above risks as medium, with additional controls warranted through a “proposed Transport Management plan” to mitigate potential consequences¹³. Ongoing engagement will be required from Council to ensure the project adequately incorporates and implements the proposed “Transport Management Plan” 	<ul style="list-style-type: none"> • Modified EPRs to secure appropriate design and protective measures. • Additional public transport services (provided by PTV) to offset congestion during construction. • Modify EPR T2 to ensure consultation on the Traffic Management Plan occurs well in advance of construction commencing.

⁹ EES, Section 8.8.2, pg 8-17

¹⁰ EES, Section 20.10.

¹¹ EES, Section 8, pg 8-10.

¹² EES Section 8.8.1 Pg 8-16.

¹³ Table 8.6, pg 8-16.

Commentary on aspect of EES	Potential measure to address concerns
<ul style="list-style-type: none"> • It is unclear from the EES whether the increased congestion, changed traffic conditions and detours will result in increased accidents and risks of damage and injury to local road users and the public from these works, and what if any mitigation practices are proposed. • The assessment on impact assumes a 2 year construction period. Elsewhere within the EES a 2.5 year project construction period is proposed¹⁴. The lack of clarity is troublesome. The duration of works has obvious impacts upon the municipality • Council has a number of concerns relating to the overall impact on the traffic network, including the known impacts on Southern Road. • EPR T2 requires the preparation of a Traffic Management Plan. Consultation on this plan should begin well in advance of construction commencing. 	
<p>4 <i>BIODIVERSITY (CHAP 10)</i></p>	
<ul style="list-style-type: none"> • A number of EPR's address the impacts on flora and fauna, and talk to a number of mitigation measures including fauna barriers and fauna sensitive lighting design, it is considered that further matters including flight paths, migration patterns and vegetation loss (including within the Waterways wetlands, Mordialloc Creek bridge and Edithvale and Seaford Wetlands) have not been fully considered or appropriately addressed. • A particular concern of Council is in relation the flight paths of migratory birds that exist currently within the Freeway reserve. It is noted that many of the bird species that inhabit the Edithvale and Seaford Wetlands are regularly observed within the wetlands in and surrounding the Freeway reserve. • It is noted that noise impacts upon the receiving environment is above the recommended levels in 5 of the 9 modelled locations. See Table 10.12. Further work is recommended in this area. 	<ul style="list-style-type: none"> • Obtain better understanding of impact on flora and fauna, particularly for migrating birds. • Modified EPRs to secure appropriate design and protective measures.

¹⁴ EES, Section 8.1 & Section 6.13.2
 PAGE 6 TRIM XX/XXXX

Commentary on aspect of EES	Potential measure to address concerns
5 <i>NOISE (CHAP 12)</i>	
<ul style="list-style-type: none"> • The EES has identified noise as an issue through both construction and operational periods. The EES proposes to deal with noise through engineering controls (sound barriers) and good practices (hours of operation), being codified within works plans. • Notwithstanding the fact that the risk has been assessed as low. It is anticipated that there will be isolated problems over the life of the project and associated justified complaints from members of the municipality. • The Environmental Performance Requirement NV 2 provides a commitment to prepare a Construction noise and vibration management plan. 	<ul style="list-style-type: none"> • Modified EPRs to secure appropriate design and protective measures.
6 <i>AIR EMISSIONS (CHAP 13)</i>	
<ul style="list-style-type: none"> • The EES identifies that dust emissions from the construction project is modelled to be within 60 metres of source and up to 100 metres worst case. • Notwithstanding the fact that the risk has been assessed as low. It is anticipated that there will be localised impacts, with associated community concerns within the municipality. • The EES assesses that there is a risk of odorous emissions from the Dinsan landfill during construction, but the likelihood is low. A review of the EPA website indicates that the Dinsan landfill, generated amenity impacts historically. Intrusive works in the immediate vicinity of the landfill, has potential to generate further community concerns. • The Environmental Performance Requirement AQ2 provides a commitment to adherence to key guidelines, but remains very generic. 	<ul style="list-style-type: none"> • Amend AQ 2 to ensure <ul style="list-style-type: none"> ○ Inclusion of intrusive construction works around the former landfills as a high risk activity for odour generation. ○ Inclusion of “contingencies” to mitigate and manage any identified odour sources and respond to community concerns.
7 <i>PROJECT CONSTRUCTION AROUND SENSITIVE ENVIRONMENTS (WETLANDS) (CHAP 16)</i>	
<ul style="list-style-type: none"> • Construction over or in close proximity to wetlands will result in increased hazards during construction. 	<ul style="list-style-type: none"> • Modified EPRs to secure appropriate design protective measures.

Commentary on aspect of EES	Potential measure to address concerns
<p>1. The management of spoil material bearing acid sulphate soils, and potential contamination of near surface sediments, arising from past industrial practices in upper reaches of Mordialloc creek, during piling and other operations.</p> <p>2. The management of and response to accidents and spills and the ability of responders to contain and manage any clean up in a wetland environment.</p> <ul style="list-style-type: none"> • These issues have potential in the short term to impact upon assets in the municipality. • The EPR W3 envisages the development of various work and management plans¹⁵, to mitigate any risks. Scrutiny of these plans will be required to ensure all high risks are adequately captured and appropriate mitigation measures developed. Diligence in the preparation and adherence to the plans will be required to ensure the residual risks are as low as predicted 	
8 FLOODING (CHAP 16)	
<ul style="list-style-type: none"> • The EES has identified that flood plain capacity and flow will be impacted by the project. • <i>Flood Plain capacity.</i> The presence of the elevated freeway within a designated flood plain, will reduce the storage capacity and result in slightly higher 1 in 100 modelled flood levels. This has implications for inundation of properties in and around the project. • Stormwater flows. The Project footprint will impact upon localised directional flows. Increased hard surface from the roadways will place extra load upon the stormwater system, and the ability for other premises downstream to discharge stormwater in the event the stormwater system is approaching capacity. Again this may have implications for localised inundation downstream of the project. • The Environmental Performance Requirements W2 – W4 provide a 	<ul style="list-style-type: none"> • Amend EPRs W2 – W4 to ensure <ul style="list-style-type: none"> ○ Prior to construction, identify the location and capacity of downstream infrastructure (Melbourne water and Council drains) to handle expected flows to that infrastructure. ○ Where capacity issues are identified, undertake works to increase the capacity of that infrastructure, prior to Freeway construction works commencing. ○ Ensure handback procedures are in place prior to construction. ○ Ensure all statutory approvals are sought and obtained from the relevant authority (Melbourne Water and Kingston City Council etc) prior to construction works. ○ In the event that a statutory approval is not required (from the above mentioned authority(s)) seek and obtain confirmation that the approach documented is acceptable.

¹⁵ See EES sections 18.9, and 16.9

Commentary on aspect of EES	Potential measure to address concerns
<p>commitment to adherence to key guidelines and liaison with responsible authority(s). The EPRs do not address a commitment to determining the status of downstream drainage capacity and ensuring the ability to handle localised flows during storm events.</p>	<ul style="list-style-type: none"> ○ Consider stormwater re-use options for project inclusion.
9 COUNCIL INFRASTRUCTURE	
<ul style="list-style-type: none"> • The EES identifies significant infrastructure owned by a number of statutory authorities. No significant Kingston City Council infrastructure or other assets that might be impacted by the project have been identified. • It is possible KCC will have assets potentially impacted by the project (eg stormwater or changed landfill gas flows). 	<ul style="list-style-type: none"> • Identify the Kingston City Council infrastructure or assets that may be affected (whether requiring upgrade, replacement or construction) by delivery of the project. • Estimate works and associated costs. • Agreement with, or commitment to, Kingston City Council to address issues and expected costs. • Consider stormwater re-use options for project inclusion.
10 GROUNDWATER (CHAP 17)	
<ul style="list-style-type: none"> • The EES has assessed that there will be minimal impact upon groundwater levels. This is supported by the base design of an elevated roadway upon an earthen embankment. In the event that detailed design requires deep excavation and dewatering this will generate local drawdown and changed groundwater flow regimes. In this event contaminants and other materials are likely to be mobilised posing a risk for receiving waters of the contaminated extracted groundwater and altered flow paths. • The EES notes several current and former service stations in the vicinity of the project. In the event of deep works and groundwater drawdown in the vicinity of these features, the changed groundwater flows may change areas of impact and receptors. • Consideration will be required in the event of the identification of offsite groundwater contamination and its potential to impact upon the project. • While the EES has assessed the risk posed to groundwater as low, groundwater impacts can be time consuming and difficult to manage. 	<ul style="list-style-type: none"> • As no separate EPR is proposed for groundwater that as a minimum, groundwater contamination, and how it will be mitigated and managed, be included within the "Contaminated soils management plan" covered by EPR C11 as per 4 above.

11 LANDFILLS (CHAP 18)	
<ul style="list-style-type: none"> • The project alignment crosses part of the former Dinsan landfill. • Landfill Gas (LFG) has been identified at the landfill. • LFG gas containment, capture and destruction is required to be installed, managed and maintained to control risks associated with LFG build up and migration. • LFG infrastructure and its maintenance will be complicated by the presence of a freeway over the top of that landfill. • The freeway, and associated services, such as drainage lines and pits, will offer potential migration pathways for landfill gas. • The design construction of the freeway and landfill containment infrastructure must be complimentary to each other to minimise challenges in relation to the above. • An approach that ensures design, construction and ongoing management of the former landfill is compliant with best practice will minimise risk. • The use of an EPA appointed auditor(s), to verify design and audit construction and ongoing management will ensure risks are minimised. This approach ensures design is best practice and construction is conducted in accordance with the design intent. This approach is applicable to this closed former landfill. • The Environmental Performance Requirements (EPR) CL3 - CL5, go some way to addressing the above, but are not explicit. <p>This approach will ensure all landfill related potential impacts are addressed including leachate containment and management.</p>	<ul style="list-style-type: none"> • Amend EPR CL3 - CL5 to ensure <ul style="list-style-type: none"> ○ Single point of oversight to minimise potential for gaps in design and compatibility between landfill design, landfill rehabilitation and management works and broader freeway design, construction and operation. ○ EPA appointed auditor(s) utilised to verify design of landfill works, audit of construction of those works and periodic audit of ongoing management of landfill in accordance with EPA publication 788 <i>Best Practice Environmental Management Siting, design, operation and rehabilitation of landfills</i> and the approach specified in EPA publication 1323 <i>Landfill licensing</i>. It is noted the landfill is not operational and unlikely to require licensing. ○ Ensure all statutory approvals are sought and obtained from the relevant authority (EPA) prior to construction works. ○ In the event that a statutory approval is not required (from the above mentioned authority) seek and obtain confirmation that the approach documented is acceptable.

<p>12 LAND CONTAMINATION (PROJECT AREA IMPACT ZONE) CHAP 18</p>	
<ul style="list-style-type: none"> • The EES examined an area extending 150 m beyond the Project footprint for land contamination impacts. The basis for this 150 m impact area has not been documented¹⁶. • The “chosen envelope” may not be sufficient to have captured all impacts arising from the project. As <ul style="list-style-type: none"> a. Part of the Freeway is known to sit upon a former solid inert landfill. EPA Publication 788 <i>Best Practice Environmental Management Siting, Design, Operation and Rehabilitation of Landfills</i> (the BPEM) recommends a minimum buffer from buildings and structures of 500 m for putrescible landfills and 200 m for solid inert landfills¹⁷. b. The project area is within a sand belt area¹⁸ the lateral transmission of pollutants within groundwater is greater in this type of media than clays, suggesting the use of more conservative estimates. c. The shallow groundwater in the area is likely to impact upon in ground services This connectivity will increases the risk of corrosion and potential for leakage. For example service stations specifically their underground services (fuel tanks and lines). • The Environmental Performance Requirements CL1 provide a commitment to prepare a soil management plan and more accurately locate sources of contamination and refine management measures which should address the above. • The EPRs do not recommend plans for all likely waste streams likely to be encountered for example Non aqueous phase liquids, asbestos or groundwater. 	<ul style="list-style-type: none"> • Amend EPR CL 1 to ensure; <ul style="list-style-type: none"> ○ Inclusion within the soil and other management plans of “contingencies” to address the containment, cleanup and verification of both found and identified wastes. ○ Confirmation that the soil management plan also covers other pollutants or wastes including but not limited to Soil contamination, Groundwater contamination. Industrial or other wastes, Asbestos bearing materials. ○ Vapours arising from soil or groundwater contamination (hydrocarbons for example) along with exclusions including acid sulphate soils, landfill gas and PFAS.
<p>13 PFAS (CHAP 18)</p>	
<ul style="list-style-type: none"> • Per and poly-fluoralkyl substances (PFAS) are an emerging chemical of concern. The EES has identified PFAS in sediments and groundwater in and around the environs of the project. The 	<ul style="list-style-type: none"> • Ensure all statutory approvals are sought and obtained from the relevant authority (EPA) prior to construction works.

¹⁶ See section 18.5 of the EES

¹⁷ EPA Publication 788 *Best Practice Environmental Management Siting, Design, Operation and Rehabilitation of Landfills, Table 5.2*

¹⁸ See EES section 18.6.1

<p>level of knowledge, monitoring requirements and government response is also evolving.</p> <ul style="list-style-type: none"> • PFAS is associated with a number of industry sectors identified over the project area including landfills (northern part of project area), sewage treatment plants (former Braeside STP) and Moorabbin airport (adjacent to the project area). Sampling and testing has confirmed their presence at the landfill and airport environs. Contamination identified to date is at levels below investigation levels 19 noting that regulatory criteria continues to evolve. Investigations levels have been obtained from the publication PFAS NEMP (HEPA 2018). • The Environmental Performance Requirement CL6 provides a commitment to prepare a PFAS management plan. 	<ul style="list-style-type: none"> • In the event that a statutory approval is not required (from the above mentioned authority) seek and obtain confirmation that the approach documented is acceptable.
14 ASBESTOS (CHAP 18)	
<ul style="list-style-type: none"> • Asbestos is a potential material of concern that has been identified during sampling and is likely to be further identified across the Project. • It is noted sewer infrastructure utilised asbestos bearing pipework. Historically, upon decommissioning this infrastructure was broken and left in place (ie pipe bursting or abandonment). It is understood this practice is no longer permitted. • Part of the former Braeside sewage treatment plant is within the project construction corridor. There is potential for asbestos to be identified within that site. It is unclear whether the parts of the treatment plant that have been subsequently developed had asbestos removed. While asbestos at depth is unlikely to pose a significant health risk, it may generate an expectation for removal and disposal. • The EES acknowledges the issue of asbestos but considers it a low risk with project management. Like other issues, asbestos can generate public concerns disproportionate to the actual risk, and may do so as part of this significant infrastructure project. An EPR has not been prepared for asbestos as it is considered low risk. 	<ul style="list-style-type: none"> • As an EPR is not proposed for Asbestos, a clear statement is recommended ensuring that it is addressed within the Soil management plan, covered by EPR CL1. As per 4 above.
15 LAND CONTAMINATION (CONTAMINATED AND ASS SOIL) CHAP 18	
<ul style="list-style-type: none"> • Contaminated soil and acid sulphate soil has been identified within the Project area. The stated preference for handling these materials is 	<ul style="list-style-type: none"> • Amend CI1 and CI2 to ensure <ul style="list-style-type: none"> ○ all statutory approvals are sought and obtained from the relevant authority (EPA

¹⁹ See EES page 18-12

<p>reuse on site because the Project is a net earth user of up to 1,500,000 m³.</p> <ul style="list-style-type: none"> • The EES estimates the project will generate between 50,000 to 65,000 m³ of contaminated soil classified as Category C²⁰ which is the lowest contaminate level. Short term storage will be required prior to either long term containment on site or transport and disposal elsewhere. • In the event that the material is suitable for reuse prior approval will be required from the EPA²¹. • There are potential implications for Council in relation to storage of these materials both in the short and long term. In the short term there will be temporary storage requirements with the associated risk of mishap and discharge associated with both stormwater runoff and/or windblown dust. • If contaminated soil is reused there will be ongoing management requirements to ensure integrity of containment. In the event that this contaminated soil use extends beyond the Project envelope, there may be the expectation that KCC bear ongoing responsibility. • The Environmental Performance Requirements CL1 and CL2 provide a commitment to prepare both contaminated soil and Acid sulfate soil management plans and more accurately locate extent and suitable interim storage and management measures which should address the above. 	<p>and Kingston City Council) prior to excavation works.</p> <ul style="list-style-type: none"> • In the event that a statutory approval is not required (from the above mentioned authority) seek and obtain confirmation that the approach documented is acceptable.
16 PROJECT CONSTRUCTION WILL REQUIRE SIGNIFICANT FILL MATERIAL	
<ul style="list-style-type: none"> • The project is identified as requiring 1,500,000 m³ of fill material during the construction phase. • The demand for fill may have implications upon the ability of other projects within the municipality to meet their needs with associated delays for completion of works and associated increased project costs. Possible examples include the rehabilitation and capping of former landfills and former extractive industry sites. The project may cause delays in those other sites. • The EES has not identified this as an issue. 	<ul style="list-style-type: none"> • That an assessment be made prior to construction of the sources of fill and expected impacts upon other regional projects. • A dispute resolution procedure be developed, including principles to resolve points of dispute. • That the assessment and dispute resolution procedure be made available to potentially affected parties at the earliest.

²⁰ Victorian Government Gazette, No. S 271 Thursday 6 August 2009, Industrial Waste Guidelines and EPA publication Industrial Waste Resource Guidelines, Soil Hazard Categorisation and Management. Table 2 Soil Hazard Categorisation Thresholds and

²¹ See Environment Protection (Scheduled Premises) Regulations 2017, Schedule 1 Schedule Premises Table, L02 ((Contaminated sites—onsite soil containment) & L04 (Contaminated sites—long term management)

17 CUMULATIVE IMPACTS (CHAP 21)

- *Surface Water.* The proximity of other significant projects has the potential to impact upon surface water flows. No detail is provided as to the likelihood, consequences or combined risks of these interactions. Identified projects within the Mordialloc Creek catchment include development of Chadwick Reserve, and Hawthorn Football Club. It is proposed that these interactions be considered within a Water Management and Monitoring Plan²².
- *Transport.* The proximity of the Level Crossing Removal Project (Edithvale and Bonbeach), have potential to generate capacity issues for Springvale Road, Wells Road and Mornington Peninsula Freeway. Appropriate coordination and staging is crucial²³.
- The EES notes that in the event that other associated projects do not proceed that this project may not be delivered as projected. The consequence would be additional travel delays²⁴.

²² EES, Section 21.4.6

²³ EES, Section 21.4.2, paragraph 4.

²⁴ EES, Section 21.4.2, paragraph 5.

Attachment 2

Notices of Motion - 22 May 2017 and 25 September 2017

**City of Kingston
Ordinary Meeting of Council**

Minutes**22 May 2017****Procedural Motion****Moved: Cr Bearsley****Seconded: Cr Hua**

That Cr Bearsley be granted an extension of time to speak on the matter

CARRIED**Procedural Motion****Moved: Cr Oxley****Seconded: Cr West**

That Cr Barth be granted an extension of time to speak on the matter

CARRIED**The Substantive motion was put and CARRIED****A Division was Called:****DIVISION:****FOR:** Crs Brownlees, Staikos, West, Eden, Barth and Oxley (6)**AGAINST:** Nil (0)**ABSTAINED:** Crs Bearsley, Gledhill and Hua (3)**CARRIED****12.6 Notice of Motion No. 22/2017 - Cr West - Mornington Peninsula Freeway Extension Announcement****Moved: Cr West****Seconded: Cr Staikos**

That Council:

1. Ask officers to provide a report on the feasibility study and business plan, when they are released, and their key findings.
2. Council respectfully request the State Government to consider and provide a report to the Council and community on the potential impact of the MPFE on:
 - a) Braeside Park and the wetlands and heritage buildings on the park,
 - b) the Kingston Green Wedge,
 - c) environmental, horticultural and Aboriginal Cultural Heritage issues, and
 - d) the amenity and road access of the residents of Aspendale Gardens (east of Wells Road), Richfield Retirement Village, Waterways, Dingley Village and the Green Wedge.
 - e) Whether the freeway will need to be constructed as an elevated freeway (Skyroad) through the Waterways and Braeside Park wetlands or any other part of the route
3. Council asks State Government to:
 - a) undertake an Environmental Effects Statement investigation into any potential impact that may be caused by the MPFE, especially to the Braeside Park and

**City of Kingston
Ordinary Meeting of Council**

Minutes**22 May 2017**

Waterways Wetlands and ensure that any negative impact is minimised;

- b) Give greater priority to the extension of Westall Road to improve motorists' access from the Dandenong Bypass to the Monash Freeway, before commencing the Mornington Peninsula Freeway extension.
- c) ensure that an overpass is constructed at or close to the intersection of the MPFE with the Dingley Bypass to allow continuity for passage of cyclists, pedestrians and equestrians along the Chain of Parks trail, whose route is currently proposed to end in a T-junction at the Dingley Bypass.
- d) ensure that the impact on Kingston's market gardens is minimised.
- e) work with Council to plan for and fund the construction of wetlands on land adjoining the Settlement Creek and Dunlops Creek and on the Green Wedge triangle abutting Springvale Road.
- f) consider the formation of a community reference group to guide decisions about reviewing and implementing plans for the MPFE.

Cr Oxley left the meeting at 10.09pm

Cr Brownlees left the meeting at 10.09pm

Cr Oxley returned to the meeting at 10.10pm

Cr Hua left the meeting at 10.11pm

Cr Gledhill left the meeting at 10.12pm

Cr Brownlees returned to the meeting at 10.13pm

Cr Gledhill returned to the meeting at 10.14pm

Cr Hua returned to the meeting at 10.15pm

CARRIED

13. Urgent Business

There were no items of urgent business.

**City of Kingston
Ordinary Meeting of Council**

Minutes**25 September 2017****12.3 Notice of Motion No. 40/2017 - Cr West - Mentone and Cheltenham Plans**

The item was withdrawn by Cr West.

12.4 Notice of Motion No. 41/2017 - Cr Staikos - Mordialloc Bypass**Moved: Cr Staikos****Seconded: Cr West**

1. Council officers brief Councillors on the matters raised in the Notice of Motion 12.6 that was carried at the May 2017 ordinary Council Meeting,
2. Council works collaboratively with VicRoads to resolve the following issues in relation to the Mordialloc Bypass:
 - a. Council supports the establishment of acoustic fence treatments on both sides of the Mordialloc bypass, with proper vegetation screening to protect the amenity of, Aspendale Gardens, Dingley Village and Waterways residents as well as the amenity of Braeside Park and the Waterways and Woodlands wetlands.
 - b. Council requests that the acoustic fence be lined with a screen of tall indigenous trees to encourage birds to fly across the freeway at a good height, to reduce the risk of being struck by traffic.
 - c. Council does not support the closure of Old Dandenong Road, between Boundary Road and Tootal Road.
 - d. As part of this project Council requests VicRoads improve the traffic management of Centre Dandenong Road, especially between Boundary Road and Tootal Road, and improve the flow of traffic at the roundabout.
 - e. Council reiterates its position to ensure that an overpass/underpass is constructed at or close to the intersection of the Mordialloc Bypass with the Dingley Bypass to allow continuity for passage of cyclists, pedestrians and equestrians along the Chain of Parks trail.
 - f. Council works with VicRoads and Melbourne Water to ensure the establishment of wetlands along the route of the Mordialloc Bypass.
 - g. That the road be located as far to the west as possible on the reservation where it passes Braeside Park and Dingley Village to make room for and the Chain of Parks shared trail to be on the eastern side of the freeway and to minimise any disruption to the Braeside Park historic buildings and rangers' facilities.
 - h. To relocate any of the Braeside Park historic buildings and rangers' facilities that would still be impacted despite (g).
 - i. That VicRoads offers public information sessions to the residents of Dingley Village, Waterways and Aspendale Gardens (east of Wells Road).

Amendment**Moved: Cr Bearsley****Seconded: Cr Hua**

1. Council officers brief Councillors on the matters raised in the Notice of Motion

**City of Kingston
Ordinary Meeting of Council**

Minutes**25 September 2017**

12.6 that was carried at the May 2017 ordinary Council Meeting,

2. Council works collaboratively with VicRoads to resolve the following issues in relation to the Mordialloc Bypass:
 - a. Council supports the establishment of acoustic fence treatments on both sides of the Mordialloc bypass, with proper vegetation screening to protect the amenity of, Aspendale Gardens, Dingley Village and Waterways residents as well as the amenity of Braeside Park and the Waterways and Woodlands wetlands.
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 - e. Council reiterates its position to ensure that an overpass/underpass is constructed at or close to the intersection of the Mordialloc Bypass with the Dingley Bypass to allow continuity for passage of cyclists, pedestrians and equestrians along the Chain of Parks trail.
 - f. Council works with VicRoads and Melbourne Water to ensure the establishment of wetlands along the route of the Mordialloc Bypass.
 - g. That the road be located as far to the west as possible on the reservation where it passes Braeside Park and Dingley Village to make room for and the Chain of Parks shared trail to be on the eastern side of the freeway and to minimise any disruption to the Braeside Park historic buildings and rangers' facilities.
 - h. To relocate any of the Braeside Park historic buildings and rangers' facilities that would still be impacted despite (g).
 - i. That VicRoads offers public information sessions to the residents of Dingley Village, Waterways and Aspendale Gardens (east of Wells Road)
 - j. Council requests a fly over at Governor Road

**The Amendment was put and CARRIED
The Amendment became the Motion**

Procedural Motion

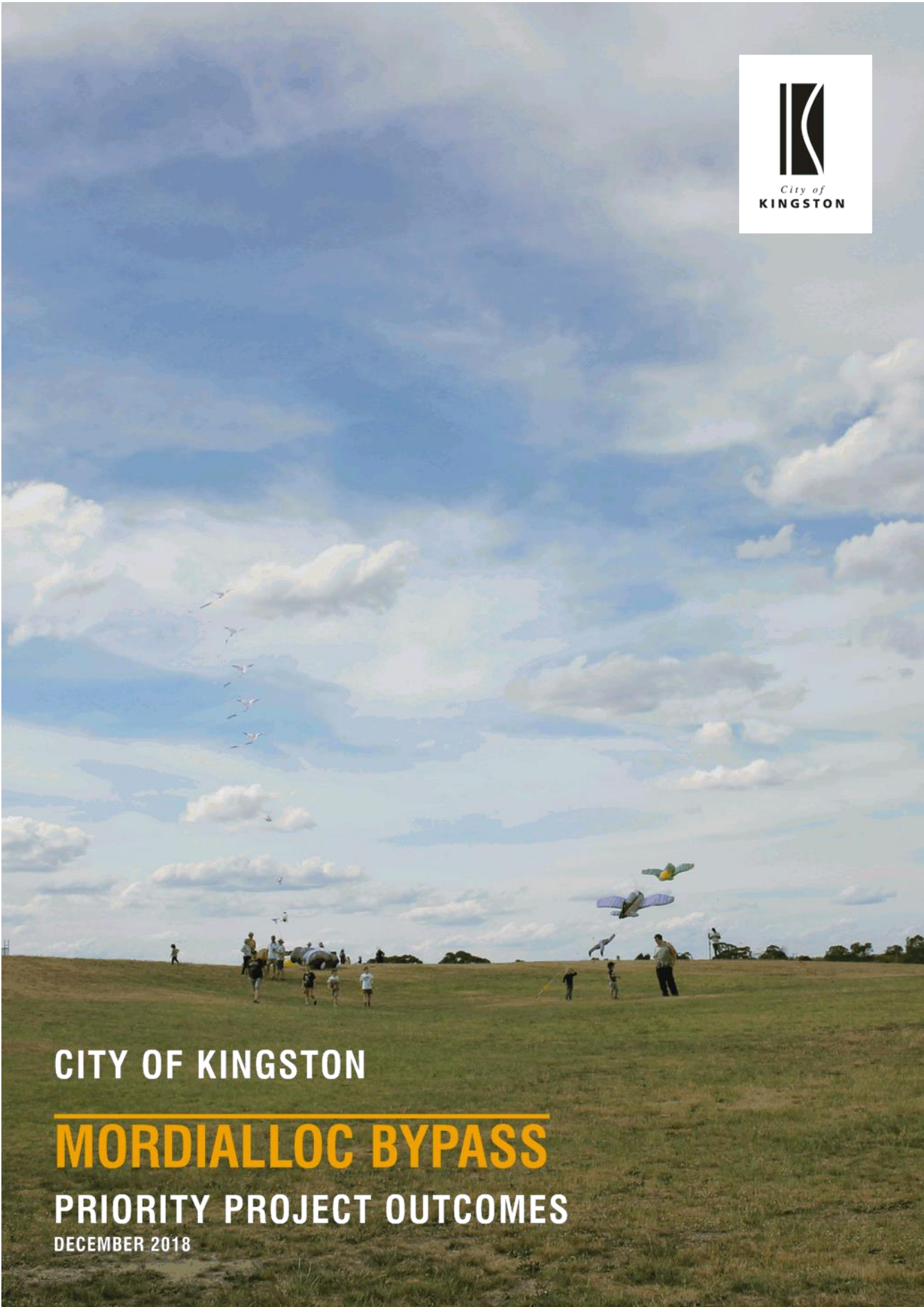
Moved: Cr Brownlees Seconded: Cr Gledhill

That the motion now be put.

**CARRIED
The Motion was put and CARRIED**

Attachment 3

City of Kingston Landscape and Visual Assessment- Priority Project
Outcomes (Spiire) or as adopted at December 10 2018 Council
Meeting



CITY OF KINGSTON

MORDIALLOC BYPASS

PRIORITY PROJECT OUTCOMES

DECEMBER 2018

This report was commissioned by City of Kingston and was prepared by Spiire Australia, Level 4, 414 La Trobe Street PO Box 16084 Melbourne Victoria 8007

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STATUS	DATE	COMPILED BY	AUTHORISED
0 - ISSUED FOR PUBLIC CONSULTATION	13.11.2018	T.S.	M.B.
1- COUNCIL ADOPTED POSITION	4.12.2018	T.S.	M.B.



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01.

EXECUTIVE SUMMARY

This report outlines the City of Kingston's adopted position with regards to the Mordialloc Bypass Project (the 'Project'). The report considers feedback from public consultation undertaken during November 2018 and will form part of Council's submission to Planning Panels Victoria as part of the Environmental Effects Statement process.

The report considers a substantial volume of project information made available to Council by the Major Road Projects Authority (MRPA) and VicRoads, including; a landscape concept plan developed by VicRoads (May 2018), reference design, project performance requirements and other background information underpinning the current design intent.

In October 2018 the Mayors of Glen Eira, Kingston and Monash joined together to call for an urgent commitment from the Victorian Government to extend Westall Road to link the Dingley Bypass and new Mordialloc Freeway with the Monash Freeway. The City of Kingston recognises the importance of the planned Mordialloc Bypass connection, however is still committed to ensuring that maximum efficiency of the network is realised through the prioritisation of the Westall Road link in conjunction with the delivery of the Mordialloc Bypass.

COUNCIL'S POSITION

The City of Kingston supports the principles and objectives of the VicRoads reference design, however on behalf of the community, Council is committed to ensuring that these principles and objectives are well realised and that the project is an exemplar that achieves sensitive integration with the region.

Based upon Council's review, 6 priority outcomes have been identified that are considered to be critical to the success of the project. Refer page opposite.

Council expects that these outcomes will be considered and integrated by VicRoads and the MRPA throughout the design development of the project to ensure that certain consequences are avoided.

Specifically, the project must not;

- ▶ Divide the community;
- ▶ Impede the realisation of strategic objectives for the region;
- ▶ Impact negatively on the liveability of the surrounding residential areas and workplaces; or
- ▶ Have a detrimental impact on the environmental values of the region.

COUNCIL ENGAGEMENT THROUGH DESIGN DEVELOPMENT

To ensure that the priority outcomes outlined in this report are addressed through the design development process, Council requests that the MRPA set up an appropriate engagement forum that enables Council and other key stakeholders to review information and provide feedback. For example; an Urban Design Assessment Panel (UDAP).

02

COUNCIL'S PRIORITY OUTCOMES



IDENTITY AND ENVIRONMENT

- 1 THE DEVELOPMENT TELLS A CLEAR STORY ABOUT PLACE; A REGION INVESTED IN AN ENVIRONMENTALLY PROGRESSIVE FUTURE



MOVEMENT AND CONNECTIONS

- 2 LINKS AND JOURNEYS ARE WELL USED: INTEGRATED, DIRECT, ACCESSIBLE, LEGIBLE, ATTRACTIVE AND SAFE
- 3 IMPORTANT COMMUNITY CONNECTIONS ARE MAINTAINED AND FUTURE STRATEGIC CONNECTIONS ARE ALLOWED FOR



AMENITY

- 4 EFFECTIVE SHORT AND LONG TERM VISUAL SCREENING
- 5 HIGH QUALITY, VISUALLY RECESSIVE BRIDGE STRUCTURES
- 6 THE SENSITIVE INTERFACES OF BRAESIDE PARK, DINGLEY VILLAGE, ASPENDALE GARDENS AND WATERWAYS COMMUNITIES ARE WELL MANAGED IN THE DESIGN RESPONSE

Green wedge - City of Kingston

03.

PROJECT CONTEXT

The landscape, communities and economies of the south-eastern region are taking on a new form. This change is being driven largely by population growth, transport infrastructure investment, new tourism and economic trends, and the vision of key strategies such as the *South East Green Wedge Management Plan*.

The Mordialloc Bypass is located entirely within the municipality of Kingston and passes through sections of the Green Wedge as well as industrial and residential areas. A number of important local strategic drivers and considerations affect the development of the freeway connection.

KINGSTON GREEN WEDGE

The *Kingston Green Wedge Plan 2012* and the *Sand Belt Open Space Development Plan 1994* are of particular relevance to the proposed route. They outline a vision for the region that will see creation of an extensive open space network or 'Chain of Parks' linked by walking and cycling trails, protection of significant environmental assets, support for more sustainable land use practices and regional renewable energy initiatives and the phasing out of heavy industry and landfill.

SIGNIFICANT CONSERVATION AREAS

Braeside Park sits immediately adjacent to the proposed freeway. The park covers an area of 295 hectares and contains areas of remnant native bushland, native grasslands and extensive seasonal wetlands. The park is managed by Parks Victoria and is identified in the Kingston Planning Scheme as an area of environmental significance and a recreational asset for the region.

To the south-west of the project site is the Ramsar listed Edithvale Seaford wetlands. They cover over 250 hectares and are the largest wetland of their type in the Westernport region and the only remnant of the once 4000 hectare Carrum Carrum Swamp.

The rare birds, including native and migratory species, that visit the Edithvale and Seaford Wetlands also visit the wetlands adjoining the freeway reserve; Waterways, Woodlands and Braeside Park Wetlands. There is substantial movement of birds flying across the freeway reserve to access the wetlands adjacent.

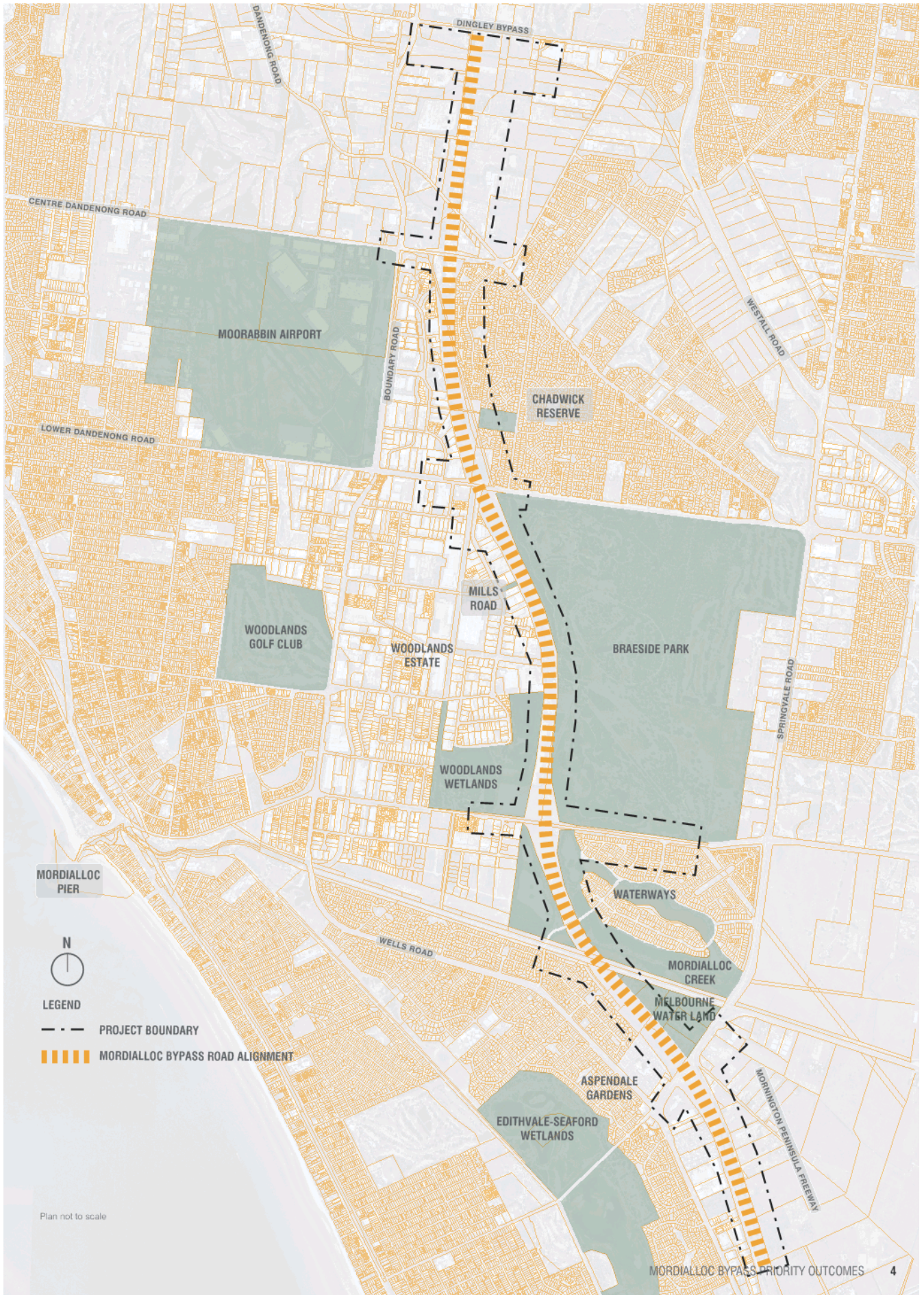
SENSITIVE RESIDENTIAL INTERFACES

Three established residential communities share a direct interface with the freeway corridor; Dingley Village, Aspendale Gardens and the Waterways estate. It is of particular importance to Council that these interfaces are well managed in terms of visual and noise amenity. The Waterways Estate is specifically subject to a Design and Development Overlay in the local planning scheme that aims to conserve and enhance a landscape character supportive of indigenous flora and fauna habitats.

EMPLOYMENT PRECINCTS

The project area also interfaces with regionally significant employment precincts and transport connections, such as Woodlands Industrial Estate and Moorabbin Airport.

In line with the Kingston Municipal Strategic Statement; Council is committed to supporting the continued growth and diversification of employment precincts to maintain their viability. The Woodlands Industrial Estate is demonstrating a positive transition towards a more diversified precinct with a range of business and land use activities, such as; a hotel, restaurant, financial services and a community garden. Maintaining and increasing connectivity to this precinct is a high priority for Council.



MORDIALLOC
PIER



LEGEND

--- PROJECT BOUNDARY

||||| MORDIALLOC BYPASS ROAD ALIGNMENT

Plan not to scale

MORDIALLOC BYPASS PRIORITY OUTCOMES

4

04.

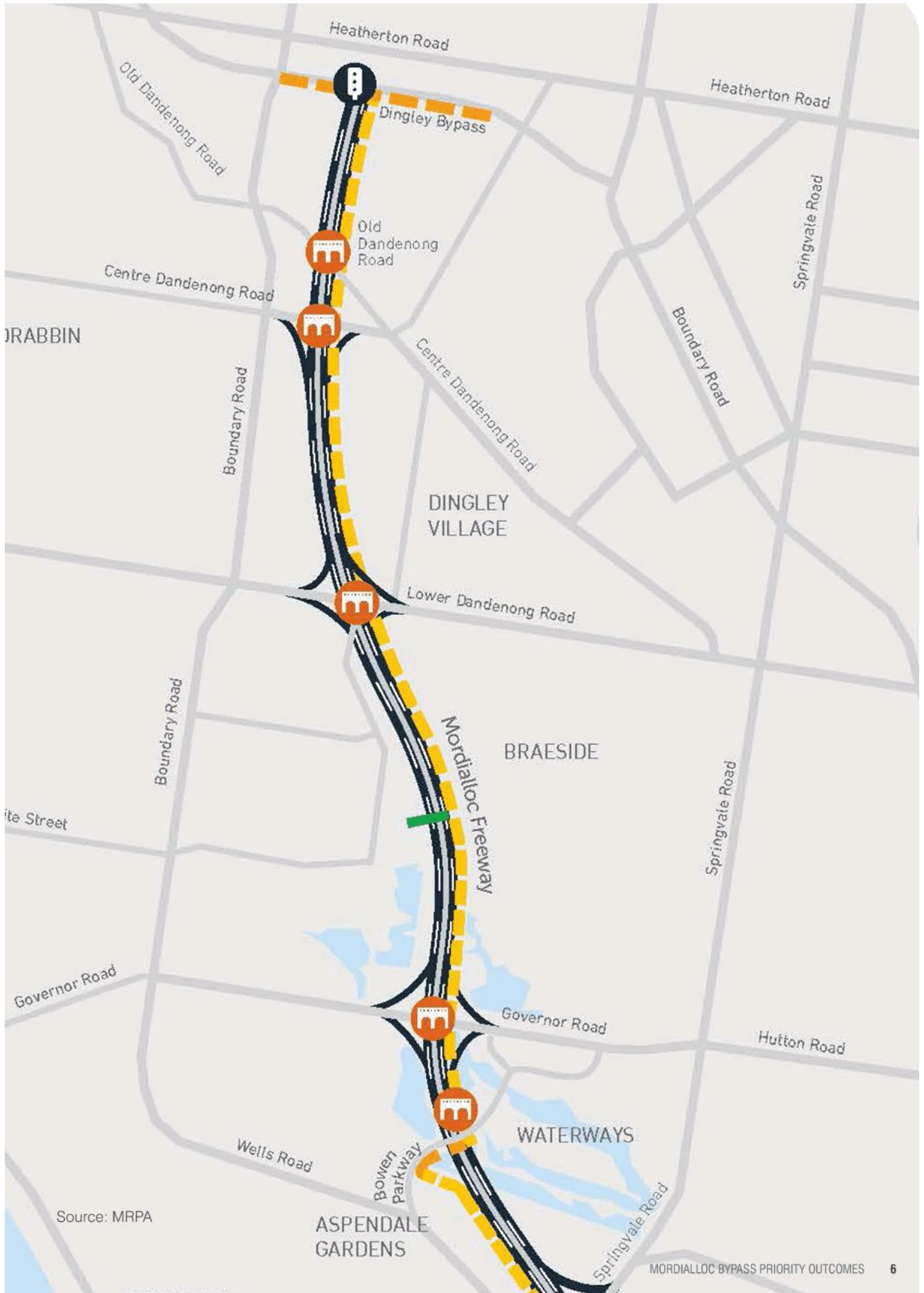
PROJECT OVERVIEW

In the 1970s a road reserve was put aside, as part of a broader, integrated freeways plan, for a six lane connection to accommodate the future urban growth of the south-eastern region. VicRoads are proposing to develop the Mordialloc Bypass within what remains of this corridor; a 9.7 kilometre freeway link that will connect the Dingley Bypass to the Mornington Peninsula Freeway at Springvale. The freeway aims to reduce congestion on arterial roads by creating a continuous connection to Frankston. The Bypass will traverse constructed wetlands, move between established residential communities, industrial areas, conservation areas, open space and agricultural land within the South East Green Wedge.

The current project proposal includes:

- ▶ a dual carriageway with two to three lanes in each direction
- ▶ bridges over Springvale, Governor, Lower Dandenong and Centre Dandenong Roads, along with new freeway entry and exit ramps
- ▶ bridges over Old Dandenong Road and through the sensitive Waterways area
- ▶ traffic lights at the Dingley Bypass intersection
- ▶ an upgrade of the existing interchange at Thames Promenade, Chelsea, with the Mornington Peninsula Freeway, along with freeway entry and exit ramps
- ▶ a new shared walking and cycling path along the length of the freeway on the eastern side
- ▶ fauna exclusion fences adjacent to Braeside Park and wetlands and habitat crossing points for fauna under the freeway
- ▶ a pedestrian underpass between Braeside Park and Woodlands industrial precinct
- ▶ extensive wetlands and water sensitive urban design treatments
- ▶ tree planting and landscaping along the length of the corridor
- ▶ noise walls to residential areas.

For more information visit <http://roadprojects.vic.gov.au/>



05.

PROJECT STATUS

A range of investigations were undertaken in 2017 and a reference design was developed by VicRoads in May 2018 that guided the project performance and contractual requirements for the recently released design and construct tender. This tender is being administered by the Major Road Projects Authority (MRPA) on behalf of VicRoads.

Kingston City Council officers have met regularly with VicRoads and the MRPA since early 2017 to provide feedback and input as the planning and design progressed. This has included officer representation on the Technical Reference Group established by the MRPA as part of the Environmental Effect Statement process.

Council has passed two Notices of Motion on 22 May 2017 and 25 September 2017 outlining issues to be resolved collaboratively with VicRoads.

As a result of this positive collaboration, a number of the points raised in the Notices of Motion have been considered and addressed by VicRoads and the MRPA in the development of the design and construct tender, including;

- ▶ Incorporation of tall indigenous tree planting along the acoustic fence adjacent to wetlands, to encourage safe crossing of birds over the freeway.
- ▶ Incorporation of acoustic screening at the interface of Aspendale Gardens, Dingley Village and Waterways residential areas.
- ▶ Retaining the Old Dandenong Road connection between Boundary and Tootal Roads.
- ▶ Establishment of wetlands along the route of the Bypass
- ▶ Accommodation of the Chain of Parks shared trail and minimal disruption to the Braeside Park historic buildings and ranger's facilities on the eastern side of the freeway.
- ▶ Public consultation with community members from Dingley Village, Waterways and Aspendale Gardens (east of Wells Road).

Outstanding issues from the Notices of Motion are captured within the requested project outcomes in this report, such as;

- ▶ Establishment of acoustic fence treatments adjacent to important open space assets and conservation areas such as Braeside Park, the Ranger's facilities and the Woodlands wetlands.

- ▶ Provision of a grade separated pedestrian and cycling connection (under or overpass) at the Dingley Bypass intersection.

The project is currently in tender design phase, which is running concurrently with an Environmental Effects Statement (EES) process. The EES investigation commenced, following finalisation of the scoping requirements of the EES by DELWP in May 2018. Findings were presented to stakeholders for comment in late October and will be finalised in early 2019 when the successful tenderer is appointed. Once finalised, the proponent will be required to address the recommendations of the EES.

The EES focuses on the potential effects on:

- ▶ biodiversity and habitat – migratory birds and the native vegetation when modifying grasslands and wetlands.
- ▶ surface water and groundwater – the quality of water flows and drainage in the Mordialloc Creek catchment and the Edithvale-Seafood Wetlands.
- ▶ noise and air quality – increased traffic and noise for residents and nearby park lands, in addition to local air quality with exposure to vehicle emissions.
- ▶ landscape and visual amenities – impacts to wetlands and local amenity from new infrastructure such as bridges and overpasses.
- ▶ traffic and transport – journey and road safety improvements.

Other studies include:

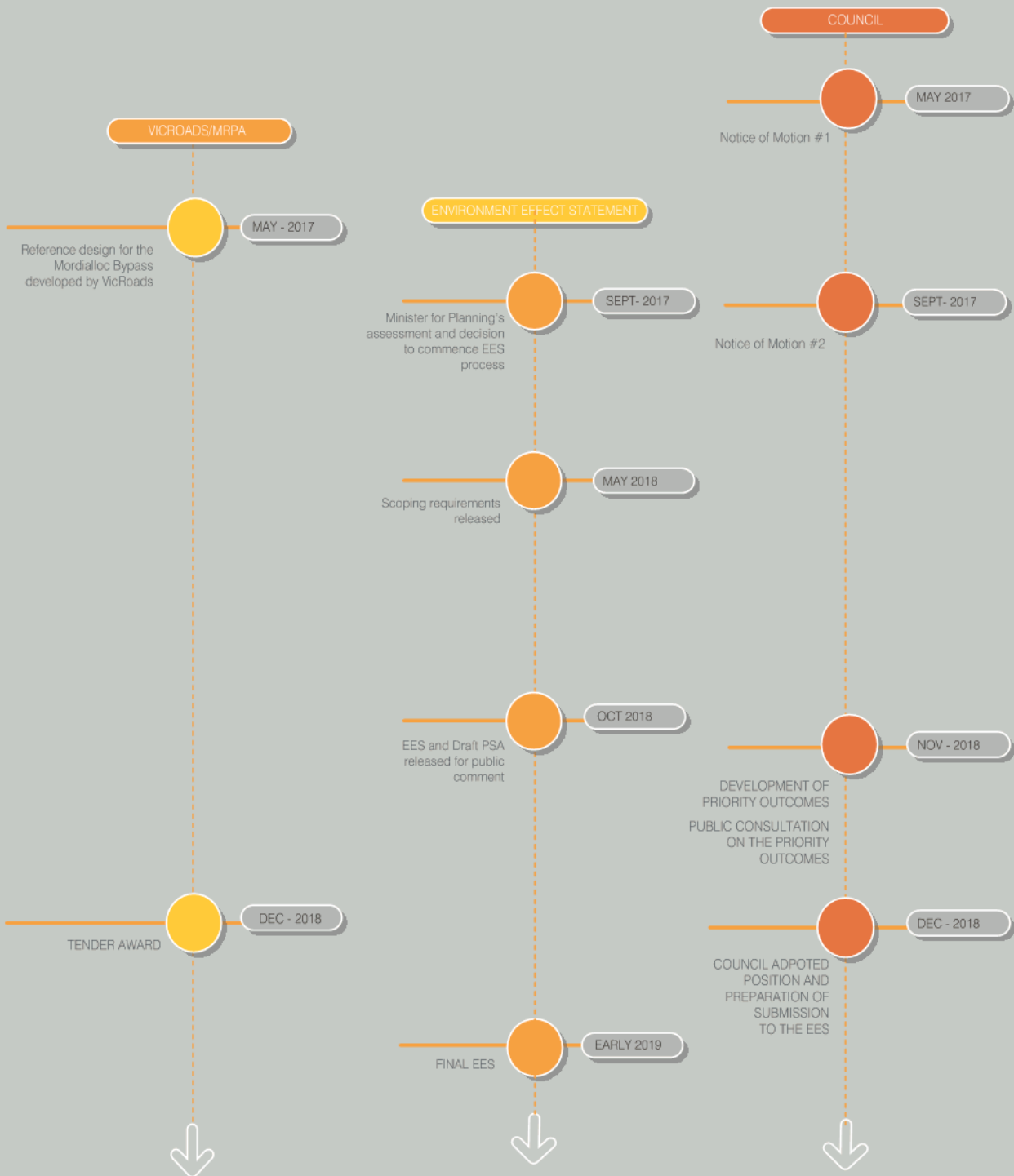
- ▶ Land contamination
- ▶ Cultural heritage
- ▶ Amenity and environmental quality
- ▶ Social, land use and infrastructure.

Council is currently reviewing the recently released Environmental Effects Statement and will undertake its own assessment of potential environmental effects associated with the project. The outcome of the review will inform Council's submissions through the EES process.

For information on the EES findings, visit <http://roadprojects.vic.gov.au/projects/mordialloc-freeway/view-the-ees>

06.

PROJECT TIMELINE



07.

COUNCILS PRIORITY OUTCOMES



IDENTITY AND ENVIRONMENT

REPRESENTING A REGION INVESTED IN AN ENVIRONMENTALLY PROGRESSIVE FUTURE THROUGH DESIGN.



Market Gardens - City of Kingston



Infrastructure and landscape design can tell a clear story about the character and identity of an area. The current proposal states the objective of 'Local Identity'; building an understanding of place through highlighting intrinsic characteristics. Council supports this objective but believes the proposed built form design, materiality, wayfinding elements and vegetation treatments do not communicate a unified story about the identity of the region that will be recognisable to motorists or pedestrians.

The current design potentially communicates a mixed message. Planting arrangements are both formal/linear (referencing agricultural uses and varying adjacent urban form) and informal/scattered (referencing indigenous vegetation communities). Noisewall forms are influenced by the nearby Dingley Bypass for continuity but are not representative of a clear theme.

Whilst the area that the freeway traverses is indeed diverse, Council believes there is a strong overarching identity that should be communicated and that strategic environmental objectives and values are strongly tied to this. This identity or 'theme' can be defined as:

“A REGION INVESTED IN AN ENVIRONMENTALLY PROGRESSIVE FUTURE”.

Council sees that this vision represents the values and aspirations of the region embodied in key strategies such as the *Kingston Green Wedge Plan 2012* and the Local Planning Scheme. In particular;

1. Protection of areas of conservation significance (eg Braeside Park and Ramsar wetlands),
2. Encouraging ecologically sensitive development,
3. Investment in renewable energy,
4. Reatain and enhance land use to encourage more sustainable agricultural practices and food security,
5. Creation of habitat and open spaces, and
6. Investment in diverse economies and healthier, better connected communities.

In terms of character and identity; Council's key strategies identify a preference towards low visual impact development, the use of indigenous vegetation in landscaping to represent pre-settlement landscapes (wetlands and open woodlands), and a muted colour palette.

IDENTITY AND ENVIRONMENT

OUTCOME 1

THE DEVELOPMENT TELLS A CLEAR STORY ABOUT PLACE – A REGION INVESTED IN AN ENVIRONMENTALLY PROGRESSIVE FUTURE

The Dingley Bypass/ Mordialloc Bypass intersection is an identified 'Green Wedge Gateway site' in the Green Wedge Plan that must incorporate 'well designed, high quality architecture that is underpinned by the principles of sustainability'. The current design for this entry and along the length of the bypass, requires further work to ensure that it communicates a strong and legible identity that supports Council's vision for the region.

THE MOTORIST'S JOURNEY

Under the current design proposal, motorists will travel along open and semi-rural areas in the northern section, past scattered Eucalypts and formal, banded shrub plantings, into closed sections of roadway, bounded on both sides by an abrupt metal noisewall interface. Along the journey there will be glimpses of the surrounding area at bridges through semi-transparent plexi-glass panels and open sections with clear views to the Woodlands Industrial Estate, Braeside Park and open tracts of undeveloped land. The wetlands, which characterise this area will not be visible on the drive, masked by opaque noisewalls.

The proposed noisewall materiality includes corten (weathered steel) sheeting panels in most sections; varying in height from 0.5 to 6 metres. Some sections are proposed to use plexi-glass panels with a gradation of semi-transparency at the base to fully transparent panels at the top. Transparent sections are mostly located on bridges and adjacent to Chadwick Park.

HOW THE DESIGN CAN STRENGTHEN COUNCIL'S ENDORSED IDENTITY FOR THE REGION

In addition to the current scheme, the development needs to demonstrate that:

1. The environmental assets of the region are valued, specifically Braeside Park and adjacent wetland systems.
2. Creation and protection of habitat and ecological diversity are core drivers of the design response
3. The region is supportive of innovative and progressive strategies that build resilience.

1. VALUING ENVIRONMENTAL ASSETS OF THE REGION

Council is pleased to see that the current proposal aims to reduce impacts on the Waterways wetlands by minimising impacts from the bridge infrastructure that will cross it and that soundwalls will be incorporated. Tall trees are proposed adjacent to the noise walls to enable water birds with long take off distances to perceive the freeway and make their way over it safely. In addition to these treatments, it is of utmost importance that noise impacts on Braeside Park are minimised, whilst retaining views to this distinctive conservation asset. Likewise, views to all adjacent wetlands should be a feature of the journey.

2. PROTECTION AND ENHANCEMENT OF BIODIVERSITY

The *City of Kingston Biodiversity Strategy 2018-2023* highlights the importance of open space in retaining and enhancing biodiversity values. Integral to this is connectivity between conservation assets. Water corridors and physical connections with high quality habitat are imperative in facilitating movement of indigenous fauna species.

Freeway developments cause a major barrier to achieving this connectivity both physically and from light and noise impacts. The current proposal attempts to overcome this by incorporating possum crossings over the freeway, noise walls and tree planting buffers, habitat crossings via culverts under the freeway and WSUD treatments that maintain hydrological function and improve water quality. Council is concerned that some of these treatments (habitat crossings) are not well proven and noise impacts to Braeside Park are not being mitigated.

To further support Council's strategic biodiversity objectives the development should use exclusively indigenous plant species and native species that achieve creation of quality local habitat. The corridor itself is also an opportunity to create connectivity.



Integrated solar panels on noise walls
Calder Freeway - Melbourne

IDENTITY AND ENVIRONMENT

OUTCOME 1 CONTINUED

THE DEVELOPMENT TELLS A CLEAR STORY ABOUT PLACE – A REGION INVESTED IN AN ENVIRONMENTALLY PROGRESSIVE FUTURE

3. DEMONSTRATING SUPPORT FOR A PROGRESSIVE FUTURE

Further treatments can be considered that strengthen the theme, such as incorporation of integrated stormwater treatments (Chadwick Reserve), design of the built form (incorporating recycled materials, recognisable motifs or patterns and solar panels), landscape design (selecting iconic locally indigenous species representative of pre-settlement landscapes) and integration with the currently proposed way finding elements and incorporation of artwork.

The weathered steel and plexi-glass panel treatment is largely sympathetic to Council's preferred design outcome, however it is imperative that the design of each of these materials work together to demonstrate a strong thematic outcome.

A unique opportunity exists to lay a 'purple' pipeline for recycled water in conjunction with the upcoming Mordialloc Freeway works to provide a strategic and cost-effective way to provide recycled water infrastructure to Kingston's Green Wedge which is home to market gardens, golf courses and parkland (with possible future open space including Hawthorn Football Club).



THE MORDIALLOC BYPASS DESIGN CAN STRENGTHEN THE IDENTITY OF THE REGION BY ADDRESSING THE FOLLOWING DESIGN CRITERIA:

1. Maintain clear views to the assets of the region to highlight places that are representative of an integrated urban ecology, specifically: Braeside Park (transparent noise wall interface), wetlands (transparent noise wall interface).
2. Create a continuous habitat corridor of trees along the length of the freeway using only locally indigenous species.
3. Showcase identifiable indigenous species at key nodes and interchanges.
4. Embed environmental targets within the development that exceed the bare minimum, such as noise walls that integrate solar panels and extensive use of recycled materials.
5. Install a 'purple pipeline' along the length of the freeway as part of Council's water security objectives for green space.
6. Provide an architectural and urban design response that references this theme eg: celebration of recycled materials within the materiality, especially at key nodes and interfaces, branding way finding elements and the built form with recognisable forms, motifs, colours and patterns that represent the theme.
7. Incorporate interpretive art and signage that informs and celebrates local flora and fauna.
8. Retain as many indigenous trees as possible using protection measures during construction and consider alternative path alignments to minimise impacts.
9. Ensure the preformed corrugated sheet 'zig zag' pattern is integrated with the theming and other urban design outcomes such as bridge design and paving treatments.
10. Expand and enhance Chadwick Reserve with the creation of wetlands in the western portion of the site to both treat and harvest stormwater for re-use.

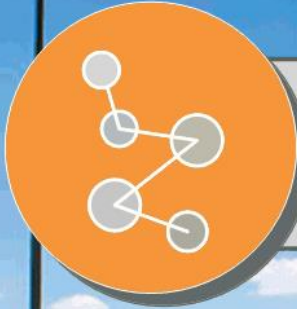
Wetlands estate - City of Kingston

MORDIALLOC BYPASS PRIORITY OUTCOMES

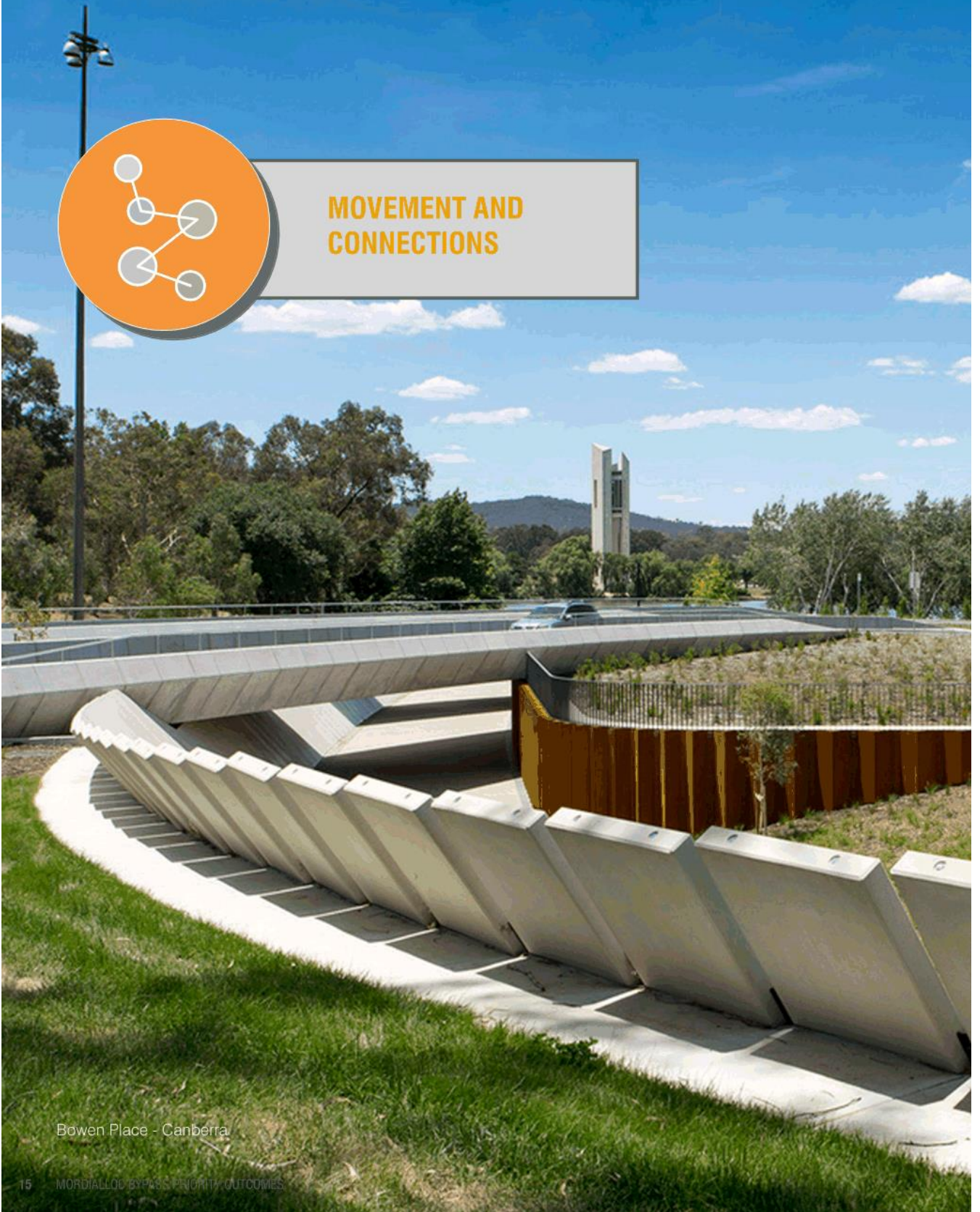
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08.

COUNCIL'S PRIORITY OUTCOMES



MOVEMENT AND CONNECTIONS



Bowen Place - Canberra



Connectivity throughout the region is a core objective of the Green Wedge Plan and is supported by current State and Federal policies relating to public health. Council's vision for the creation of a 'Chain of Parks' is underpinned by a comprehensive cycling, walking and equestrian trail network.

Council is pleased to see the incorporation of a continuous shared use path (SUP) along the length of the freeway and provision for linkages to the existing footpath network. A number of treatments are also proposed in the reference design to create an attractive environment for users and mitigate the impacts of noise and freeway infrastructure, however Council believes further enhancements could be made to ensure that the connections are well used and that the key connections identified in the Green Wedge Plan are achieved.

MOVEMENT AND CONNECTIONS

OUTCOME 2

LINKS AND JOURNEYS ARE WELL USED: INTEGRATED, DIRECT, ACCESSIBLE, LEGIBLE, ATTRACTIVE AND SAFE

The road environment can be a significant barrier to walking, cycling and other users, for example; equestrian. Inhospitable conditions and poor amenity created by radiant heat from vast road surfaces and noise from speeding traffic should be mitigated.

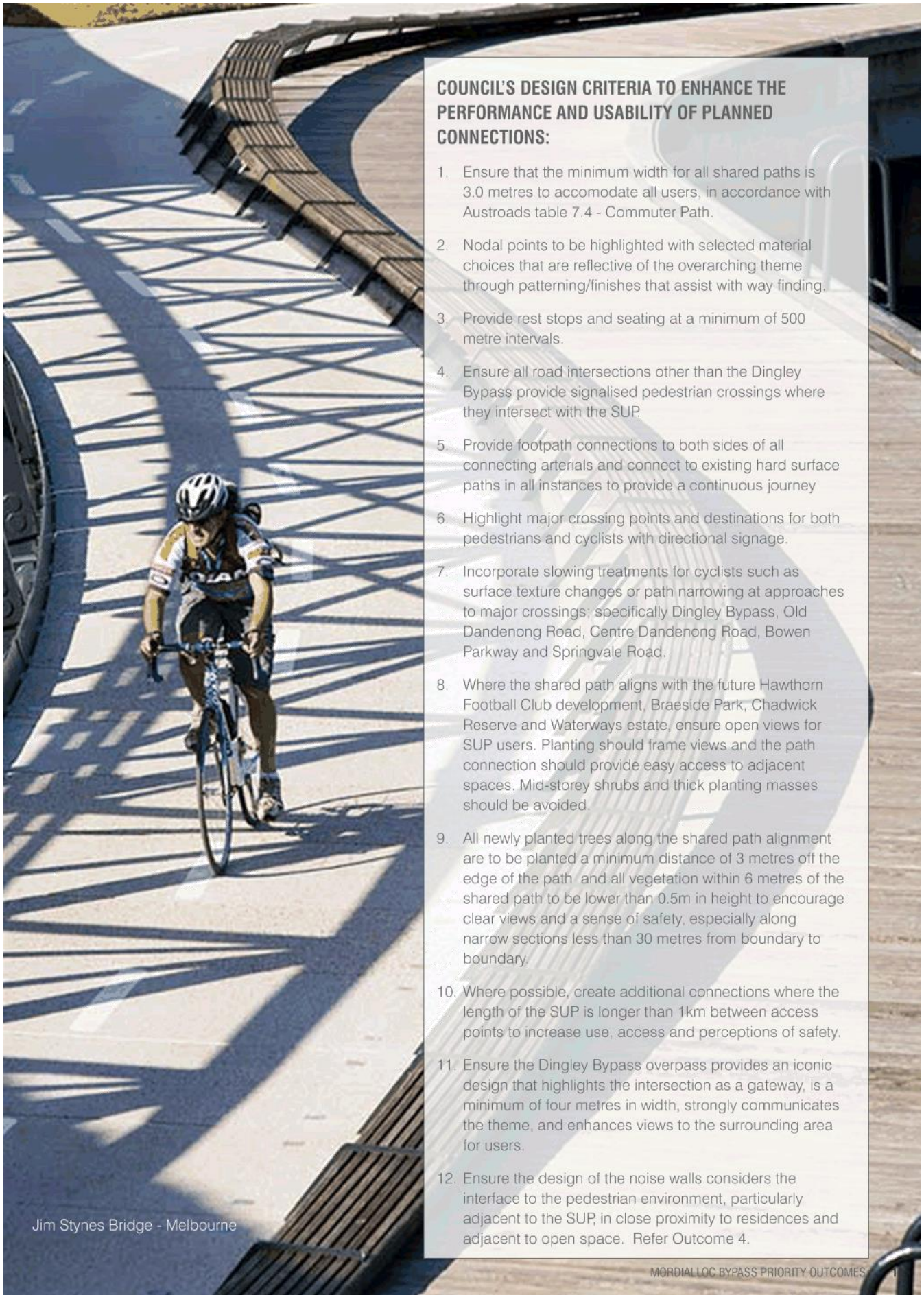
Other factors that can pose a barrier to users include;

- ▶ Indirect routes and inadequate path widths
- ▶ Surface materials that are difficult to traverse
- ▶ Heat exposure/ lack of shade
- ▶ Lack of rest points
- ▶ Unsafe spaces; such as narrow, linear spaces with limited visibility, natural surveillance or escape points
- ▶ Lack of way finding treatments

The proposed SUP located on the eastern side of freeway is a critical strategic walking and cycling link for the broader region. It is more than a local connection as it forms a key route within the broader network, linking to employment and education centres. It is imperative that every effort is made to improve its usability. Likewise all connections and pathways implemented as part of the project should use architectural interventions, landscape treatments and wayfinding elements that encourage use.

The current arrangement of noise walls creates an excessive sense of enclosure for both the motorist and pedestrian environments in some sections of the corridor. In particular, trail users will be directed through a 500 metre long, narrow section between 3-6 metre high noise walls and residential back fences adjacent to Dingley Village, varying in width from approximately 30 metres down to 7 metres. This arrangement will require treatments that ensure public surveillance is maximised and a sense of safety and comfort are achieved for users.

The currently proposed shared use path in the southern section, diverts users onto Bowen Parkway and around the western edge of the wetlands before reconnecting with the planned SUP to Springvale Road. As such, it does not provide an attractive and direct connection in this location.



Jim Stynes Bridge - Melbourne

COUNCIL'S DESIGN CRITERIA TO ENHANCE THE PERFORMANCE AND USABILITY OF PLANNED CONNECTIONS:

1. Ensure that the minimum width for all shared paths is 3.0 metres to accommodate all users, in accordance with Austroads table 7.4 - Commuter Path.
2. Nodal points to be highlighted with selected material choices that are reflective of the overarching theme through patterning/finishes that assist with way finding.
3. Provide rest stops and seating at a minimum of 500 metre intervals.
4. Ensure all road intersections other than the Dingley Bypass provide signalised pedestrian crossings where they intersect with the SUP.
5. Provide footpath connections to both sides of all connecting arterials and connect to existing hard surface paths in all instances to provide a continuous journey
6. Highlight major crossing points and destinations for both pedestrians and cyclists with directional signage.
7. Incorporate slowing treatments for cyclists such as surface texture changes or path narrowing at approaches to major crossings; specifically Dingley Bypass, Old Dandenong Road, Centre Dandenong Road, Bowen Parkway and Springvale Road.
8. Where the shared path aligns with the future Hawthorn Football Club development, Braeside Park, Chadwick Reserve and Waterways estate, ensure open views for SUP users. Planting should frame views and the path connection should provide easy access to adjacent spaces. Mid-storey shrubs and thick planting masses should be avoided.
9. All newly planted trees along the shared path alignment are to be planted a minimum distance of 3 metres off the edge of the path and all vegetation within 6 metres of the shared path to be lower than 0.5m in height to encourage clear views and a sense of safety, especially along narrow sections less than 30 metres from boundary to boundary.
10. Where possible, create additional connections where the length of the SUP is longer than 1km between access points to increase use, access and perceptions of safety.
11. Ensure the Dingley Bypass overpass provides an iconic design that highlights the intersection as a gateway, is a minimum of four metres in width, strongly communicates the theme, and enhances views to the surrounding area for users.
12. Ensure the design of the noise walls considers the interface to the pedestrian environment, particularly adjacent to the SUP, in close proximity to residences and adjacent to open space. Refer Outcome 4.

MORDIALLOC BYPASS PRIORITY OUTCOMES

MOVEMENT AND CONNECTIONS

OUTCOME 2 CONTINUED

LINKS AND JOURNEYS ARE WELL USED: INTEGRATED, DIRECT, ACCESSIBLE, LEGIBLE, ATTRACTIVE AND SAFE

COUNCIL REQUESTS THAT THE FOLLOWING IMPROVEMENTS ARE MADE TO PROPOSED CONNECTIONS AS PART OF THE MORDIALLOC BYPASS PROJECT;

1. A PEDESTRIAN OVERPASS AT DINGLEY BYPASS

- ▶ Provide an overpass at the Dingley Bypass intersection instead of a signalised intersection to create an attractive, safe and direct connection to the existing shared use trail north of the Bypass and future 'Chain of Parks' network.

2. WIDEN THE PROPOSED UNDERPASS

- ▶ Ensure the underpass connection is a minimum of 6 metres wide. Currently the performance requirements state the minimum width requirement is 3.4 metres with a preferred nominal width of 6 metres. Anything less than 6 metres wide will create an unwelcoming space. Further design treatments should be incorporated into any underpass design. Refer design criteria on following page.

3. CREATE A MORE DIRECT OFF ROAD CONNECTION AT BOWEN PARKWAY/ WATERWAYS ESTATE

- ▶ Achieve the most direct route possible for cyclists from Bowen Parkway to the south side of the Mordialloc Creek. Consider a suspended path system over the sensitive Waterways wetland.

4. CONNECTIONS AT ASPENDALE GARDENS

- ▶ Ensure that the proposed SUP adjacent to Aspendale Gardens provides connections that link with the current reserve and footpath network, for example at Ferntree Grove and Bungalow Way.

PRECEDENTS



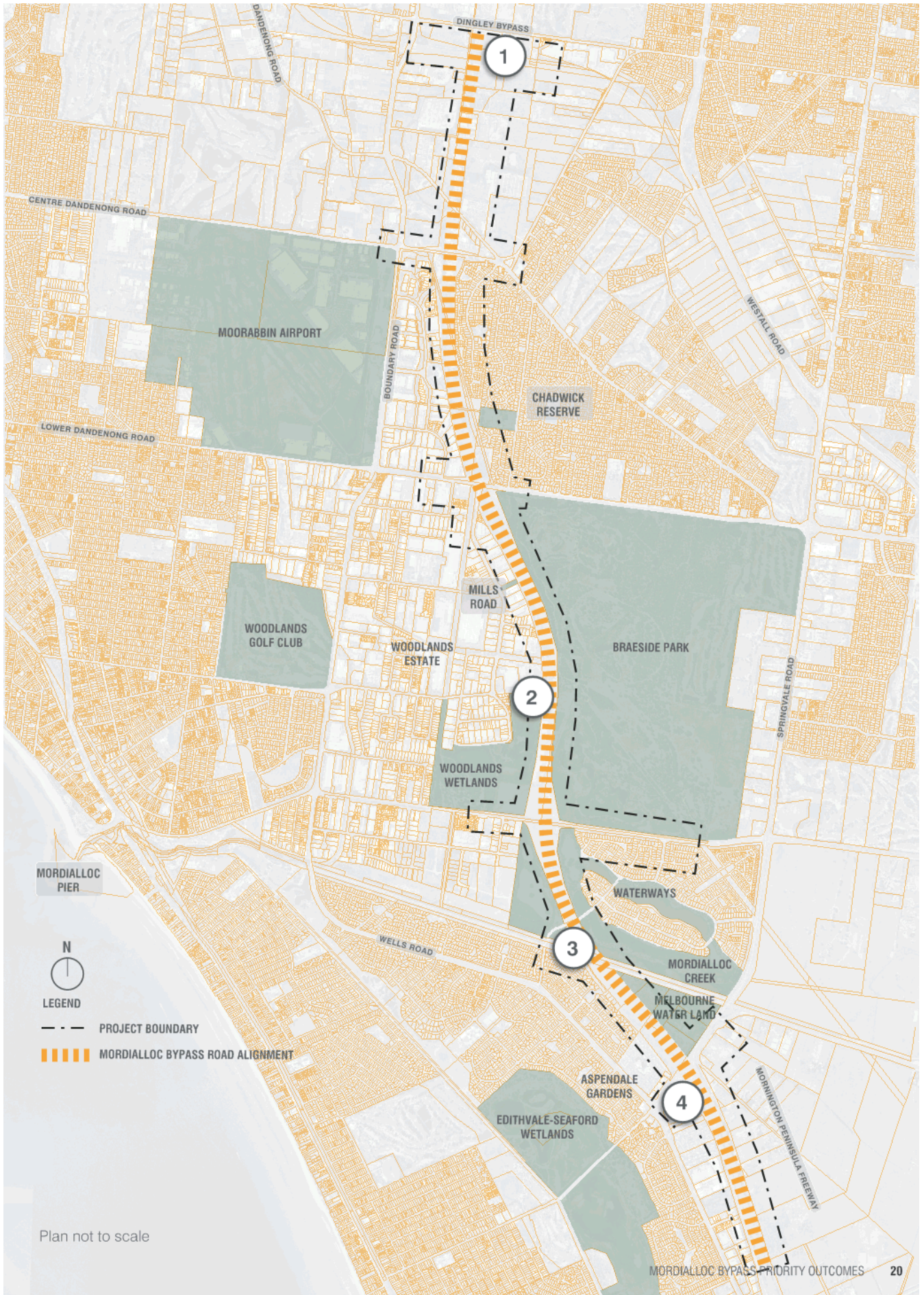
Craigieburn Bypass- Pedestrian overpass



Bowen place - Canberra - Widened underpass



Western Distributor - Melbourne - Proposed suspended cycle way



MOVEMENT AND CONNECTIONS

OUTCOME 2 CONTINUED

LINKS AND JOURNEYS ARE WELL USED: INTEGRATED, DIRECT, ACCESSIBLE, LEGIBLE, ATTRACTIVE AND SAFE

UNDERPASS DESIGN

Although the current design performance requirements request 'adequate lighting', splayed wing walls and a 'sense of openness' at entries to the underpass, performance requirements need to be strengthened to ensure the best possible outcome.

Considering that the connection is 25 to 30 metres in length, Council would also like to see higher standards for lighting, including penetration of daylight, and high quality wall treatments to ensure a safe, attractive and well-used crossing environment is provided.

The design should seek to reduce the extent of wing walls wherever possible and incorporate treatments that improve their appearance and deter graffiti.



Proposed pedestrian underpass at Braeside Park - Aerial view (source: <https://roadprojects.vic.gov.au/projects/mordialloc-freeway/view-the-ees>). Narrow entries create a dark, inhospitable and potentially unsafe environment.



Proposed pedestrian underpass at Braeside Park - ground level (source: <https://roadprojects.vic.gov.au/projects/mordialloc-freeway/view-the-ees>). Sheer wing-walls create a harsh entry appearance and palette for graffiti.

COUNCIL'S UNDERPASS DESIGN CRITERIA;

In addition to current design guidance;

1. Ensure that dimensions of no less than 6 metres wide are adopted to ensure generous visibility from one end of the tunnel to the other.
2. Wherever possible, allow natural light to penetrate the connection between the carriageways.
3. Reduce the extent and apply a cladding, artwork or an integral pattern to underpass head and wing walls that is reflective of the theme and assists in reducing graffiti.
4. In addition to the application of CPTED (Crime Prevention Through Environmental Design) principles ensure a Safety In Design analysis is undertaken.
5. Wing walls to be splayed at a maximum angle to ensure the tunnel is not elongated.
6. Materials used should be consistent with the overarching theme, such as natural tonal colours that reflect surrounding open space (also refer Outcome 4).
7. Incorporate planting treatments at the underpass entries that soften and integrate wing walls.
8. Create generous and welcoming spaces at the entries to the underpass with multi-directional surveillance and clear lines of sight maximised within 30 metres of the entry.

PRECEDENTS



Bowen Place - Canberra. Natural light to central underpass



Drysdale Bypass splayed wing walls with integration of planting and cladding to create an attractive and welcoming environment.



Jim Stynes Bridge. Clear view lines through underpass.

MOVEMENT AND CONNECTIONS

OUTCOME 3

IMPORTANT COMMUNITY CONNECTIONS ARE MAINTAINED AND STRATEGIC FUTURE CONNECTIONS ARE ALLOWED FOR

The current design proposes pedestrian crossing points at road intersections and at one other location along the length of the 9.7km bypass. This is considered inadequate to maintain current connections that are well used by the community or have potential to be. The Green Wedge Plan calls for "frequent habitat links and pedestrian/cycle crossing points (along the Bypass) that meet safety standards and guidelines."

EXISTING CONNECTIONS

The *Kingston Health and Wellbeing Plan (2013-2017)* outlines the value in maintaining and improving existing connections to local employment precincts, open space and homes as a way of encouraging active transport and associated health benefits.

Whilst pedestrian counts have been undertaken to inform the inclusion of a priority underpass connection at Braeside Park to Woodlands Industrial Estate, investigations have not considered the opportunities to improve connectivity.

Although informal, a critical link exists at Chadwick Reserve through to Garden Boulevard, connecting the residents of Dingley Village with a range of services, employment destinations and a community garden within the Woodlands Estate.

STRATEGIC FUTURE CONNECTIONS

The Kingston Green Wedge Plan identifies the future pedestrian and cycling network that supports the 'Chain of Parks' vision. This future network includes an important SUP link along the western edge of the freeway. This path will enable residential communities and workers east of the freeway to connect to community destinations such as Karkarook Park, Patterson Lakes Recreation Reserve and link to planned walking trails. The current plan has precluded the development of this link in future in some locations; such as immediately north of Centre Dandenong Road.

Another connection identified is a walking path along the Mordialloc Creek in the north. The current design includes provision for movement of water under the freeway and habitat connections at this location, however pedestrians will not be able to connect to the western side of the freeway to the future walking path.

A future path connection is identified through the triangular Melbourne Water site at the southern end of the project area to the East Link Trail. Adequate space must be allowed for beneath bridge infrastructure to ensure this connection can be established in future.

CONSIDERING THE STRATEGIC VISION FOR THE REGIONAL CYCLING AND PEDESTRIAN NETWORK, COUNCIL REQUESTS THAT THE FOLLOWING OUTCOMES ARE ACHIEVED AS PART OF THE MORDIALLOC BYPASS PROJECT;

1. UNDERBRIDGE CONNECTIONS

- ▶ Allow sufficient space under the Waterways overpass, south of Mordialloc Creek, for a future 3 metre wide SUP connection to Pillars Road and the East Link Trail via the Melbourne Water site.

2. SECOND SIGNALISED PEDESTRIAN CROSSING

- ▶ Make provision for a future, second signalised pedestrian crossing east-west at the Dingley Bypass intersection to facilitate a connection from the eastern SUP to the future western SUP and Mordialloc Creek walking trail identified in the Green Wedge Plan. This connection will also enable links to future community hubs and open space assets at the month of the project area for workers and residents on the western side of the freeway.

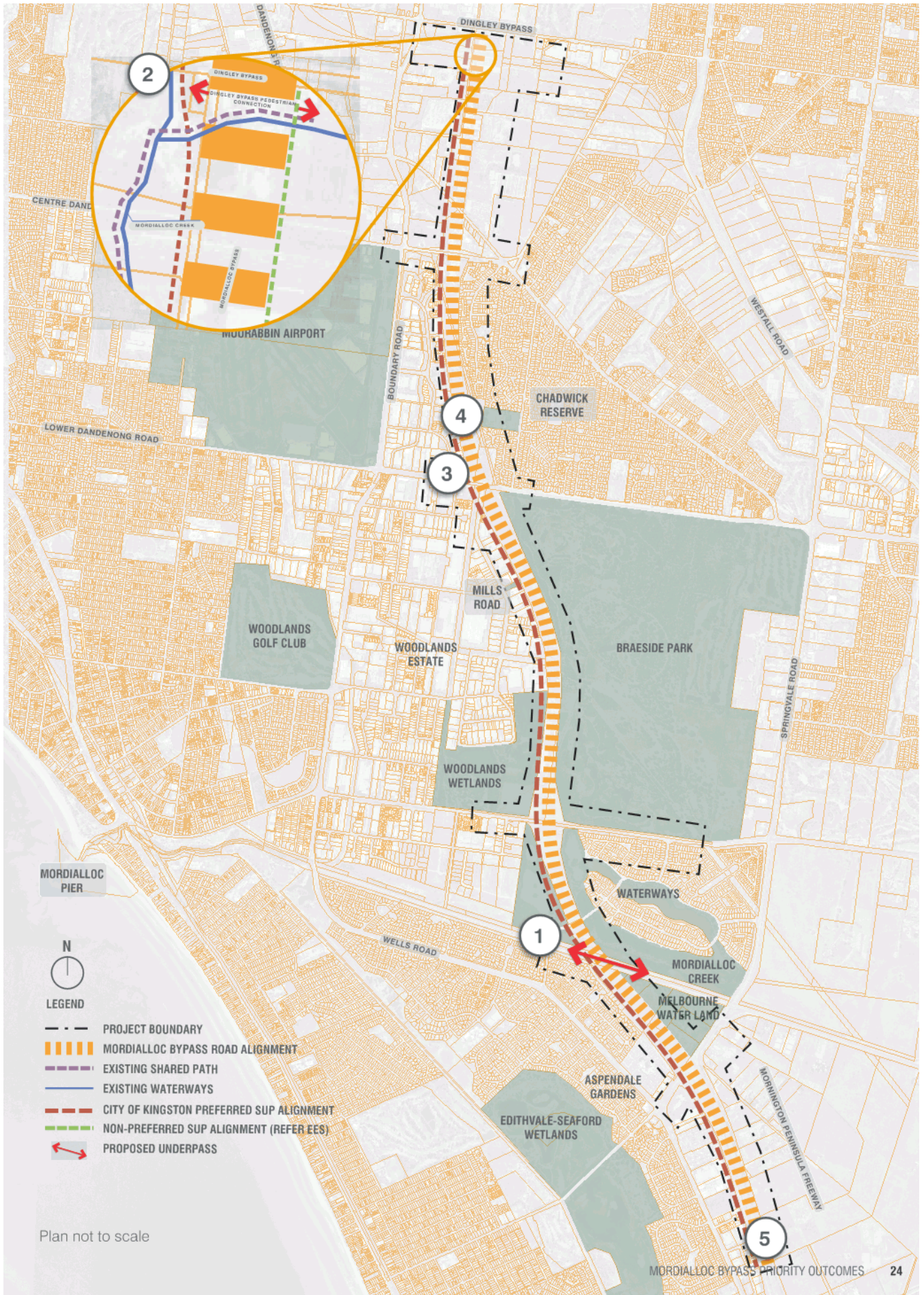
3. WESTERN SUP RESERVE

- ▶ Allow sufficient space for a future 3 metre wide shared path connection along the length of the western side of the freeway. Ensure currently proposed cross-freeway connections are designed to provide a connection to the future SUP.

4. CHADWICK RESERVE TO GARDEN BOULEVARD IN THE WOODLANDS ESTATE.

- ▶ Formalise the existing connection between the Dingley Village residential area and existing community services, workplaces and destinations on the other side of the corridor (Redwood Gardens) with an underpass. Integrate the path connection at this location with the wetland treatment proposed at Outcome 1.

5. CONNECT THE SUP TO THE PENINSULA LINK TRAIL



09.

COUNCIL'S PRIORITY OUTCOMES



AMENITY

Peninsula Link - Pedestrian Overpass



AMENITY

OUTCOME 4

EFFECTIVE SHORT AND LONG TERM VISUAL SCREENING - NOISEWALLS AND VEGETATION

Sensitive views to the roadway from SUP's and residential interfaces require adequate screening in the short term (from high quality structures) and long term from vegetation. Likewise unattractive interfaces to the road environment such as industrial buildings require screening treatments.

Council is pleased to see that extensive tree planting is proposed at industrial interfaces, at intersections and in available areas adjacent to the road corridor. However there are a number of locations where there is no space available for tree planting along the roadway. For long sections (sometimes over a kilometre) the motorist will have an immediate interface with noisewalls varying in height from 0.5 to 6 metres.

Effective short-term screening relies largely on the careful placement of noise walls and the quality of their design. The design outcome for the noisewalls must address both the motorist experience and pedestrian/residential interfaces. Given the long term realisation of intended screening using vegetation, the noisewalls must stand alone as a quality interface before the adjacent vegetation matures.

The interfaces and transition of noise walls should be well considered. The current design shows a 'stop-start' approach to the placement of noisewalls. The use of tapering at key locations will help integrate the walls and allow for a better Urban Design outcome.



Currently proposed SUP/noisewall interface (source: <https://roadprojects.vic.gov.au/projects/mordialloc-freeway/view-the-ees>). A mix of shrubs and trees should be provided to soften the noisewall edge (Refer Peninsula link - Image 1).



The proposed interface between noisewalls above shows an abrupt transition. (source: <https://roadprojects.vic.gov.au/projects/mordialloc-freeway/view-the-ees>). Transitions should taper and integrate with adjacent forms (refer image 2 Deer Park Bypass) or continue at a consistent height rather than creating a sudden drop as pictured above.

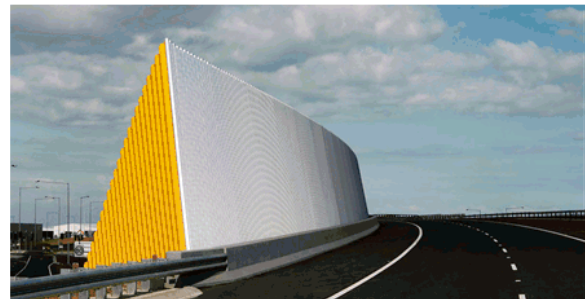
COUNCIL'S DESIGN CRITERIA FOR THE NOISEWALL AND SCREENING VEGETATION TREATMENTS;

1. Ensure that the Governor Road overpass incorporates a high quality noisewall finish that assists in reducing visual bulk and enhances the presentation of the bridge when viewed from the Waterways Estate.
2. Where panels are required to change in height ensure transition or tapering is carefully considered.
3. Where possible mounding should be used to minimise the heights of the noise walls with the addition of planting to help soften the wall's verticality.
4. Noisewall materials must compliment the natural setting, be recessive and use a muted colour palette. Bright iridescent colours should not be used.
5. Perforated patterns within the plexi panels sections should reflect the broader theme - Refer outcome 1.
6. Support structures shall be integrated and or contained within the panels so that they are hidden on both sides of the wall.
7. Offset of noisewall from the shared path to be a minimum distance of 6 metres to allow for screening vegetation without interrupting view lines.
8. All trees planted adjacent to noisewalls or within the road reserve at wetlands, residential and industrial interfaces to be installed as advanced stock (minimum 15L).

PRECEDENT



1. Peninsula Link - Well selected shrubs and small trees soften the edge of the noisewalls and compliment the architecture.



2. Deer Park Bypass - The ends of the noisewalls taper to provide a more considered architectural outcome.



3. East Link - Walls should be a consistent height wherever possible, as pictured above.

AMENITY

OUTCOME 5

HIGH QUALITY, VISUALLY RECESSIVE BRIDGE STRUCTURES

Clause 22.04-3 of the Kingston planning scheme sets out Council policy that all planning outcomes in the Green Wedge must result in an urban form which is of a high design standard and low visual impact.

The overpass structures along the corridor vary from 'Cut Throughs' without Blade Pier (Old Dandenong Road and Centre Dandenong Road), 'Cut Throughs' with Blade Pier and Barrier (Lower Dandenong Road) and 'Spill Through' with Blade Support and Barriers (Governor Road and Springvale Road). The design intent for these structures is yet to be resolved through the detailed design process, however Council recognises their importance as thresholds for motorists and pedestrians crossing the Bypass corridor. Achieving a quality outcome at an appropriate scale that communicates a clear identity and design consistency is imperative.

The most significant structure proposed is the bridge over the Waterways wetlands. The current design is driven by reducing impacts to sensitive ecological values and flow-paths (minimising piers) and achieving the required height over the Bowen Parkway and flood levels of the wetlands. This height requirement will result in a visually prominent structure, particularly when viewed from the Waterways Estate. Similarly it will allow for expansive views across the region.

Adjacent batters to all bridges should be planted to help soften the bridge abutments and integrate them with the surrounding landform.

Where cut through's occur, wall interfaces should be integrated with parapets and gantries to avoid harsh structural outcomes. Patterning and form that relates to the theme of the road corridor should be incorporated to increase amenity and consistency from a range of vantage points.



Indicative bridge design shows bulky heastock and piers (source: <https://roadprojects.vic.gov.au/projects/mordialloc-freeway/view-the-ees>). Piers and headstock should be minimal in scale and slender of form to ensure low visual impact. All batters should be planted rather than grassed to help soften the bridge structure and for ease of maintenance.



View to proposed bridge abutment (source: <https://roadprojects.vic.gov.au/projects/mordialloc-freeway/view-the-ees>) Abutment walls should be designed with the interface of the bridge parapets and balustrades in mind. Pattern and colour should also be considered to strengthen the design theme, minimise graffiti, assist in wayfinding and create an attractive roadside environment.

COUNCIL'S DESIGN CRITERIA FOR BRIDGE STRUCTURES;

1. Cladding or patterning should be applied to the blade supports and barriers that reflects the theme of the corridor. Refer Outcome 1.
2. Design treatments should be applied to both abutments and the undercroft of the Waterways Wetlands bridge, to minimise the visual impact at the pedestrian scale and to reflect the landscape character of the wetlands. The treatment should be based on the overarching corridor theme for consistency. Refer Outcome 1.
3. Integration between plexi-glass panels and fixing to road barrier treatments and, if applicable, weathered steel noise walls should be galvanised and hidden for a seamless application.
4. Plexi-glass panels should not be stepped and are to be visually consistent with the weathered steel noise walls.
5. The abutment for the bridge to the Bowen Parkway is to incorporate raked retaining walls or sloped rock beaching to ensure a recessive look is achieved that allows minimal impact on view lines for cyclists and pedestrians.

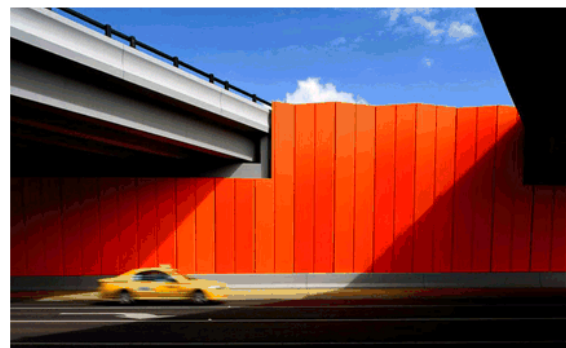
PRECEDENTS



East Link - Use of rounded edges to the headstock and narrowed blade piers softens the form of the overall structure.



East Link - Plexi-glass panel noise walls are consistent in height rather than stepped.



Deer Park Bypass - integration of the abutment wall and bridge provides a seamless and clean design outcome.

AMENITY

OUTCOME 6

THE SENSITIVE INTERFACES OF BRAESIDE PARK, DINGLEY VILLAGE, ASPENDALE GARDENS AND WATERWAYS COMMUNITIES ARE WELL MANAGED IN THE DESIGN RESPONSE

The current design for the noisewalls has been developed in response to recommendations of an acoustic impacts study. Walls are located where noise impacts will exceed acceptable levels adjacent to residential areas. There is currently no legislative requirement to address noise impacts adjacent to open space or industrial areas. As such, there are no proposed noise mitigation treatments adjacent to Braeside Park, Woodlands Industrial Estate or undeveloped sections of the Green Wedge near the Dingley Bypass and Springvale Road.

The protection of recreational amenity and conservation values of Braeside Park is considered to be of particular importance. Noise impacts need to be mitigated to the same level as residential areas to protect the quiet parkland environment for recreational users and fauna, particularly birdlife.

Whilst the proposed noisewalls aim to address the noise impacts to residential areas, the height in some locations comes within 6 metres of residential fences. These instances require careful design consideration to mitigate visual impacts from sensitive viewpoints and ensure optimum solar access.

The most prominent visual impact of the development will be from the Waterways Estate, where the Bypass infrastructure will be highly visible across the open wetlands.

Disturbance to sensitive areas, particularly residential interfaces, wetland habitat and the habits of birdlife and other fauna, from construction activities must be well considered, with rigorous requirements incorporated in the Construction Management Plan.



Where the SUP runs adjacent to noise walls, they should not appear to be the 'back side' as shown above. An attractive, pedestrian scale outcome should be achieved.



Close proximity of noisewalls to pathways can create a negative sense of enclosure and an exaggerated perception of the scale of the walls. Transparent noise walls should be proposed where they are located within 40 metres of shared user paths.

IN ADDITION TO THE CRITERIA OUTLINED IN OUTCOME 4, COUNCIL REQUESTS THE ADOPTION OF THE FOLLOWING SENSITIVE INTERFACE DESIGN CRITERIA;

1. A transparent noisewall must be installed at the interface of Braeside Park to protect this significant conservation and recreation asset from noise impacts.
2. Noisewalls that interface with the SUP must not appear to be the 'back side' and incorporate treatments that are of pedestrian scale.
3. Noisewalls that come within 40 metres of residential areas and the SUP must be transparent.
4. Advanced tree planting must be undertaken close to Waterways residences to interrupt views to the bridge.
5. The Waterways overpass must be a high quality bridge design that is visually recessive - refer Outcome 5.
6. Construction impacts on the waterways should be addressed and management plans submitted to Council for review and approval.
7. Exclusion zones that apply to all open space and existing vegetation, must be developed with input from Council and adhered to throughout the construction phase.



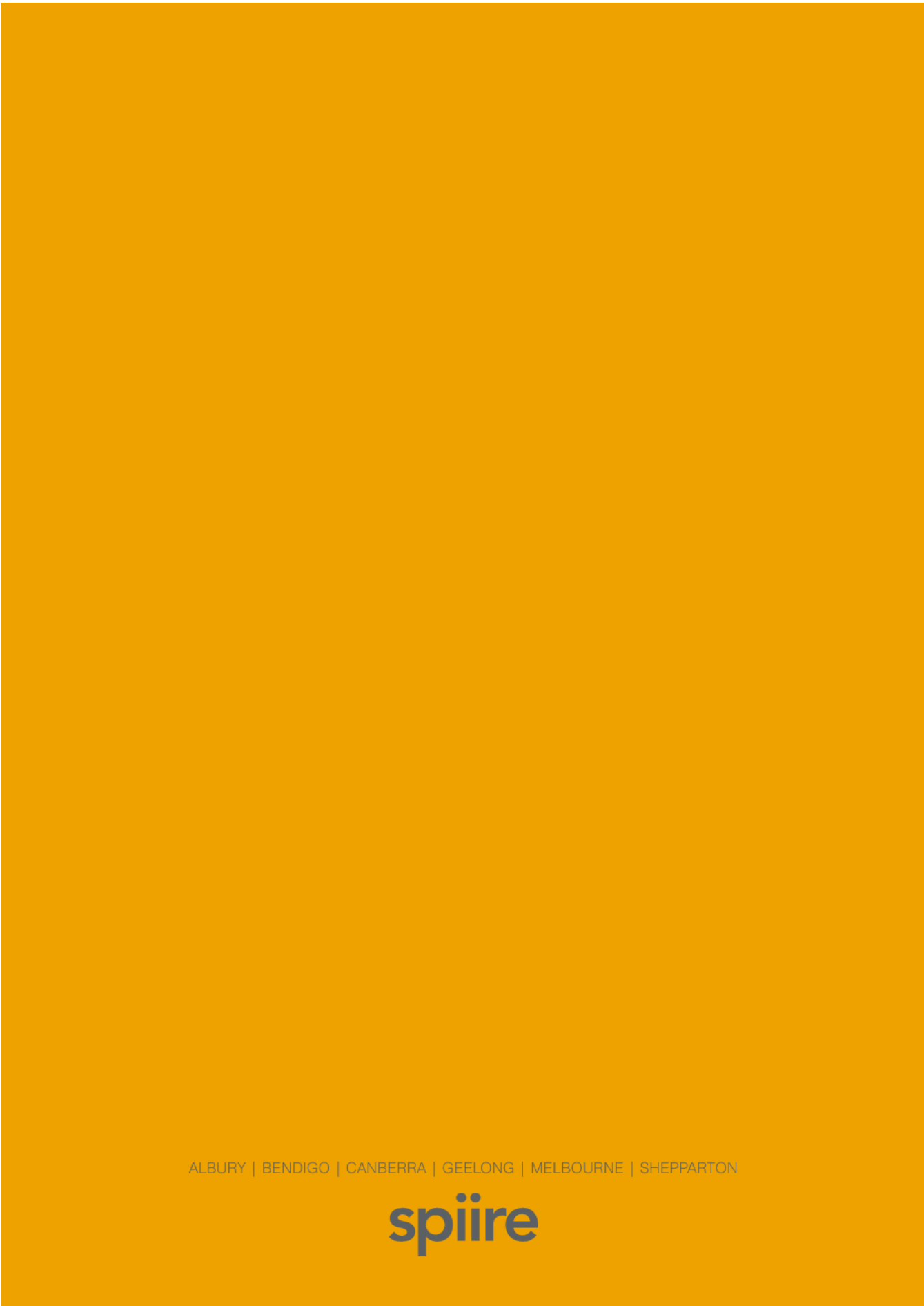
Peninsula Link - Advanced tree planting is used adjacent to bridges to soften the visual impact.



East Link - Transparent noise walls to be used adjacent to open space.



Waterways Wetland - Protection of all open space especially the waterways wetland must be priority throughout the construction phase.



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spiire

Ordinary Meeting of Council

10 December 2018

Agenda Item No: 8.6

MORDIALLOC FREEWAY - DRAFT LANDSCAPE AND VISUAL DESIGN ASSESSMENT

Contact Officer: Justin Kelly, Urban & Sustainable Design Advisor

Purpose of Report

The purpose of this report is for Council to adopt the Landscape and Visual Design Assessment of the proposed Mordialloc Freeway project to form the basis of Council's priority outcomes to be negotiated through the Environmental Effects Statement (EES) process being conducted.

Disclosure of Officer / Contractor Direct or Indirect Interest

No Council officer/s and/or Contractor/s who have provided advice in relation to this report have declared a Conflict of Interest regarding the matter under consideration.

OFFICER RECOMMENDATION

That Council:

1. Note the community feedback received during the consultation period.
2. Adopt the 'Mordialloc Freeway – Priority Project Outcomes' report at Appendix 1.

1. Executive Summary

The Mordialloc Freeway is a proposed new 9km freeway linking the end of the Mornington Peninsula Freeway at Springvale Road to the Dingley Bypass. The Minister for Planning has determined to carry out an Environment Effects Statement (EES) for the project. The EES will consider the potential environmental, social, economic and cultural impacts, and how these will be managed.

Concurrent to the EES process, officers have engaged Spiire (Landscape Architects) to undertake a critical review of the landscape and urban design effects and the visual impact of the Mordialloc Freeway.

The work progressed by Spiire has been informed by consideration of Council's Notices of Motions (22/2017 and 44/2017). The report prepared by Spiire considers both positive and negative implications of the proposed design and identifies a series of 'Priority Outcomes' for Council to use to influence the Major Roads Projects Authority during, and subsequent to, the EES process.

A number of priority outcomes are identified in the report that are considered to be critical to the success of the project. These outcomes build on, and seek to deliver, the intent of previous Council Notices of Motion (22/2017 and 44/2017) in relation the project. These priority outcomes are provided below, with further detail in relation individual objectives provided within the report at Appendix 1:

1. The development tells a clear story about place – *A region invested in an environmentally progressive future.*
2. Important community connections are maintained.
3. Links and journeys are integrated, direct, accessible, legible, attractive and safe. Allowance is provided for strategic future connections.
4. Effective short and long term visual and acoustic screening is provided.
5. The design incorporates high quality, visually recessive bridge structures.
6. The sensitive interfaces of Braeside Park, Dingley Village, Aspendale Gardens and Waterways communities are well managed in the design response.

Consultation in relation the draft report was undertaken from November 14 to December 3 2018 and included promotion via social media, Your Kingston Your Say and a mail out to 1,255 residents and landowners immediately surrounding the Freeway alignment.

During this period there were 181 visits to the KingstonYourSay webpage with 10 respondents leaving feedback on the guestbook and a further 6 individual submissions received via email. Feedback received suggested broad support for the direction of the Spiire work with some changes proposed in response to feedback as outlined under section 3.2 of this report.

It is proposed that the work completed by Spiire will be used to advocate for changes to the design as proposed by the Major Road Projects Authority (MRPA) Design Team and their successful tenderers. The findings will also provide a foundation for future Council advocacy through the Environmental Effects Statement process and hearing, currently scheduled for late February 2019. This will be supported by a legal advocate and a series of appointed experts.

2. Background

Council Notices of Motion

Council has passed two Notices of Motion on 22 May 2017 and 25 September 2017 outlining a range of issues to be resolved collaboratively with VicRoads. A number of these issues have been resolved through subsequent discussions with VicRoads/MRPA, with the balance of issues, where relevant to visual landscape and urban design outcomes, addressed in the Council initiated 'Priority Project Outcomes' report at Appendix 1. Page seven of the attached report provides a summary of the Council Notices of Motion and the extent to which matters are addressed in the current design.

Mordialloc Freeway Project

Officers have met regularly with VicRoads and the MRPA since early 2017 to provide feedback and input as the Mordialloc Freeway design has progressed. Most notably, this has included officer representation on the Technical Reference Group established by the MRPA as part of the Environmental Effects Statement (EES) process.

The Mordialloc Freeway is a proposed new 9km freeway linking the end of the Mornington Peninsula Freeway at Springvale Road to the Dingley Bypass. The Minister for Planning has determined to carry out an Environment Effects Statement (EES) for the project. The EES will consider the potential environmental, social, economic and cultural impacts, and how these will be managed.

Key features of the Reference Design prepared by the MRPA include:

- A dual carriageway with two to three lanes in each direction.
- Bridges over Springvale, Governor, Lower Dandenong and Centre Dandenong Roads, along with new freeway entry and exit ramps.
- Bridges over Old Dandenong Road and the sensitive wetlands in the Waterways area.
- A new shared walking and cycling path along the length of the freeway on the eastern side.
- A single pedestrian underpass is proposed between Braeside Park and Woodlands industrial precinct.
- Noise walls will be included adjacent to residential areas.
- Fauna exclusion fences adjacent to Braeside Park and wetlands and habitat crossing points for fauna under the freeway.
- Tree planting and landscaping is proposed along the length of the corridor.

Council Officers have engaged Spiire (Landscape Architects) to undertake a critical review of the landscape and urban design effects and the visual impact of the Mordialloc Freeway.

The work progressed by Spiire has been informed by consideration of Council's Notices of Motion's (22/2017 and 44/2017). The report prepared by Spiire considers both positive and negative implications of the proposed design and identifies a series of 'Priority Outcomes' for Council to lobby the MRPA during and subsequent to the EES process.

Environmental Effects Statement Process

The Mordialloc Freeway project is currently in tender design phase, which is running concurrently with an EES process. The EES investigation commenced, following the Minister's endorsement of the scoping requirements, in May 2018.

The EES and associated documentation is currently on public exhibition until 14 December 2018 and it is anticipated that an Advisory Committee Hearing will occur in February/March 2019. Officers are currently preparing a submission to the EES for Council consideration through the December meeting cycle.

3. Discussion

3.1 Council Plan Alignment

Goal 1 - Our well-planned, liveable city supported by infrastructure to meet future needs

Direction 1.2 - Effectively influence the urban and architectural design of the City

The report at Appendix 1 identifies a range of priority outcomes, which, if incorporated into the MRPA's reference design would achieve improved landscape and urban design outcomes.

3.2 Consultation/Internal Review

The EES is currently on public exhibition and the MRPA are accepting submissions from the public until 14 December 2018. Council's detailed submission to the EES is provided in IC18/1955, noting that it is proposed that the Spiire report at Appendix 1 be included as an attachment to the Council submission.

Internal consultation has been undertaken with relevant Departments including traffic and transport, infrastructure and parks. This has involved meetings to discuss design issues around the freeway corridor and review of the report produced by Spiire. Officers have also engaged with the Major Road Projects Authority and VicRoads throughout the preparation of the report.

Officers have continued to engage with representatives from Parks Victoria on the draft report to ensure that both parties have broad agreement in their position and approach, particularly as it relates to the treatment of the Freeway interface with Braeside Park.

A summary of consultation undertaken in relation the preparation of the Priority Project Outcomes Report is provided below:

3.2.1 Community Consultation

A draft of the Spiire report 'Mordialloc Freeway – Priority Project Outcomes' was presented to Council at the Councillor Information Session on 12 November. As an outcome of this briefing it was agreed that targeted community consultation would be undertaken in relation the draft report. The intent of this consultation was to test with the impacted communities and stakeholders the level of support for changes proposed in the report, prior to further Council led advocacy to MRPA.

Consultation commenced on Wednesday 14 November and included promotion via social media, Kingston Your Say and a mail out to 1,255 residents and landowners immediately surrounding the Freeway alignment. The consultation period closed on 3 December.

During the consultation period Council received 10 submissions via the guestbook on Your Kingston Your Say and 6 submissions via direct email. Over 180 people visited Council's website to view information in relation the project.

Officer's review of the community feedback received found that while some do not support the freeway project at all, most respondents were generally supportive of the steps taken by Council in preparing the Spiire review and of the Priority Outcomes identified. Specifically, the feedback sought further enhancements and improvements to the Freeway design to consider:

- The opportunity to expand and enhance Chadwick Reserve.
- The need to prioritise Westall Road extension in the broader network.
- Concerns in relation visual presentation of Governor Road overpass.
- Support for Council work and in particular the pedestrian linkage between Chadwick Reserve and Woodlands Estate
- Additional and improved shared user path connections that considers a regional scale by prioritising connections into the surrounding residential areas of Waterways, Dingley Village and Aspendale Gardens.
- Discussion around the need for the shared user path to be designed for use by children and runners, including consideration of a gravel surface.

In response to the feedback received, a number of changes have been made to the draft Spiire report that was presented to Council on 12 November. Key changes are outlined in more detail below:

- The report has been updated at page 13 and 14 to reflect suggestions to expand and enhance Chadwick Reserve through the creation of wetlands in the western portion to harvest and treat runoff from the freeway. The report also highlights the opportunity that exists for the project to provide for expanded open space and improvements to sporting fields to maximise their use across the community.
- At page 23, the report has been updated to include objectives to promote better connections into Chadwick Reserve. This includes:
 - Integration of the shared user path into the proposed wetlands and Chadwick Reserve also incorporating the proposed connection to Garden Boulevard (Woodlands Industrial Estate).
 - Ensuring that, where the shared path takes the western alignment alongside Aspendale Gardens, connections are made with current reserves and footpaths at locations such as Ferntree Grove and Bungalow Way.
- The inclusion of commentary in relation the need to advocate to extend the proposed shared user path along the Mornington Peninsula Freeway, through Chelsea Heights, to connect with the Peninsula Link Trail.
- The Amenity section on Page 28 has been strengthened with a specific reference to the importance of a high quality finish on the Governor Road overpass given the potential for visual bulk due to its visibility from Waterways.
- Commentary has been included to specify that the design of the shared user path should be appropriate for all users, including cyclists, runners, children and horse riders.

3.3 Operation and Strategic Issues

3.3.1 Mordialloc Freeway – Priority Project Outcomes Report (Spiire)

Prepared by landscape architecture and design firm Spiire, the report at Appendix 1 outlines Council's expectations with regards visual and landscape design improvements and changes sought to the Mordialloc Freeway Project (the 'Project').

The report considers a substantial volume of project information made available to Council by the Major Road Projects Authority (MRPA) and VicRoads, including; a landscape concept plan developed by VicRoads (May 2018), reference design, project performance requirements and other background information underpinning the current design intent.

The report is broadly supportive of the principles and objectives of the VicRoads reference design, however seeks to ensure that the project is an exemplar that achieves sensitive integration with the region. In this regard, the report proposes a number of changes and improvements to the current MRPA design.

Seven priority outcomes are identified in the report that are considered to be critical to the success of the project. These outcomes build on, and seek to deliver, the intent of previous Council Notices of Motion (22/2017 and 44/2017) in relation the project. These priority outcomes are provided below, with further detail in relation individual objectives provided within the report at Appendix 1:

1. The development tells a clear story about place – *A region invested in an environmentally progressive future.*
2. Important community connections are maintained.

3. Links and journeys are integrated, direct, accessible, legible, attractive and safe. Allowance is provided for strategic future connections.
4. Effective short and long term visual and acoustic screening is provided.
5. The design incorporates high quality, visually recessive bridge structures.
6. The sensitive interfaces of Braeside Park, Dingley Village, Aspendale Gardens and Waterways communities are well managed in the design response.

Within the report at Appendix 1, a series of detailed objectives and design criteria are provided to ensure that the 6 priority outcomes are achieved. The report recommends that these outcomes be considered and integrated by VicRoads and the MRPA throughout the design and development of the project

4. Conclusion

It is recommended that Council adopt the final 'Priority Project Outcomes' report at Appendix 1 and further advocate for changes to the design as proposed by the Major Road Projects Authority (MRPA) Design Team and their successful tenderers. The findings will also provide a foundation for future Council advocacy through the Environmental Effects Statement process and hearing, currently scheduled for late February 2019.

Appendices

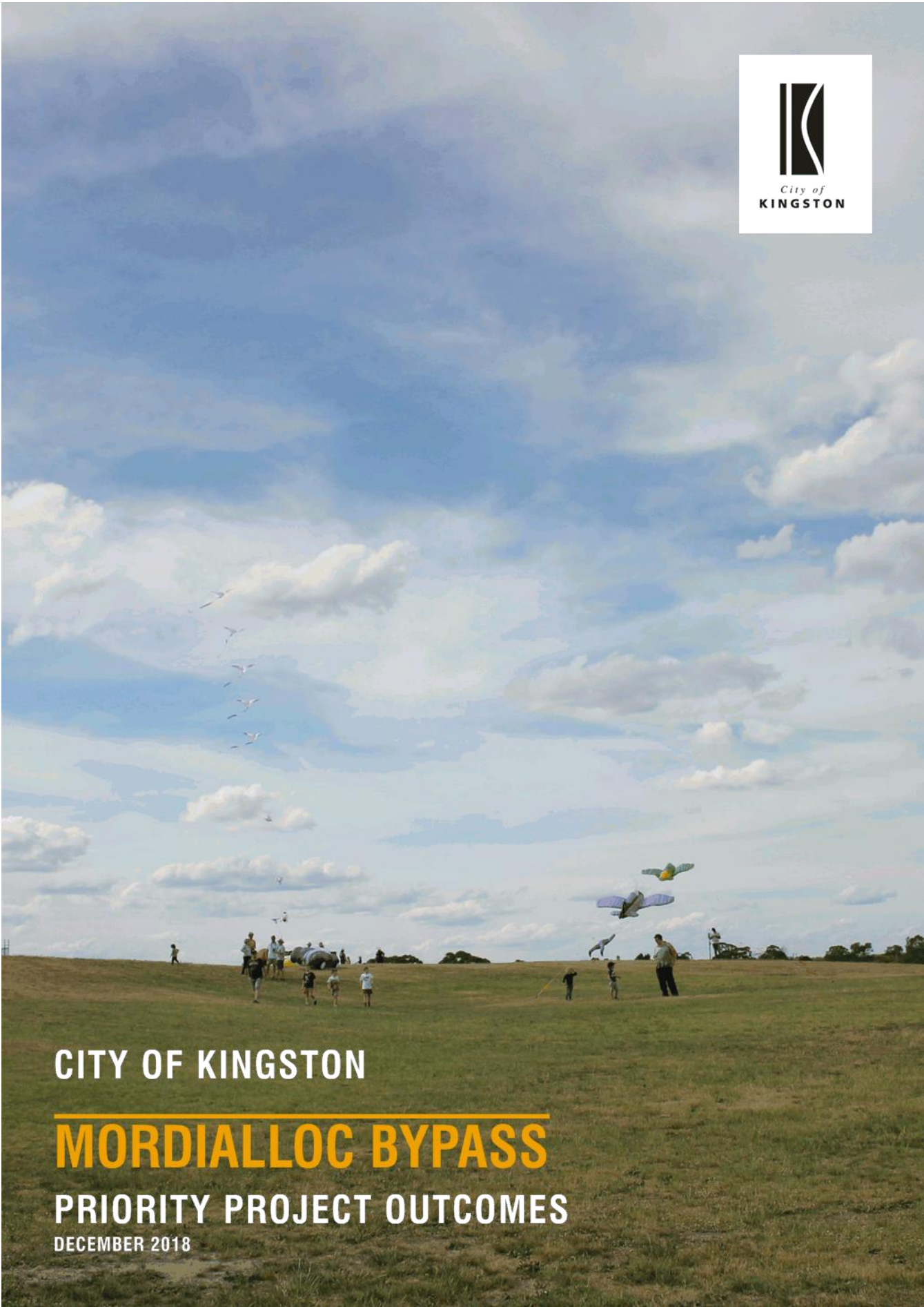
Appendix 1 - Mordialloc Freeway Landscape and Visual Design Assessment -
Priority Outcomes Report (Ref 18/623594) [↓](#)

Author/s: Justin Kelly, Urban & Sustainable Design Advisor
Reviewed and Approved By: Paul Marsden, Manager City Strategy
Jonathan Guttman, General Manager Planning and
Development

8.6

MORDIALLOC FREEWAY - DRAFT LANDSCAPE AND VISUAL DESIGN ASSESSMENT

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CITY OF KINGSTON

MORDIALLOC BYPASS

PRIORITY PROJECT OUTCOMES

DECEMBER 2018

This report was commissioned by City of Kingston and was prepared by Spiire Australia, Level 4, 414 La Trobe Street PO Box 16084 Melbourne Victoria 8007

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STATUS	DATE	COMPILED BY	AUTHORISED
0 - ISSUED FOR PUBLIC CONSULTATION	13.11.2018	T.S.	M.B.
1- COUNCIL ADOPTED POSITION	4.12.2018	T.S.	M.B.



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01.

EXECUTIVE SUMMARY

This report outlines the City of Kingston's adopted position with regards to the Mordialloc Bypass Project (the 'Project'). The report considers feedback from public consultation undertaken during November 2018 and will form part of Council's submission to Planning Panels Victoria as part of the Environmental Effects Statement process.

The report considers a substantial volume of project information made available to Council by the Major Road Projects Authority (MRPA) and VicRoads, including; a landscape concept plan developed by VicRoads (May 2018), reference design, project performance requirements and other background information underpinning the current design intent.

In October 2018 the Mayors of Glen Eira, Kingston and Monash joined together to call for an urgent commitment from the Victorian Government to extend Westall Road to link the Dingley Bypass and new Mordialloc Freeway with the Monash Freeway. The City of Kingston recognises the importance of the planned Mordialloc Bypass connection, however is still committed to ensuring that maximum efficiency of the network is realised through the prioritisation of the Westall Road link in conjunction with the delivery of the Mordialloc Bypass.

COUNCIL'S POSITION

The City of Kingston supports the principles and objectives of the VicRoads reference design, however on behalf of the community, Council is committed to ensuring that these principles and objectives are well realised and that the project is an exemplar that achieves sensitive integration with the region.

Based upon Council's review, 6 priority outcomes have been identified that are considered to be critical to the success of the project. Refer page opposite.

Council expects that these outcomes will be considered and integrated by VicRoads and the MRPA throughout the design development of the project to ensure that certain consequences are avoided.

Specifically, the project must not;

- ▶ Divide the community;
- ▶ Impede the realisation of strategic objectives for the region;
- ▶ Impact negatively on the liveability of the surrounding residential areas and workplaces; or
- ▶ Have a detrimental impact on the environmental values of the region.

COUNCIL ENGAGEMENT THROUGH DESIGN DEVELOPMENT

To ensure that the priority outcomes outlined in this report are addressed through the design development process, Council requests that the MRPA set up an appropriate engagement forum that enables Council and other key stakeholders to review information and provide feedback. For example; an Urban Design Assessment Panel (UDAP).

02

COUNCIL'S PRIORITY OUTCOMES



IDENTITY AND ENVIRONMENT

- 1 THE DEVELOPMENT TELLS A CLEAR STORY ABOUT PLACE; A REGION INVESTED IN AN ENVIRONMENTALLY PROGRESSIVE FUTURE



MOVEMENT AND CONNECTIONS

- 2 LINKS AND JOURNEYS ARE WELL USED: INTEGRATED, DIRECT, ACCESSIBLE, LEGIBLE, ATTRACTIVE AND SAFE
- 3 IMPORTANT COMMUNITY CONNECTIONS ARE MAINTAINED AND FUTURE STRATEGIC CONNECTIONS ARE ALLOWED FOR



AMENITY

- 4 EFFECTIVE SHORT AND LONG TERM VISUAL SCREENING
- 5 HIGH QUALITY, VISUALLY RECESSIVE BRIDGE STRUCTURES
- 6 THE SENSITIVE INTERFACES OF BRAESIDE PARK, DINGLEY VILLAGE, ASPENDALE GARDENS AND WATERWAYS COMMUNITIES ARE WELL MANAGED IN THE DESIGN RESPONSE

Green wedge - City of Kingston

03.

PROJECT CONTEXT

The landscape, communities and economies of the south-eastern region are taking on a new form. This change is being driven largely by population growth, transport infrastructure investment, new tourism and economic trends, and the vision of key strategies such as the *South East Green Wedge Management Plan*.

The Mordialloc Bypass is located entirely within the municipality of Kingston and passes through sections of the Green Wedge as well as industrial and residential areas. A number of important local strategic drivers and considerations affect the development of the freeway connection.

KINGSTON GREEN WEDGE

The *Kingston Green Wedge Plan 2012* and the *Sand Belt Open Space Development Plan 1994* are of particular relevance to the proposed route. They outline a vision for the region that will see creation of an extensive open space network or 'Chain of Parks' linked by walking and cycling trails, protection of significant environmental assets, support for more sustainable land use practices and regional renewable energy initiatives and the phasing out of heavy industry and landfill.

SIGNIFICANT CONSERVATION AREAS

Braeside Park sits immediately adjacent to the proposed freeway. The park covers an area of 295 hectares and contains areas of remnant native bushland, native grasslands and extensive seasonal wetlands. The park is managed by Parks Victoria and is identified in the Kingston Planning Scheme as an area of environmental significance and a recreational asset for the region.

To the south-west of the project site is the Ramsar listed Edithvale Seaford wetlands. They cover over 250 hectares and are the largest wetland of their type in the Westernport region and the only remnant of the once 4000 hectare Carrum Carrum Swamp.

The rare birds, including native and migratory species, that visit the Edithvale and Seaford Wetlands also visit the wetlands adjoining the freeway reserve; Waterways, Woodlands and Braeside Park Wetlands. There is substantial movement of birds flying across the freeway reserve to access the wetlands adjacent.

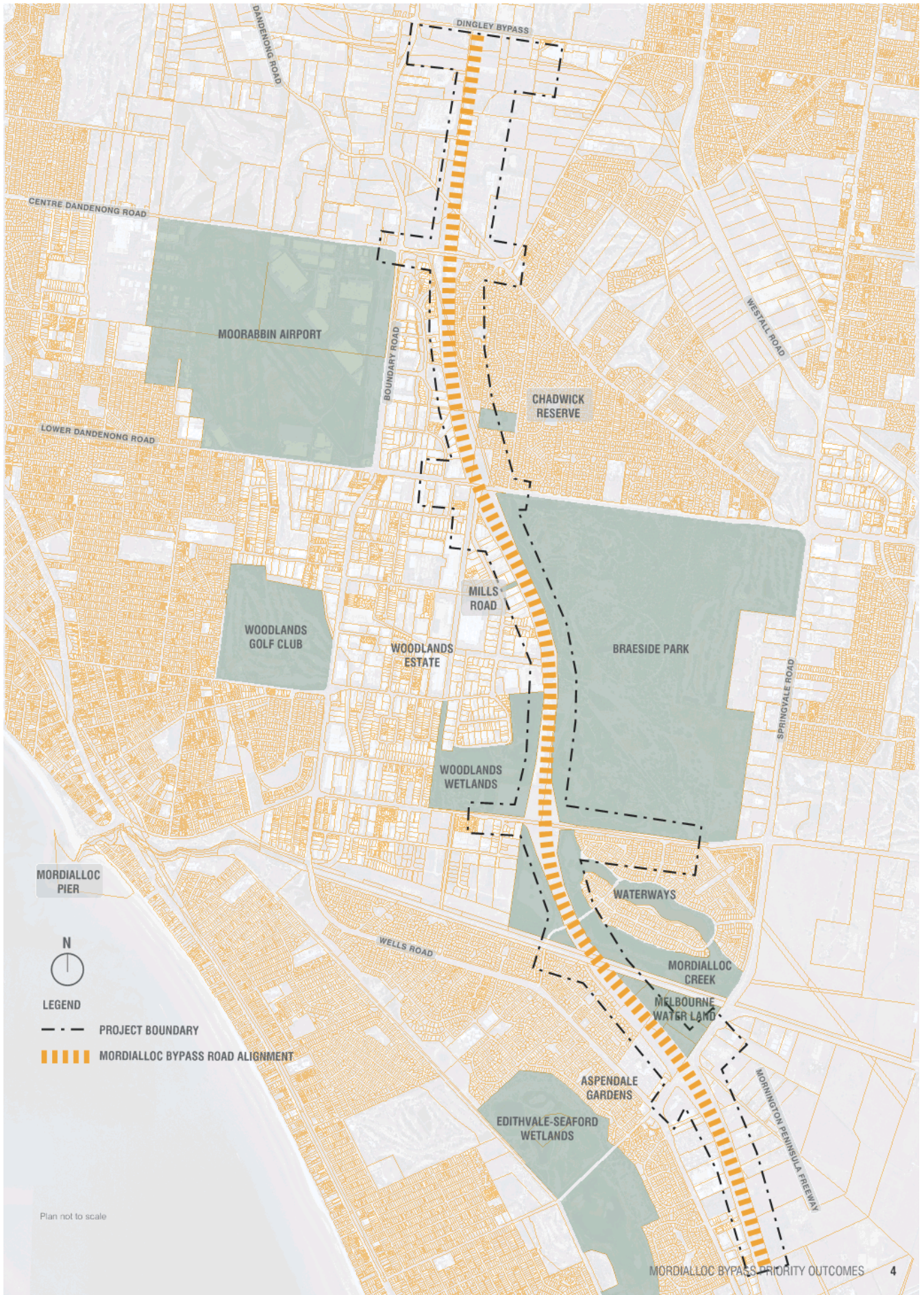
SENSITIVE RESIDENTIAL INTERFACES

Three established residential communities share a direct interface with the freeway corridor; Dingley Village, Aspendale Gardens and the Waterways estate. It is of particular importance to Council that these interfaces are well managed in terms of visual and noise amenity. The Waterways Estate is specifically subject to a Design and Development Overlay in the local planning scheme that aims to conserve and enhance a landscape character supportive of indigenous flora and fauna habitats.

EMPLOYMENT PRECINCTS

The project area also interfaces with regionally significant employment precincts and transport connections, such as Woodlands Industrial Estate and Moorabbin Airport.

In line with the Kingston Municipal Strategic Statement; Council is committed to supporting the continued growth and diversification of employment precincts to maintain their viability. The Woodlands Industrial Estate is demonstrating a positive transition towards a more diversified precinct with a range of business and land use activities, such as; a hotel, restaurant, financial services and a community garden. Maintaining and increasing connectivity to this precinct is a high priority for Council.



04.

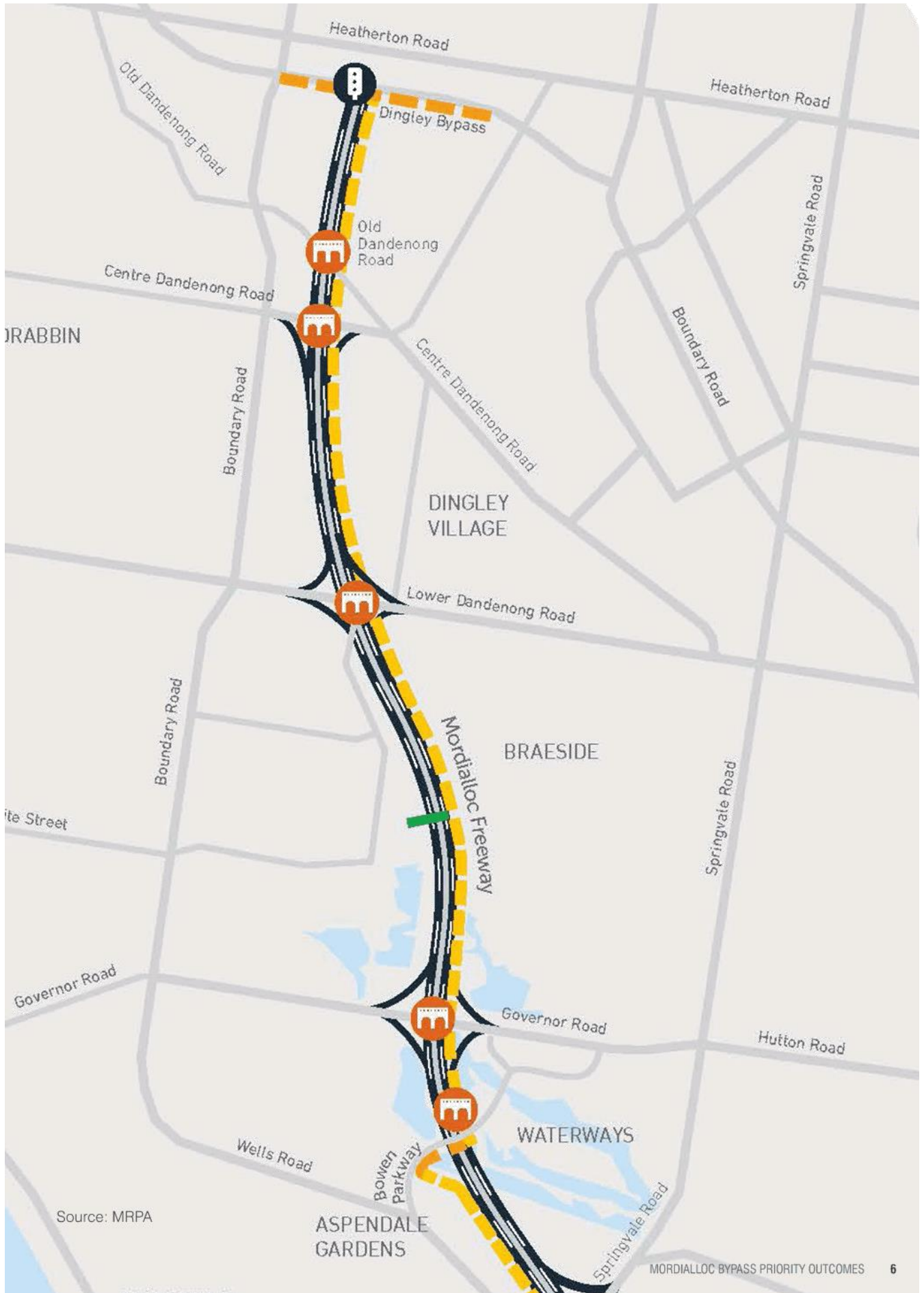
PROJECT OVERVIEW

In the 1970s a road reserve was put aside, as part of a broader, integrated freeways plan, for a six lane connection to accommodate the future urban growth of the south-eastern region. VicRoads are proposing to develop the Mordialloc Bypass within what remains of this corridor; a 9.7 kilometre freeway link that will connect the Dingley Bypass to the Mornington Peninsula Freeway at Springvale. The freeway aims to reduce congestion on arterial roads by creating a continuous connection to Frankston. The Bypass will traverse constructed wetlands, move between established residential communities, industrial areas, conservation areas, open space and agricultural land within the South East Green Wedge.

The current project proposal includes:

- ▶ a dual carriageway with two to three lanes in each direction
- ▶ bridges over Springvale, Governor, Lower Dandenong and Centre Dandenong Roads, along with new freeway entry and exit ramps
- ▶ bridges over Old Dandenong Road and through the sensitive Waterways area
- ▶ traffic lights at the Dingley Bypass intersection
- ▶ an upgrade of the existing interchange at Thames Promenade, Chelsea, with the Mornington Peninsula Freeway, along with freeway entry and exit ramps
- ▶ a new shared walking and cycling path along the length of the freeway on the eastern side
- ▶ fauna exclusion fences adjacent to Braeside Park and wetlands and habitat crossing points for fauna under the freeway
- ▶ a pedestrian underpass between Braeside Park and Woodlands industrial precinct
- ▶ extensive wetlands and water sensitive urban design treatments
- ▶ tree planting and landscaping along the length of the corridor
- ▶ noise walls to residential areas.

For more information visit <http://roadprojects.vic.gov.au/>



05.

PROJECT STATUS

A range of investigations were undertaken in 2017 and a reference design was developed by VicRoads in May 2018 that guided the project performance and contractual requirements for the recently released design and construct tender. This tender is being administered by the Major Road Projects Authority (MRPA) on behalf of VicRoads.

Kingston City Council officers have met regularly with VicRoads and the MRPA since early 2017 to provide feedback and input as the planning and design progressed. This has included officer representation on the Technical Reference Group established by the MRPA as part of the Environmental Effect Statement process.

Council has passed two Notices of Motion on 22 May 2017 and 25 September 2017 outlining issues to be resolved collaboratively with VicRoads.

As a result of this positive collaboration, a number of the points raised in the Notices of Motion have been considered and addressed by VicRoads and the MRPA in the development of the design and construct tender, including;

- ▶ Incorporation of tall indigenous tree planting along the acoustic fence adjacent to wetlands, to encourage safe crossing of birds over the freeway.
- ▶ Incorporation of acoustic screening at the interface of Aspendale Gardens, Dingley Village and Waterways residential areas.
- ▶ Retaining the Old Dandenong Road connection between Boundary and Tootal Roads.
- ▶ Establishment of wetlands along the route of the Bypass
- ▶ Accommodation of the Chain of Parks shared trail and minimal disruption to the Braeside Park historic buildings and ranger's facilities on the eastern side of the freeway.
- ▶ Public consultation with community members from Dingley Village, Waterways and Aspendale Gardens (east of Wells Road).

Outstanding issues from the Notices of Motion are captured within the requested project outcomes in this report, such as;

- ▶ Establishment of acoustic fence treatments adjacent to important open space assets and conservation areas such as Braeside Park, the Ranger's facilities and the Woodlands wetlands.

- ▶ Provision of a grade separated pedestrian and cycling connection (under or overpass) at the Dingley Bypass intersection.

The project is currently in tender design phase, which is running concurrently with an Environmental Effects Statement (EES) process. The EES investigation commenced, following finalisation of the scoping requirements of the EES by DELWP in May 2018. Findings were presented to stakeholders for comment in late October and will be finalised in early 2019 when the successful tenderer is appointed. Once finalised, the proponent will be required to address the recommendations of the EES.

The EES focuses on the potential effects on:

- ▶ biodiversity and habitat – migratory birds and the native vegetation when modifying grasslands and wetlands.
- ▶ surface water and groundwater – the quality of water flows and drainage in the Mordialloc Creek catchment and the Edithvale-Seafood Wetlands.
- ▶ noise and air quality – increased traffic and noise for residents and nearby park lands, in addition to local air quality with exposure to vehicle emissions.
- ▶ landscape and visual amenities – impacts to wetlands and local amenity from new infrastructure such as bridges and overpasses.
- ▶ traffic and transport – journey and road safety improvements.

Other studies include:

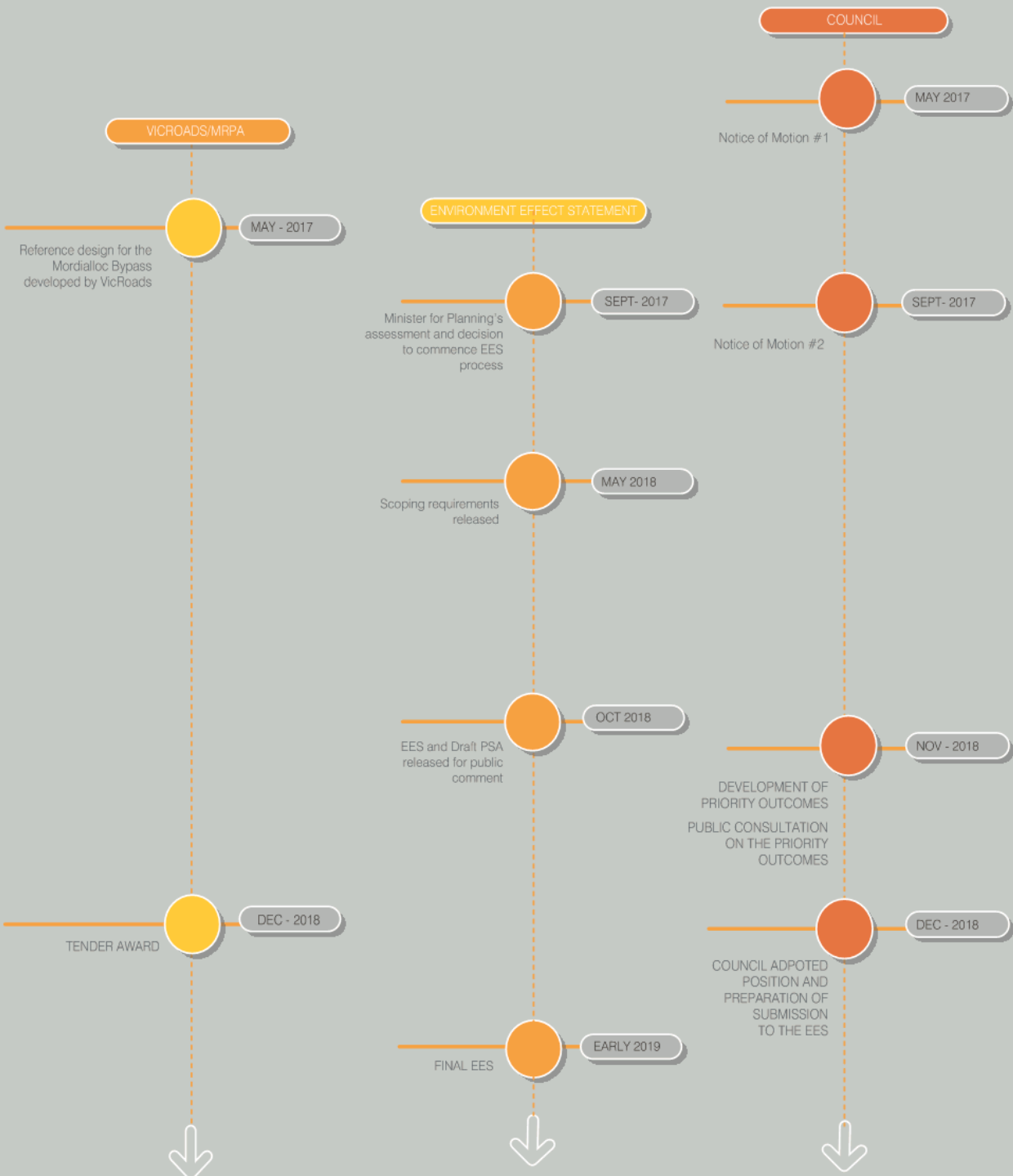
- ▶ Land contamination
- ▶ Cultural heritage
- ▶ Amenity and environmental quality
- ▶ Social, land use and infrastructure.

Council is currently reviewing the recently released Environmental Effects Statement and will undertake its own assessment of potential environmental effects associated with the project. The outcome of the review will inform Council's submissions through the EES process.

For information on the EES findings, visit <http://roadprojects.vic.gov.au/projects/mordialloc-freeway/view-the-ees>

06.

PROJECT TIMELINE



07.

COUNCILS PRIORITY OUTCOMES



IDENTITY AND ENVIRONMENT

REPRESENTING A REGION INVESTED IN AN ENVIRONMENTALLY PROGRESSIVE FUTURE THROUGH DESIGN.

Market Gardens - City of Kingston



Infrastructure and landscape design can tell a clear story about the character and identity of an area. The current proposal states the objective of 'Local Identity'; building an understanding of place through highlighting intrinsic characteristics. Council supports this objective but believes the proposed built form design, materiality, wayfinding elements and vegetation treatments do not communicate a unified story about the identity of the region that will be recognisable to motorists or pedestrians.

The current design potentially communicates a mixed message. Planting arrangements are both formal/linear (referencing agricultural uses and varying adjacent urban form) and informal/scattered (referencing indigenous vegetation communities). Noisewall forms are influenced by the nearby Dingley Bypass for continuity but are not representative of a clear theme.

Whilst the area that the freeway traverses is indeed diverse, Council believes there is a strong overarching identity that should be communicated and that strategic environmental objectives and values are strongly tied to this. This identity or 'theme' can be defined as:

“A REGION INVESTED IN AN ENVIRONMENTALLY PROGRESSIVE FUTURE”.

Council sees that this vision represents the values and aspirations of the region embodied in key strategies such as the *Kingston Green Wedge Plan 2012* and the Local Planning Scheme. In particular;

1. Protection of areas of conservation significance (eg Braeside Park and Ramsar wetlands),
2. Encouraging ecologically sensitive development,
3. Investment in renewable energy,
4. Reatain and enhance land use to encourage more sustainable agricultural practices and food security,
5. Creation of habitat and open spaces, and
6. Investment in diverse economies and healthier, better connected communities.

In terms of character and identity; Council's key strategies identify a preference towards low visual impact development, the use of indigenous vegetation in landscaping to represent pre-settlement landscapes (wetlands and open woodlands), and a muted colour palette.

IDENTITY AND ENVIRONMENT

OUTCOME 1

THE DEVELOPMENT TELLS A CLEAR STORY ABOUT PLACE – A REGION INVESTED IN AN ENVIRONMENTALLY PROGRESSIVE FUTURE

The Dingley Bypass/ Mordialloc Bypass intersection is an identified 'Green Wedge Gateway site' in the Green Wedge Plan that must incorporate 'well designed, high quality architecture that is underpinned by the principles of sustainability'. The current design for this entry and along the length of the bypass, requires further work to ensure that it communicates a strong and legible identity that supports Council's vision for the region.

THE MOTORIST'S JOURNEY

Under the current design proposal, motorists will travel along open and semi-rural areas in the northern section, past scattered Eucalypts and formal, banded shrub plantings, into closed sections of roadway, bounded on both sides by an abrupt metal noisewall interface. Along the journey there will be glimpses of the surrounding area at bridges through semi-transparent plexi-glass panels and open sections with clear views to the Woodlands Industrial Estate, Braeside Park and open tracts of undeveloped land. The wetlands, which characterise this area will not be visible on the drive, masked by opaque noisewalls.

The proposed noisewall materiality includes corten (weathered steel) sheeting panels in most sections; varying in height from 0.5 to 6 metres. Some sections are proposed to use plexi-glass panels with a gradation of semi-transparency at the base to fully transparent panels at the top. Transparent sections are mostly located on bridges and adjacent to Chadwick Park.

HOW THE DESIGN CAN STRENGTHEN COUNCIL'S ENDORSED IDENTITY FOR THE REGION

In addition to the current scheme, the development needs to demonstrate that:

1. The environmental assets of the region are valued, specifically Braeside Park and adjacent wetland systems.
2. Creation and protection of habitat and ecological diversity are core drivers of the design response
3. The region is supportive of innovative and progressive strategies that build resilience.

1. VALUING ENVIRONMENTAL ASSETS OF THE REGION

Council is pleased to see that the current proposal aims to reduce impacts on the Waterways wetlands by minimising impacts from the bridge infrastructure that will cross it and that soundwalls will be incorporated. Tall trees are proposed adjacent to the noise walls to enable water birds with long take off distances to perceive the freeway and make their way over it safely. In addition to these treatments, it is of utmost importance that noise impacts on Braeside Park are minimised, whilst retaining views to this distinctive conservation asset. Likewise, views to all adjacent wetlands should be a feature of the journey.

2. PROTECTION AND ENHANCEMENT OF BIODIVERSITY

The *City of Kingston Biodiversity Strategy 2018-2023* highlights the importance of open space in retaining and enhancing biodiversity values. Integral to this is connectivity between conservation assets. Water corridors and physical connections with high quality habitat are imperative in facilitating movement of indigenous fauna species.

Freeway developments cause a major barrier to achieving this connectivity both physically and from light and noise impacts. The current proposal attempts to overcome this by incorporating possum crossings over the freeway, noise walls and tree planting buffers, habitat crossings via culverts under the freeway and WSUD treatments that maintain hydrological function and improve water quality. Council is concerned that some of these treatments (habitat crossings) are not well proven and noise impacts to Braeside Park are not being mitigated.

To further support Council's strategic biodiversity objectives the development should use exclusively indigenous plant species and native species that achieve creation of quality local habitat. The corridor itself is also an opportunity to create connectivity.



Integrated solar panels on noise walls
Calder Freeway - Melbourne

IDENTITY AND ENVIRONMENT

OUTCOME 1 CONTINUED

THE DEVELOPMENT TELLS A CLEAR STORY ABOUT PLACE – A REGION INVESTED IN AN ENVIRONMENTALLY PROGRESSIVE FUTURE

3. DEMONSTRATING SUPPORT FOR A PROGRESSIVE FUTURE

Further treatments can be considered that strengthen the theme, such as incorporation of integrated stormwater treatments (Chadwick Reserve), design of the built form (incorporating recycled materials, recognisable motifs or patterns and solar panels), landscape design (selecting iconic locally indigenous species representative of pre-settlement landscapes) and integration with the currently proposed way finding elements and incorporation of artwork.

The weathered steel and plexi-glass panel treatment is largely sympathetic to Council's preferred design outcome, however it is imperative that the design of each of these materials work together to demonstrate a strong thematic outcome.

A unique opportunity exists to lay a 'purple' pipeline for recycled water in conjunction with the upcoming Mordialloc Freeway works to provide a strategic and cost-effective way to provide recycled water infrastructure to Kingston's Green Wedge which is home to market gardens, golf courses and parkland (with possible future open space including Hawthorn Football Club).



THE MORDIALLOC BYPASS DESIGN CAN STRENGTHEN THE IDENTITY OF THE REGION BY ADDRESSING THE FOLLOWING DESIGN CRITERIA:

1. Maintain clear views to the assets of the region to highlight places that are representative of an integrated urban ecology, specifically: Braeside Park (transparent noise wall interface), wetlands (transparent noise wall interface).
2. Create a continuous habitat corridor of trees along the length of the freeway using only locally indigenous species.
3. Showcase identifiable indigenous species at key nodes and interchanges.
4. Embed environmental targets within the development that exceed the bare minimum, such as noise walls that integrate solar panels and extensive use of recycled materials.
5. Install a 'purple pipeline' along the length of the freeway as part of Council's water security objectives for green space.
6. Provide an architectural and urban design response that references this theme eg: celebration of recycled materials within the materiality, especially at key nodes and interfaces, branding way finding elements and the built form with recognisable forms, motifs, colours and patterns that represent the theme.
7. Incorporate interpretive art and signage that informs and celebrates local flora and fauna.
8. Retain as many indigenous trees as possible using protection measures during construction and consider alternative path alignments to minimise impacts.
9. Ensure the preformed corrugated sheet 'zig zag' pattern is integrated with the theming and other urban design outcomes such as bridge design and paving treatments.
10. Expand and enhance Chadwick Reserve with the creation of wetlands in the western portion of the site to both treat and harvest stormwater for re-use.

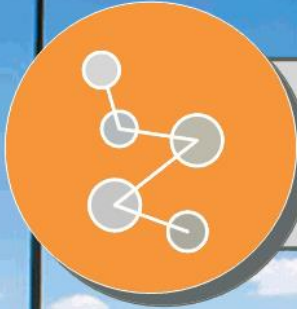
Wetlands estate - City of Kingston

MORDIALLOC BYPASS PRIORITY OUTCOMES

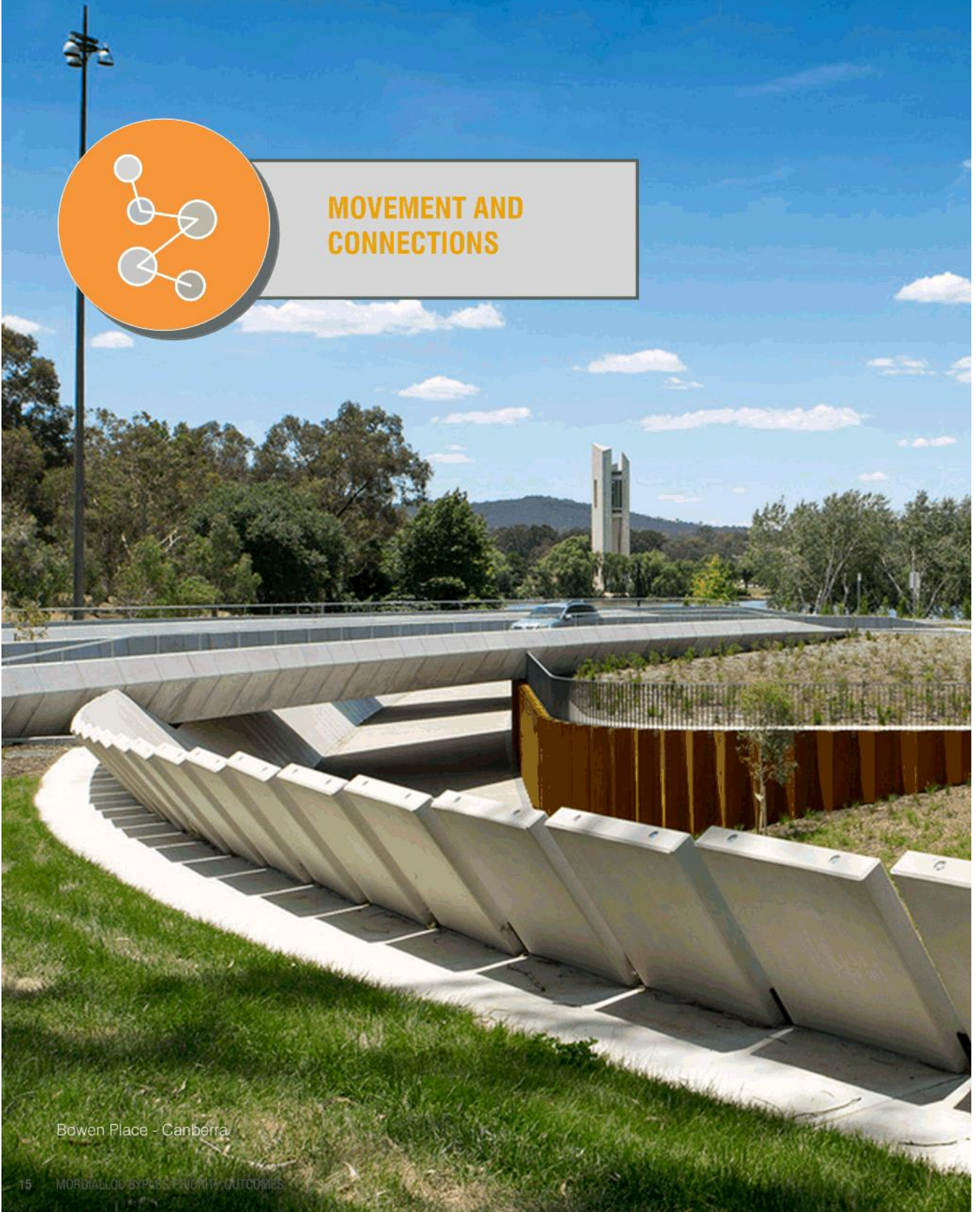
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08.

COUNCIL'S PRIORITY OUTCOMES



MOVEMENT AND CONNECTIONS



Bowen Place - Canberra



Connectivity throughout the region is a core objective of the Green Wedge Plan and is supported by current State and Federal policies relating to public health. Council's vision for the creation of a 'Chain of Parks' is underpinned by a comprehensive cycling, walking and equestrian trail network.

Council is pleased to see the incorporation of a continuous shared use path (SUP) along the length of the freeway and provision for linkages to the existing footpath network. A number of treatments are also proposed in the reference design to create an attractive environment for users and mitigate the impacts of noise and freeway infrastructure, however Council believes further enhancements could be made to ensure that the connections are well used and that the key connections identified in the Green Wedge Plan are achieved.

MOVEMENT AND CONNECTIONS

OUTCOME 2

LINKS AND JOURNEYS ARE WELL USED: INTEGRATED, DIRECT, ACCESSIBLE, LEGIBLE, ATTRACTIVE AND SAFE

The road environment can be a significant barrier to walking, cycling and other users, for example; equestrian. Inhospitable conditions and poor amenity created by radiant heat from vast road surfaces and noise from speeding traffic should be mitigated.

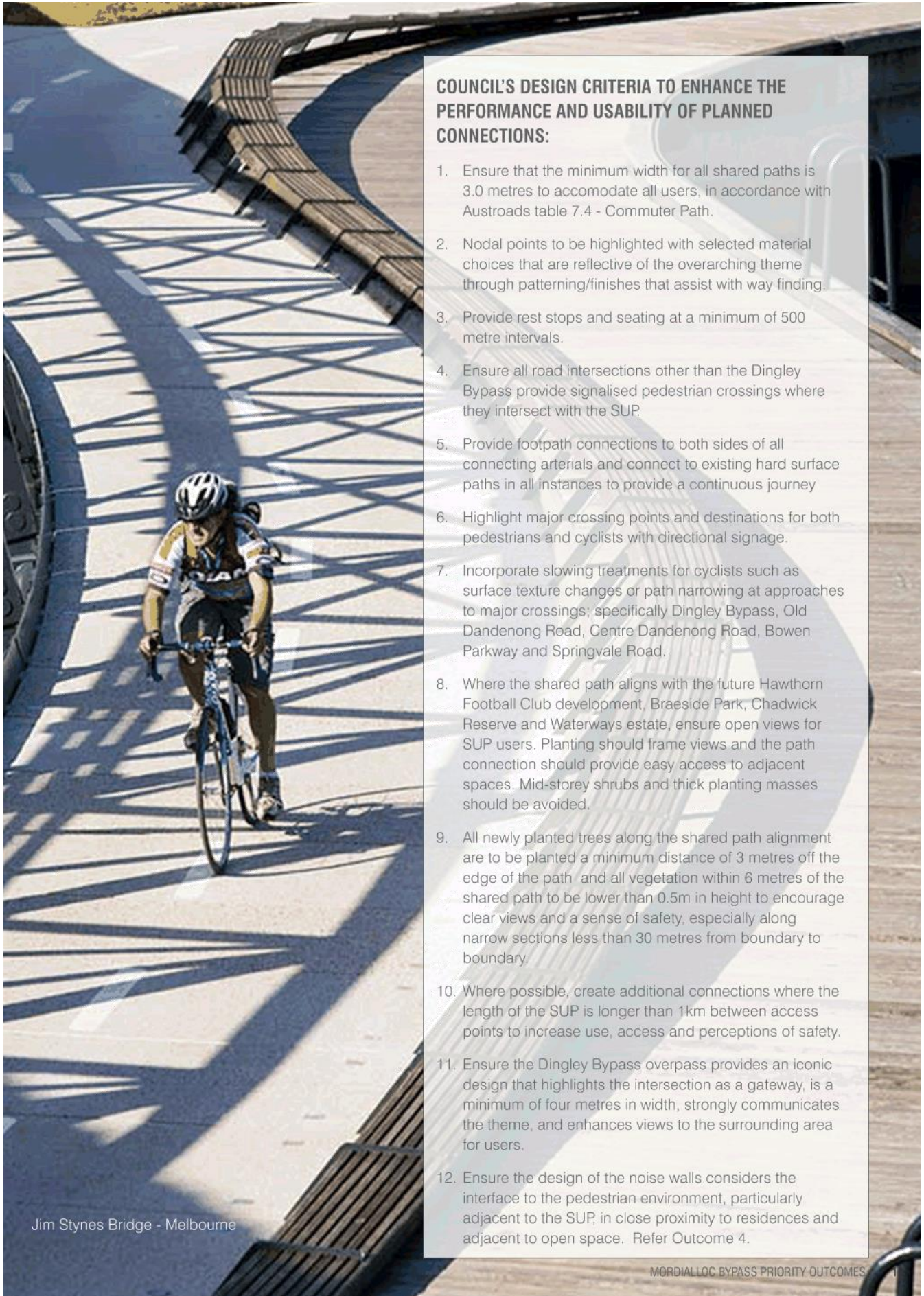
Other factors that can pose a barrier to users include;

- ▶ Indirect routes and inadequate path widths
- ▶ Surface materials that are difficult to traverse
- ▶ Heat exposure/ lack of shade
- ▶ Lack of rest points
- ▶ Unsafe spaces; such as narrow, linear spaces with limited visibility, natural surveillance or escape points
- ▶ Lack of way finding treatments

The proposed SUP located on the eastern side of freeway is a critical strategic walking and cycling link for the broader region. It is more than a local connection as it forms a key route within the broader network, linking to employment and education centres. It is imperative that every effort is made to improve its usability. Likewise all connections and pathways implemented as part of the project should use architectural interventions, landscape treatments and wayfinding elements that encourage use.

The current arrangement of noise walls creates an excessive sense of enclosure for both the motorist and pedestrian environments in some sections of the corridor. In particular, trail users will be directed through a 500 metre long, narrow section between 3-6 metre high noise walls and residential back fences adjacent to Dingley Village, varying in width from approximately 30 metres down to 7 metres. This arrangement will require treatments that ensure public surveillance is maximised and a sense of safety and comfort are achieved for users.

The currently proposed shared use path in the southern section, diverts users onto Bowen Parkway and around the western edge of the wetlands before reconnecting with the planned SUP to Springvale Road. As such, it does not provide an attractive and direct connection in this location.



Jim Stynes Bridge - Melbourne

COUNCIL'S DESIGN CRITERIA TO ENHANCE THE PERFORMANCE AND USABILITY OF PLANNED CONNECTIONS:

1. Ensure that the minimum width for all shared paths is 3.0 metres to accommodate all users, in accordance with Austroads table 7.4 - Commuter Path.
2. Nodal points to be highlighted with selected material choices that are reflective of the overarching theme through patterning/finishes that assist with way finding.
3. Provide rest stops and seating at a minimum of 500 metre intervals.
4. Ensure all road intersections other than the Dingley Bypass provide signalised pedestrian crossings where they intersect with the SUP.
5. Provide footpath connections to both sides of all connecting arterials and connect to existing hard surface paths in all instances to provide a continuous journey
6. Highlight major crossing points and destinations for both pedestrians and cyclists with directional signage.
7. Incorporate slowing treatments for cyclists such as surface texture changes or path narrowing at approaches to major crossings; specifically Dingley Bypass, Old Dandenong Road, Centre Dandenong Road, Bowen Parkway and Springvale Road.
8. Where the shared path aligns with the future Hawthorn Football Club development, Braeside Park, Chadwick Reserve and Waterways estate, ensure open views for SUP users. Planting should frame views and the path connection should provide easy access to adjacent spaces. Mid-storey shrubs and thick planting masses should be avoided.
9. All newly planted trees along the shared path alignment are to be planted a minimum distance of 3 metres off the edge of the path and all vegetation within 6 metres of the shared path to be lower than 0.5m in height to encourage clear views and a sense of safety, especially along narrow sections less than 30 metres from boundary to boundary.
10. Where possible, create additional connections where the length of the SUP is longer than 1km between access points to increase use, access and perceptions of safety.
11. Ensure the Dingley Bypass overpass provides an iconic design that highlights the intersection as a gateway, is a minimum of four metres in width, strongly communicates the theme, and enhances views to the surrounding area for users.
12. Ensure the design of the noise walls considers the interface to the pedestrian environment, particularly adjacent to the SUP, in close proximity to residences and adjacent to open space. Refer Outcome 4.

MOVEMENT AND CONNECTIONS

OUTCOME 2 CONTINUED

LINKS AND JOURNEYS ARE WELL USED: INTEGRATED, DIRECT, ACCESSIBLE, LEGIBLE, ATTRACTIVE AND SAFE

COUNCIL REQUESTS THAT THE FOLLOWING IMPROVEMENTS ARE MADE TO PROPOSED CONNECTIONS AS PART OF THE MORDIALLOC BYPASS PROJECT;

1. A PEDESTRIAN OVERPASS AT DINGLEY BYPASS

- ▶ Provide an overpass at the Dingley Bypass intersection instead of a signalised intersection to create an attractive, safe and direct connection to the existing shared use trail north of the Bypass and future 'Chain of Parks' network.

2. WIDEN THE PROPOSED UNDERPASS

- ▶ Ensure the underpass connection is a minimum of 6 metres wide. Currently the performance requirements state the minimum width requirement is 3.4 metres with a preferred nominal width of 6 metres. Anything less than 6 metres wide will create an unwelcoming space. Further design treatments should be incorporated into any underpass design. Refer design criteria on following page.

3. CREATE A MORE DIRECT OFF ROAD CONNECTION AT BOWEN PARKWAY/ WATERWAYS ESTATE

- ▶ Achieve the most direct route possible for cyclists from Bowen Parkway to the south side of the Mordialloc Creek. Consider a suspended path system over the sensitive Waterways wetland.

4. CONNECTIONS AT ASPENDALE GARDENS

- ▶ Ensure that the proposed SUP adjacent to Aspendale Gardens provides connections that link with the current reserve and footpath network, for example at Ferntree Grove and Bungalow Way.

PRECEDENTS



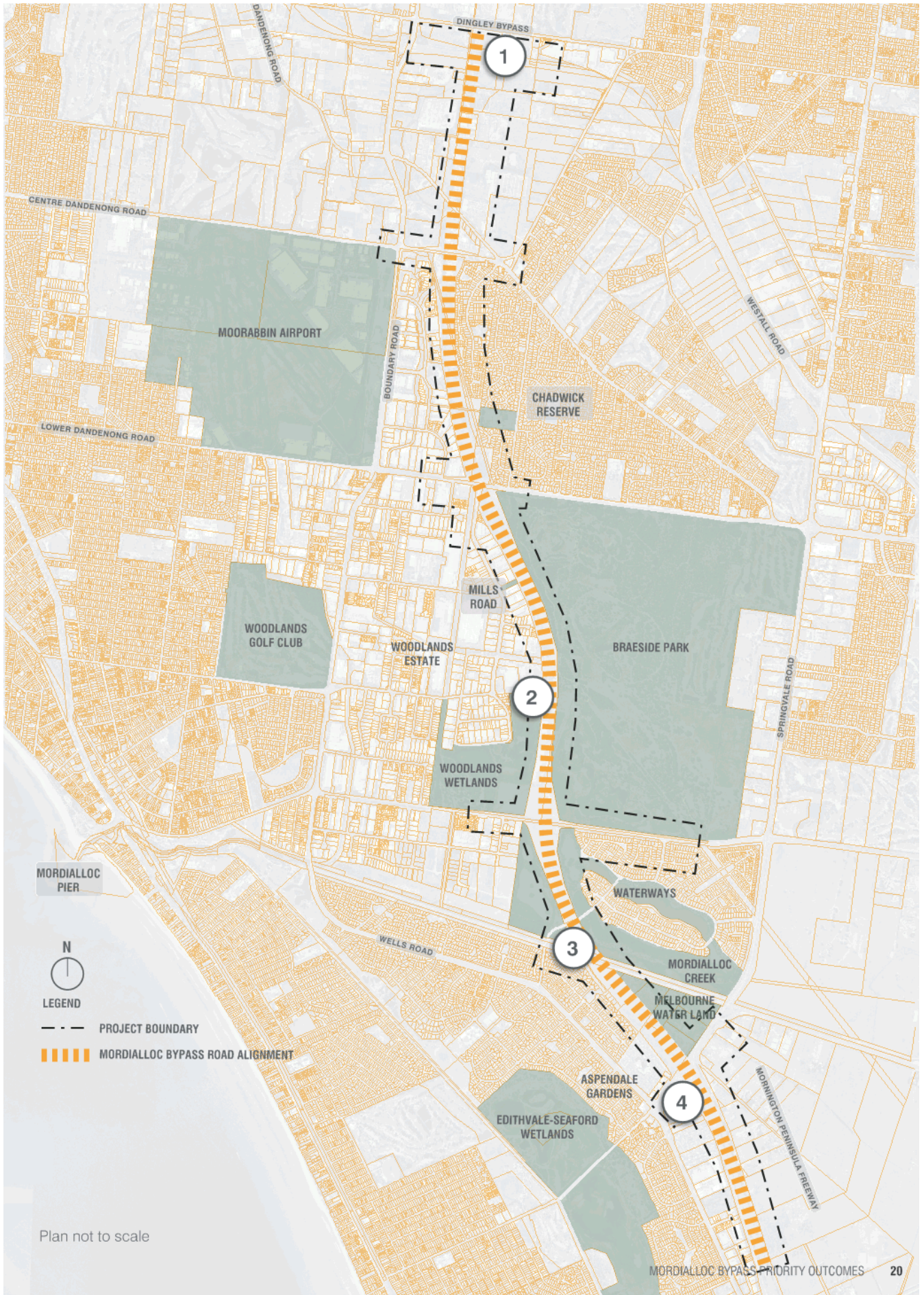
Craigieburn Bypass- Pedestrian overpass



Bowen place - Canberra - Widened underpass



Western Distributor - Melbourne - Proposed suspended cycle way



MOVEMENT AND CONNECTIONS

OUTCOME 2 CONTINUED

LINKS AND JOURNEYS ARE WELL USED: INTEGRATED, DIRECT, ACCESSIBLE, LEGIBLE, ATTRACTIVE AND SAFE

UNDERPASS DESIGN

Although the current design performance requirements request 'adequate lighting', splayed wing walls and a 'sense of openness' at entries to the underpass, performance requirements need to be strengthened to ensure the best possible outcome.

Considering that the connection is 25 to 30 metres in length, Council would also like to see higher standards for lighting, including penetration of daylight, and high quality wall treatments to ensure a safe, attractive and well-used crossing environment is provided.

The design should seek to reduce the extent of wing walls wherever possible and incorporate treatments that improve their appearance and deter graffiti.



Proposed pedestrian underpass at Braeside Park - Aerial view (source: <https://roadprojects.vic.gov.au/projects/mordialloc-freeway/view-the-ees>). Narrow entries create a dark, inhospitable and potentially unsafe environment.



Proposed pedestrian underpass at Braeside Park - ground level (source: <https://roadprojects.vic.gov.au/projects/mordialloc-freeway/view-the-ees>). Sheer wing-walls create a harsh entry appearance and palette for graffiti.

COUNCIL'S UNDERPASS DESIGN CRITERIA;

In addition to current design guidance;

1. Ensure that dimensions of no less than 6 metres wide are adopted to ensure generous visibility from one end of the tunnel to the other.
2. Wherever possible, allow natural light to penetrate the connection between the carriageways.
3. Reduce the extent and apply a cladding, artwork or an integral pattern to underpass head and wing walls that is reflective of the theme and assists in reducing graffiti.
4. In addition to the application of CPTED (Crime Prevention Through Environmental Design) principles ensure a Safety In Design analysis is undertaken.
5. Wing walls to be splayed at a maximum angle to ensure the tunnel is not elongated.
6. Materials used should be consistent with the overarching theme, such as natural tonal colours that reflect surrounding open space (also refer Outcome 4).
7. Incorporate planting treatments at the underpass entries that soften and integrate wing walls.
8. Create generous and welcoming spaces at the entries to the underpass with multi-directional surveillance and clear lines of sight maximised within 30 metres of the entry.

PRECEDENTS



Bowen Place - Canberra. Natural light to central underpass



Drysdale Bypass splayed wing walls with integration of planting and cladding to create an attractive and welcoming environment.



Jim Stynes Bridge. Clear view lines through underpass.

MOVEMENT AND CONNECTIONS

OUTCOME 3

IMPORTANT COMMUNITY CONNECTIONS ARE MAINTAINED AND STRATEGIC FUTURE CONNECTIONS ARE ALLOWED FOR

The current design proposes pedestrian crossing points at road intersections and at one other location along the length of the 9.7km bypass. This is considered inadequate to maintain current connections that are well used by the community or have potential to be. The Green Wedge Plan calls for "frequent habitat links and pedestrian/cycle crossing points (along the Bypass) that meet safety standards and guidelines."

EXISTING CONNECTIONS

The *Kingston Health and Wellbeing Plan (2013-2017)* outlines the value in maintaining and improving existing connections to local employment precincts, open space and homes as a way of encouraging active transport and associated health benefits.

Whilst pedestrian counts have been undertaken to inform the inclusion of a priority underpass connection at Braeside Park to Woodlands Industrial Estate, investigations have not considered the opportunities to improve connectivity.

Although informal, a critical link exists at Chadwick Reserve through to Garden Boulevard, connecting the residents of Dingley Village with a range of services, employment destinations and a community garden within the Woodlands Estate.

STRATEGIC FUTURE CONNECTIONS

The Kingston Green Wedge Plan identifies the future pedestrian and cycling network that supports the 'Chain of Parks' vision. This future network includes an important SUP link along the western edge of the freeway. This path will enable residential communities and workers east of the freeway to connect to community destinations such as Karkarook Park, Patterson Lakes Recreation Reserve and link to planned walking trails. The current plan has precluded the development of this link in future in some locations; such as immediately north of Centre Dandenong Road.

Another connection identified is a walking path along the Mordialloc Creek in the north. The current design includes provision for movement of water under the freeway and habitat connections at this location, however pedestrians will not be able to connect to the western side of the freeway to the future walking path.

A future path connection is identified through the triangular Melbourne Water site at the southern end of the project area to the East Link Trail. Adequate space must be allowed for beneath bridge infrastructure to ensure this connection can be established in future.

CONSIDERING THE STRATEGIC VISION FOR THE REGIONAL CYCLING AND PEDESTRIAN NETWORK, COUNCIL REQUESTS THAT THE FOLLOWING OUTCOMES ARE ACHIEVED AS PART OF THE MORDIALLOC BYPASS PROJECT;

1. UNDERBRIDGE CONNECTIONS

- ▶ Allow sufficient space under the Waterways overpass, south of Mordialloc Creek, for a future 3 metre wide SUP connection to Pillars Road and the East Link Trail via the Melbourne Water site.

2. SECOND SIGNALISED PEDESTRIAN CROSSING

- ▶ Make provision for a future, second signalised pedestrian crossing east-west at the Dingley Bypass intersection to facilitate a connection from the eastern SUP to the future western SUP and Mordialloc Creek walking trail identified in the Green Wedge Plan. This connection will also enable links to future community hubs and open space assets at the month of the project area for workers and residents on the western side of the freeway.

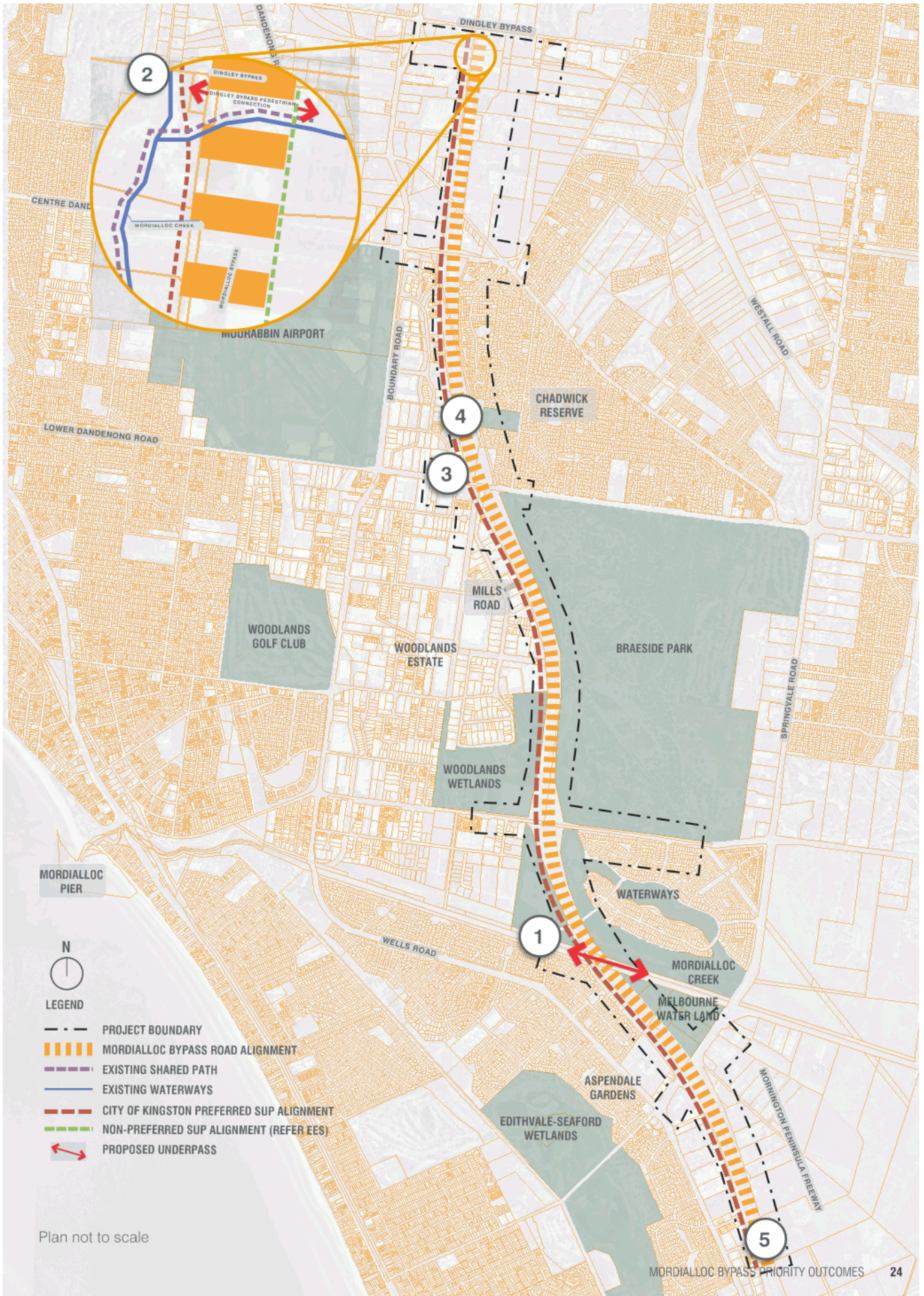
3. WESTERN SUP RESERVE

- ▶ Allow sufficient space for a future 3 metre wide shared path connection along the length of the western side of the freeway. Ensure currently proposed cross-freeway connections are designed to provide a connection to the future SUP.

4. CHADWICK RESERVE TO GARDEN BOULEVARD IN THE WOODLANDS ESTATE.

- ▶ Formalise the existing connection between the Dingley Village residential area and existing community services, workplaces and destinations on the other side of the corridor (Redwood Gardens) with an underpass. Integrate the path connection at this location with the wetland treatment proposed at Outcome 1.

5. CONNECT THE SUP TO THE PENINSULA LINK TRAIL



09.

COUNCIL'S PRIORITY OUTCOMES



AMENITY

Peninsula Link - Pedestrian Overpass



The Mordialloc Bypass is a substantial piece of infrastructure that bisects and runs within close proximity to regionally significant conservation areas, open space, residential areas and employment precincts. Without careful mitigation strategies, the development will have significant noise and negative visual amenity impacts on the surrounding area.

The Mordialloc Bypass project is required, as a minimum, to deliver visual and noise mitigation treatments in accordance with the recommendations of acoustic and visual impact assessments. This response is typically achieved with tree planting, noise walls and mounding. The EES process has included an assessment of the degree to which these technical parameters have been met and Council is currently undertaking a review of these findings.

Council is committed to ensuring that the development exceeds bare minimum standards. The following outcomes are based on a high level review of potential noise and visual impacts, including design quality of the built form and landscape response, in the reference design material supplied to Council by the MRPA and VicRoads.

AMENITY

OUTCOME 4

EFFECTIVE SHORT AND LONG TERM VISUAL SCREENING - NOISEWALLS AND VEGETATION

Sensitive views to the roadway from SUP's and residential interfaces require adequate screening in the short term (from high quality structures) and long term from vegetation. Likewise unattractive interfaces to the road environment such as industrial buildings require screening treatments.

Council is pleased to see that extensive tree planting is proposed at industrial interfaces, at intersections and in available areas adjacent to the road corridor. However there are a number of locations where there is no space available for tree planting along the roadway. For long sections (sometimes over a kilometre) the motorist will have an immediate interface with noisewalls varying in height from 0.5 to 6 metres.

Effective short-term screening relies largely on the careful placement of noise walls and the quality of their design. The design outcome for the noisewalls must address both the motorist experience and pedestrian/residential interfaces. Given the long term realisation of intended screening using vegetation, the noisewalls must stand alone as a quality interface before the adjacent vegetation matures.

The interfaces and transition of noise walls should be well considered. The current design shows a 'stop-start' approach to the placement of noisewalls. The use of tapering at key locations will help integrate the walls and allow for a better Urban Design outcome.



Currently proposed SUP/noisewall interface (source: <https://roadprojects.vic.gov.au/projects/mordialloc-freeway/view-the-ees>). A mix of shrubs and trees should be provided to soften the noisewall edge (Refer Peninsula link - Image 1).



The proposed interface between noisewalls above shows an abrupt transition. (source: <https://roadprojects.vic.gov.au/projects/mordialloc-freeway/view-the-ees>). Transitions should taper and integrate with adjacent forms (refer image 2 Deer Park Bypass) or continue at a consistent height rather than creating a sudden drop as pictured above.

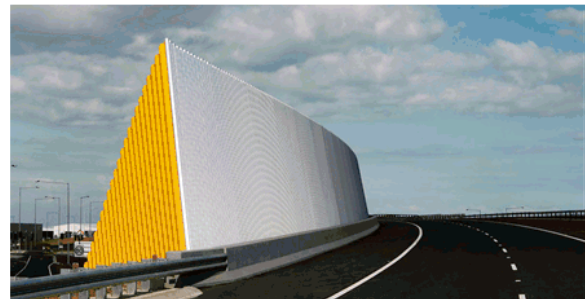
COUNCIL'S DESIGN CRITERIA FOR THE NOISEWALL AND SCREENING VEGETATION TREATMENTS;

1. Ensure that the Governor Road overpass incorporates a high quality noisewall finish that assists in reducing visual bulk and enhances the presentation of the bridge when viewed from the Waterways Estate.
2. Where panels are required to change in height ensure transition or tapering is carefully considered.
3. Where possible mounding should be used to minimise the heights of the noise walls with the addition of planting to help soften the wall's verticality.
4. Noisewall materials must compliment the natural setting, be recessive and use a muted colour palette. Bright iridescent colours should not be used.
5. Perforated patterns within the plexi panels sections should reflect the broader theme - Refer outcome 1.
6. Support structures shall be integrated and or contained within the panels so that they are hidden on both sides of the wall.
7. Offset of noisewall from the shared path to be a minimum distance of 6 metres to allow for screening vegetation without interrupting view lines.
8. All trees planted adjacent to noisewalls or within the road reserve at wetlands, residential and industrial interfaces to be installed as advanced stock (minimum 15L).

PRECEDENT



1. Peninsula Link - Well selected shrubs and small trees soften the edge of the noisewalls and compliment the architecture.



2. Deer Park Bypass - The ends of the noisewalls taper to provide a more considered architectural outcome.



3. East Link - Walls should be a consistent height wherever possible, as pictured above.

AMENITY

OUTCOME 5

HIGH QUALITY, VISUALLY RECESSIVE BRIDGE STRUCTURES

Clause 22.04-3 of the Kingston planning scheme sets out Council policy that all planning outcomes in the Green Wedge must result in an urban form which is of a high design standard and low visual impact.

The overpass structures along the corridor vary from 'Cut Throughs' without Blade Pier (Old Dandenong Road and Centre Dandenong Road), 'Cut Throughs' with Blade Pier and Barrier (Lower Dandenong Road) and 'Spill Through' with Blade Support and Barriers (Governor Road and Springvale Road). The design intent for these structures is yet to be resolved through the detailed design process, however Council recognises their importance as thresholds for motorists and pedestrians crossing the Bypass corridor. Achieving a quality outcome at an appropriate scale that communicates a clear identity and design consistency is imperative.

The most significant structure proposed is the bridge over the Waterways wetlands. The current design is driven by reducing impacts to sensitive ecological values and flow-paths (minimising piers) and achieving the required height over the Bowen Parkway and flood levels of the wetlands. This height requirement will result in a visually prominent structure, particularly when viewed from the Waterways Estate. Similarly it will allow for expansive views across the region.

Adjacent batters to all bridges should be planted to help soften the bridge abutments and integrate them with the surrounding landform.

Where cut through's occur, wall interfaces should be integrated with parapets and gantries to avoid harsh structural outcomes. Patterning and form that relates to the theme of the road corridor should be incorporated to increase amenity and consistency from a range of vantage points.



Indicative bridge design shows bulky heastock and piers (source: <https://roadprojects.vic.gov.au/projects/mordialloc-freeway/view-the-ees>). Piers and heastock should be minimal in scale and slender of form to ensure low visual impact. All batters should be planted rather than grassed to help soften the bridge structure and for ease of maintenance.



View to proposed bridge abutment (source: <https://roadprojects.vic.gov.au/projects/mordialloc-freeway/view-the-ees>) Abutment walls should be designed with the interface of the bridge parapets and balustrades in mind. Pattern and colour should also be considered to strengthen the design theme, minimise graffiti, assist in wayfinding and create an attractive roadside environment.

COUNCIL'S DESIGN CRITERIA FOR BRIDGE STRUCTURES;

1. Cladding or patterning should be applied to the blade supports and barriers that reflects the theme of the corridor. Refer Outcome 1.
2. Design treatments should be applied to both abutments and the undercroft of the Waterways Wetlands bridge, to minimise the visual impact at the pedestrian scale and to reflect the landscape character of the wetlands. The treatment should be based on the overarching corridor theme for consistency. Refer Outcome 1.
3. Integration between plexi-glass panels and fixing to road barrier treatments and, if applicable, weathered steel noise walls should be galvanised and hidden for a seamless application.
4. Plexi-glass panels should not be stepped and are to be visually consistent with the weathered steel noise walls.
5. The abutment for the bridge to the Bowen Parkway is to incorporate raked retaining walls or sloped rock beaching to ensure a recessive look is achieved that allows minimal impact on view lines for cyclists and pedestrians.

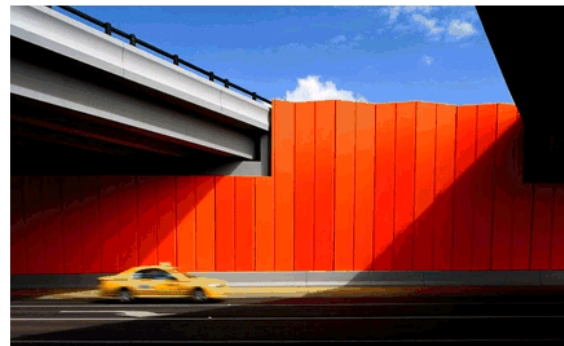
PRECEDENTS



East Link - Use of rounded edges to the headstock and narrowed blade piers softens the form of the overall structure.



East Link - Plexi-glass panel noise walls are consistent in height rather than stepped.



Deer Park Bypass - integration of the abutment wall and bridge provides a seamless and clean design outcome.

AMENITY

OUTCOME 6

THE SENSITIVE INTERFACES OF BRAESIDE PARK, DINGLEY VILLAGE, ASPENDALE GARDENS AND WATERWAYS COMMUNITIES ARE WELL MANAGED IN THE DESIGN RESPONSE

The current design for the noise walls has been developed in response to recommendations of an acoustic impacts study. Walls are located where noise impacts will exceed acceptable levels adjacent to residential areas. There is currently no legislative requirement to address noise impacts adjacent to open space or industrial areas. As such, there are no proposed noise mitigation treatments adjacent to Braeside Park, Woodlands Industrial Estate or undeveloped sections of the Green Wedge near the Dingley Bypass and Springvale Road.

The protection of recreational amenity and conservation values of Braeside Park is considered to be of particular importance. Noise impacts need to be mitigated to the same level as residential areas to protect the quiet parkland environment for recreational users and fauna, particularly birdlife.

Whilst the proposed noise walls aim to address the noise impacts to residential areas, the height in some locations comes within 6 metres of residential fences. These instances require careful design consideration to mitigate visual impacts from sensitive viewpoints and ensure optimum solar access.

The most prominent visual impact of the development will be from the Waterways Estate, where the Bypass infrastructure will be highly visible across the open wetlands.

Disturbance to sensitive areas, particularly residential interfaces, wetland habitat and the habits of birdlife and other fauna, from construction activities must be well considered, with rigorous requirements incorporated in the Construction Management Plan.



Where the SUP runs adjacent to noise walls, they should not appear to be the 'back side' as shown above. An attractive, pedestrian scale outcome should be achieved.



Close proximity of noise walls to pathways can create a negative sense of enclosure and an exaggerated perception of the scale of the walls. Transparent noise walls should be proposed where they are located within 40 metres of shared user paths.

IN ADDITION TO THE CRITERIA OUTLINED IN OUTCOME 4, COUNCIL REQUESTS THE ADOPTION OF THE FOLLOWING SENSITIVE INTERFACE DESIGN CRITERIA;

1. A transparent noisewall must be installed at the interface of Braeside Park to protect this significant conservation and recreation asset from noise impacts.
2. Noisewalls that interface with the SUP must not appear to be the 'back side' and incorporate treatments that are of pedestrian scale.
3. Noisewalls that come within 40 metres of residential areas and the SUP must be transparent.
4. Advanced tree planting must be undertaken close to Waterways residences to interrupt views to the bridge.
5. The Waterways overpass must be a high quality bridge design that is visually recessive - refer Outcome 5.
6. Construction impacts on the waterways should be addressed and management plans submitted to Council for review and approval.
7. Exclusion zones that apply to all open space and existing vegetation, must be developed with input from Council and adhered to throughout the construction phase.



Peninsula Link - Advanced tree planting is used adjacent to bridges to soften the visual impact.



East Link - Transparent noise walls to be used adjacent to open space.



Waterways Wetland - Protection of all open space especially the waterways wetland must be priority throughout the construction phase.

ALBURY | BENDIGO | CANBERRA | GEELONG | MELBOURNE | SHEPPARTON

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Ordinary Meeting of Council

10 December 2018

Agenda Item No: 8.7

RESPONSE TO NOTICE OF MOTION 3/2018 - CAPITAL WORKS PARKDALE SHOPPING CENTRE

Contact Officer: Angela Stubbs, Coordinator Business Directions

Purpose of Report

The purpose of this report is to respond to the Notice of Motion 3/2018 regarding the capital works planned for the Parkdale Shopping Centre in Como Parade West, with an outline of the proposed improvements.

Disclosure of Officer / Contractor Direct or Indirect Interest

No Council officer/s and/or Contractor/s who have provided advice in relation to this report have declared a Conflict of Interest regarding the matter under consideration.

OFFICER RECOMMENDATION

That Council:

1. Support the proposal to implement capital works as detailed in sections 3 and 4 to improve pedestrian safety, streetscape and infrastructure around the intersection of Como Parade West and Parkers Road.
2. Continue to advocate to Public Transport Victoria to increase supply of commuter parking along the railway reservation in Parkdale.

1. Executive Summary

Council has allocated \$500,000 across the 2018/2019 and 2019/2020 financial years to design and implement capital works improvements within Parkdale shopping centre.

The Business Directions team has undertaken engagement with Parkdale traders, customers and residents in the area immediately surrounding the Parkdale activity centre. The aim of this engagement was to seek feedback from traders and shoppers within Parkdale as to what they value and what they would like to see improved in the physical environment around the street trading area.

The outcome from this process has highlighted a lack of parking and a desire to improve pedestrian safety, improve the paving standard, plant more trees and generally beautify the footpath trading area.

Following a review of existing conditions throughout the shopping centre, the community's feedback aligns with the opportunity to undertake a range of streetscape and pedestrian safety improvements around the intersection of Como Parade West and Parkers Road.

2. Background

At the 26 February 2018 Council Meeting, through a Notice of Motion, Council resolved the following:

12.2 Notice of Motion No. 3/2018 - Capital Works Parkdale Shopping Centre

That the capital works planned for the Parkdale Shopping Centre in Como Parade West appearing as Item 4.7 in Council's Capital Works 5 Year Plan be brought forward from the 20/21 – 21/22 budgets to the 18/19 – 19/20 budgets.

That the final design outcomes for those works be determined following consultation with business owners and local residents.

There has not been any significant infrastructure works within the shopping centre precinct of Parkdale in the last 15 years. Prior to this time, there was an update to the pedestrian crossing at the northern end of the shopping centre (Como Parade West). In addition, more recently there has been streetscape works to the front of Shirley Bourke Theatre in 2012 and an upgrade of the drainage crossing Como Parade East (north side of Parkers Road).

Traffic works have included the installation of the pedestrian crossing installed opposite the library in Parkers Road and works by Public Transport Victoria (PTV) in the upgrade of the roundabout at the Parkers Road level crossing to make this area at the level crossing safer for pedestrians.

Infrastructure works identified as part of the Infrastructure Capital works prioritisation process include the following:

- Resurfacing of Como Parade West between Parkers Road and Alameda Street planned in the next two years.
- Alameda Street (Como Parade West to The Corso) – on the five-year Kerb and Channel Renewal Program.
- Some minor area footpath renewal required at corner of Heslop Street and Parkers Road.

Stakeholder feedback has clearly reflected the issue of limited parking in Parkdale; the Traffic department is reviewing the demand and needs within the shopping Precinct.

In addition, there was a strong preference for improved paving, safe pedestrian access, tree planting, and beautification of the footpath trading area.

Council officers have also received feedback regarding interest in street art to enhance the “village” personality of Parkdale. To be considered separate to the improvement works in consultation with Council's Arts and Culture team.

The full details of the initial trader and resident consultation process have been included in Appendix 1.

At its meeting of 27 June 2016, Council adopted the findings of the Parkdale Local Area Traffic Management Study (LATM). The study included various traffic treatments within the Parkdale shopping precinct in response to the increasing pedestrian activity around the centre and the interface with traffic along Como Parade (both East and West). Stage 1 of these measures were recently completed with various flat top speed humps, kerb extensions installed in Como Parade (both East and West) and traffic islands installed in Parkers Road, as well as the earlier pedestrian crossing opposite the library. Stage 2 of these measures will see a further four measures installed in Como Parade East and West. Stage 3 of these measures focuses on the area adjacent to the railway crossing including the roundabout at the intersection of Parkers Road with Como Parade West and the intersection of Parkers Road with Como Parade East. The proposed measures in Stage 3 included side road entry treatments/humps. These measures will be integrated into Parkdale Capital Works project. The Stage 3 LATM measures will be funded from the annual capital works Traffic Management Program.

Council passed a resolution at its Ordinary Meeting of 29 January 2018 to write to Public Transport Victoria (PTV) to reactivate the proposal to construct formal car parking spaces along Como Parade East and Como Parade West. Council received a formal response from PTV acknowledging the request, however there was no commitment to constructing additional car parking. With the level crossing removals at Mentone and Cheltenham due to commence in 2019, there will be disruptions to car parks at these two stations. It is Council's view that there is an opportunity for additional car parking to be provided at other stations along the railway line to accommodate car parking demand displaced from these car parks and to provide a net increase in car parking capacity along the line once these projects are complete. Council will therefore continue to advocate for additional car parking to be provided at Parkdale Railway Station.

3. Discussion

3.1 Council Plan Alignment

Goal 1 - Our well-planned, liveable city supported by infrastructure to meet future needs.

Direction 1.2 - Effectively influence the urban and architectural design of the City.

Direction 1.2 - Effectively influence the urban and architectural design of the City.

The Capital Works Program underpins delivery of projects and development of services across all Planned Outcomes.

3.2 Consultation/Internal Review

Internal consultation included City Strategy, Infrastructure, Public Place Projects, Traffic and Transport, and Business Directions departments.

Business Directions has completed consultation with the business and local community, and valuable feedback has informed the proposed works, with the engagement findings summary included within Appendix 1.

The engagement process was completed in June 2018 and included an online survey campaign run for three weeks. Communication to businesses and residents via postcard drop in the area with a link to a "Have Your Say" Parkdale survey. The engagement was supported by face to face communications with traders via a street walk with Council's Business Directions Officer, Angela Stubbs and by further street interactions with Cr Geoff Gledhill, Jonathan Guttmann, Ross Gregory and Tania Asper.

The survey feedback and trader consultation indicated foot traffic areas and surfaces adjacent to the roundabout junction of Como Parade West, Heslop Street and Parkers Road should be the focus for design work and street improvements. Any planned works will be coordinated with the Traffic team's recommendations for improved pedestrian and vehicle movement.

The community is cohesive and articulate about their "village" atmosphere and family friendly shopping strip. There is pride in the historical significance of the area and its beach amenities. The proposed design will be sensitive to this aspect and reflect the feedback received for improved paving, safe pedestrian access, more street furniture and tree planting, and better signage and inclusion of street art.

3.3 Operation and Strategic Issues

3.3.1 Coordination of planned road and traffic management upgrades with Public Place design for streetscape improvements.

The challenge for this project is to simultaneously improve pavement, pedestrian movement and safety, traffic management, as well as the appearance of the Como Parade West intersection with Parkers Road to build on the successful shopping village ambience.

Integration of road upgrade activities and streetscape improvements requires project coordination across three Council departments (Infrastructure, Public Place Projects and Business Directions) to ensure disruption to the community is minimised.

3.3.2 Scope of Works

Given the different components of the project, the Parkdale Capital Works project needs to be fully integrated, it is essential that feasibility costing of optional design elements and the flow on implications of planned road and traffic upgrades be considered as a whole integrated project. The indicative area to focus streetscape improvements and proposed pavement treatments is attached as Appendix 2.

3.4 Proposed Improvements

The community's feedback aligns with the proposal to undertake a range of improvements to upgrade the southern end of the shopping centre surrounding the roundabout at the intersection of Como Parade West and Parkers Road (see Appendix 2). The objectives of this work would be to:

- Install a new pedestrian crossing opposite the station entrance to improve access and safety (pedestrian counts have indicated high numbers crossing Como Parade West close to the station).
- Undertake streetscape improvements to upgrade the pedestrian areas, incorporating attractive landscaping where possible.
- Improve the condition of deteriorated small areas of roads and kerbs.
- Investigate opportunities to improve the appearance of the retaining wall at the entrance to the station.
- Explore options for Street Art works as a separate project in consultation with local schools, PTV, Metro trains and Council Arts and Culture team.
- Integrate the measures with Stage 3 Works of the Parkdale LATM including side road entry treatments/humps proposed close to the railway crossing including the roundabout at the intersection of Como Parade West and the intersection of Parkers Road with Como Parade East.

4. Conclusion

It is recommended that a coordinated project approach to both streetscape improvements, pedestrian safety and road and traffic upgrades be undertaken to minimise disruption to Parkdale traders and the local community. This will provide a significant improvement in amenity and pedestrian safety to the village shopping precinct.

The integrated project approach will also reduce potential 'budget creep' and possible duplication of works required if carried out with due consideration to road and drainage infrastructure below ground as well as works planned above ground.

Additional enhancements to the area by way of street art can be explored once infrastructure and street planting has been completed. These works will be coordinated with the local community, schools and council's Arts and Culture team.

4.1 Environmental Implications

Not Applicable.

4.2 Social Implications

Council officers have consulted with the residential and business community to ensure that works are planned and implemented by a fair, equitable and transparent approach with the investment of funds to achieve best value for the community. Pedestrian safety and road upgrade work is proposed to be integrated into the streetscape improvements.

The planned works will contribute to the local economy and provide an improved level of amenity and liveability for residents and visitors to the Parkdale shopping precinct.

4.3 Resource Implications

Council resolved in the adopted budget to make available \$100K in 2018/19 Financial Year for new design and consultation work, and \$400K for construction in 2019/20 Financial Year in the Parkdale (Como Parade West) shopping centre area. The agreed streetscape improvement area will have regard to the available budget (\$500k in total).

Future year's works programs, and available resourcing across the Infrastructure and Business Directions teams, have been reviewed and the works have been brought forward. The coordination of these improvements with other planned works by the Traffic and Infrastructure teams will ensure that allocated funds across council are leveraged to achieve further efficiencies.

Concept plans will be developed to progress the detail design phase during 2018/2019, with implementation scheduled for 2019/2020.

4.4 Legal / Risk Implications

Carrying out the renewal of infrastructure and streetscape improvements in a planned and well-coordinated manner with appropriate prioritisation of these works will ensure that any risk to public safety is minimised.

To ensure that the condition of Council assets is maintained to a standard for minimal exposure to public liability claims. This aligns well with Kingston's Road Management Plan.

Appendices

Appendix 1 - Memo Parkdale Engagement - Capital Works (Ref 18/492579)



Appendix 2 - Appendix 2 - Parkdale Treatment Areas (Ref 18/610896) [↓](#)

Author/s: Angela Stubbs, Coordinator Business Directions
Reviewed and Approved By: Tania Asper, Manager Economic Growth and Innovation
Rachelle Quattrocchi, Manager Infrastructure
Jonathan Guttmann, General Manager Planning and Development

8.7

RESPONSE TO NOTICE OF MOTION 3/2018 - CAPITAL WORKS PARKDALE SHOPPING CENTRE

1	Memo Parkdale Engagement - Capital Works.....	273
2	Appendix 2 - Parkdale Treatment Areas	277



memo

TO	Rachelle Quattrocchi
CC	Tania Asper
FROM	Angela Stubbs
DATE	9 July 2018
SUBJECT	Parkdale Engagement – informing Capital Works 2018/19

In April 2018, Council resolved to make available \$100K in FY18/19 for new design and consultation work, and \$400K for construction in FY19/20 in the Parkdale (Como Parade West) shopping centre area. The Economic Growth & Innovation team engaged with the Parkdale community to seek feedback to inform Council's place making design team re improvements to the Como Parade West shopping strip and surrounding area.

To engage the local Parkdale community and seek their input into what they liked about Parkdale and what they would see as improvements to the physical landscape, an online feedback survey (via Council's 'Have Your Say' portal) was developed. In consultation with Council's communications team, a postcard was designed to attract attention and provide a 'call to action to direct the community to the online survey. This was distributed by hand to traders between June 1 and June 25, 2018 and letterbox dropped to surrounding residential areas. As the postcard was distributed in the street and shops in Como Pde West, some additional verbal feedback was gained (refer to attached spreadsheet).

The questions in the survey were aimed at:

- Seeking ideas and suggestions for potential improvements to the streetscape along Como Parade West – from Birdwood St to Herbert St
- Hearing from traders, customers and residents about what they currently like in the neighbourhood shopping village, and
- Identifying what they would like to see improved in the physical environment of Parkdale.

To raise awareness of the 'Have Your Say about Parkdale" campaign a digital screen promotion was also displayed in the Parkdale Library.

Social media posts on the Kingston Council and Kingston Business Facebook timelines also drew attention to the opportunity to provide comments to Council.

500 postcards were produced and distributed by hand to traders and residents. Additional postcards were left in the Parkdale Library, the Shirley Burke theatre/gallery and in the local cafes.

The community feedback received indicated there is a popular perception of Parkdale having a 'village' feel, being friendly and relaxed with great food offers and offering specialty shops. Family friendly and a vibrant meeting place with character and quirkiness was a common thread in responses.

In the "Other" category, free text responses included:

- The car park near the crossing should be paved
- Traffic and pedestrian management needs improvement so that parking and access is safer. and more amenable. There is no linkage with either side of Como Parade (east and West) which could improve the total atmosphere and unite the village feel more.
- Increase in the number of 30 minute parking spaces. Reduce the amount of 2 hour parking spots.
- Raised profile zebra pedestrian crossings at the Parkers Road roundabout, like those in Martin St, Gardenvale
- Lack of 30 minute parking spots. Customers are frustrated that they can't find a parking spot when they want to do a quick stop to pick up their medications.
- Info about the area - good walks, where the beach is, local icons and their history -some pictures of the village- really celebrate its history and its future
- Less cafes and more variety in the store offerings
- Wider footpaths
- Improved rubbish collections. More bins especially on the railway line side of Como West. Improved parking along Como Pde East.
- No bike riding through the shopping strip area to keep elderly safe
- Better cleaning of the footpath area
- Improved pedestrian crossings around Parkers Rd/ Como Parade East &West. Better provision for safe cycling.
- Upgrade of vacant land next to Library.
- Revitalised old petrol station site on Como Parade West
- More festive atmosphere at Christmas eg decorations instead of paper wrapped around rubbish bins
- Re-furnish Art Deco shops.
- Remove power poles - underground services.

In response to the question "What would you like to see improved in the streetscape", feedback was divided:

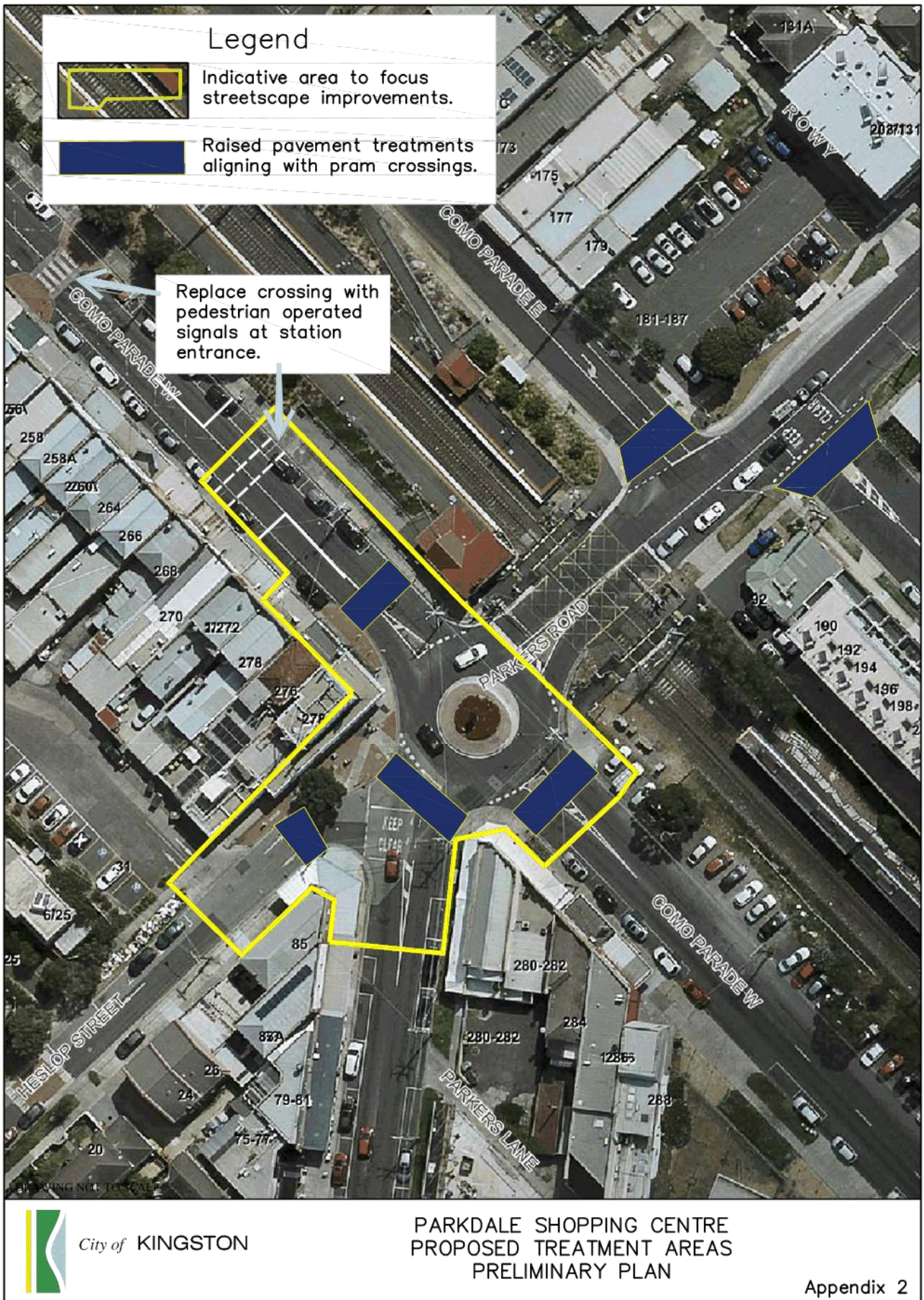
Surfaces (i.e. paving, paths)	25
Street furniture	22
Landscaping	21
Signage	10
Public art	13
Other	18

The demographics of respondents included:

Owner-occupier	39	Online response
Ratepayer	26	Online response
Renter	3	Online response
Business Owner	7	Online response
Worker	4	Online response
Student	3	Online response
Traders	6	Verbal feedback
Resident	1	Phone feedback

It appears from the feedback there is an issue of limited parking in Parkdale which will be referred to the Traffic Department for consultation. In addition, there is a strong preference for improved paving, safe pedestrian access, more street furniture and tree planting, and better signage and inclusion of more street art.

The community is cohesive and articulate about their “village” atmosphere and family friendly shopping strip. There is pride in the historical significance of the area and its beach amenities. In developing an upgraded capital works program the comments by respondents should reflect and enhance these elements and address the practical needs of surfaces, safety and mobility in the shopping strip.



9. Community Sustainability Reports

Ordinary Meeting of Council

10 December 2018

Agenda Item No: 9.1

ASPENDALE SENIOR CITIZEN WORKS - FINANCIAL IMPACT

Contact Officer: Leigh Stewart, Principal Maintenance Planning Officer

Purpose of Report

The purpose of this report is to advise Council of the financial impact of the works associated with the Aspendale Senior Citizens Club budget submission as endorsed at the 25th June Council Meeting.

Disclosure of Officer / Contractor Direct or Indirect Interest

No Council officer/s and/or Contractor/s who have provided advice in relation to this report have declared a Conflict of Interest regarding the matter under consideration.

OFFICER RECOMMENDATION

That Council notes the financial implications associated with the Council resolution to fully fund works contained within the Aspendale Senior Citizens Club 2018/19 budget submission.

1. Executive Summary

This report provides Council with information of the cost associated with the current and future proposed works at Aspendale Senior Citizens Club.

2. Background

Council adopted the 2018/19 budget with the requirement to fund a range of works detailed in the Aspendale Senior Citizens Club budget submission (refer to Attachment 1). The resolution required a report on the financial implications of the budget submission.

Following a detailed investigation, the current expected cost for all the proposed one off works is \$75,100.00 (Excl.GST) and an ongoing estimated \$7,000.00 for cleaning and technologies.

The Aspendale Senior Citizen Club has no lease in place and currently negotiating a lease for the facility with Council representatives.

Some items of work have already been completed, whilst more disruptive works is scheduled to take place during December 2018/January 2019, when the Aspendale Senior Citizen Club is not operating, e.g. floor sanding works.

3. Discussion

3.1 Council Plan Alignment

Goal 1 - Our well-planned, liveable city supported by infrastructure to meet future needs
Direction 1.3 - Infrastructure and property investment for a functional city now and into the future

Investing in creating an improved environment for the Aspendale Senior Citizens Club and visitors to the facility while improving the overall appearances of the building for the organisation and Council with assist in achieving the stated direction.

3.2 Consultation/Internal Review

Regular discussion has taken place with Council's Social Development Area and Aspendale Senior Citizens Committee and the works requested are either already completed or are scheduled to take place during December 2018 - January 2019 to minimise disruption.

3.2.1 Capital Budget Implications

The list of items requested by the Aspendale Senior Citizen Club, as detailed in attachment 1 of this report, is estimated to cost \$75,100.00. Councillors are asked to note the budget impact to address all the items contained within the Aspendale Senior Citizens Club 2018/19 budget submission.

3.2.2 Ongoing Budget Implications

In addition to the one-off capital items above, several items to be funded will require ongoing Council operational funding in the future. These operational costs include contract cleaning and provision of internet services. These ongoing costs are estimated at approximately \$7,000.00 per annum and Council are asked to note these on-going costs.

3.2.3 Building Lease/Licencing Issues

The Aspendale Seniors Group have not previously held a formal agreement for the use of the building. The Aspendale Seniors Group have confirmed their willingness to consider a formal lease with additional use, subject to there being no detrimental effect on their club.

4. Conclusion

The budget submission received from Aspendale Seniors Group will require a one-off capital contribution of \$75,100.00 and an ongoing allocation of \$7,000.00 per annum. It is proposed that Council note the budget impact to address all the items contained within the Aspendale Senior Citizens Club 2018/19 budget submission.

4.1 Environmental Implications

Improvements within the Aspendale Senior Citizens Club will be made in accordance with the relevant standards and Environmental Sustainable Design (ESD) policy. External security lighting to be LED energy efficient.

4.2 Social Implications

The proposed amenity improvements to Aspendale Senior Citizens Club are intended to improve the overall presentation and functionality of the room as a primary meeting space within the building.

The most significant works would be scheduled over the holiday break to minimise disruption to meetings, and activities held at the venue.

4.3 Resource Implications

Following detailed investigations of the items requested by Council in June 2018, officers have identified that a total budget allocation of \$75,100.00 (Excl. GST) is required during 2018/19. A further \$7,000.00 ongoing allowance will be needed as a recurrent cost for cleaning and technology costs. These costs were not budgeted for but will now be addressed in the mid-year budget review.

4.4 Legal / Risk Implications

Council is asked to note that support for this request for funding may potentially lead to additional requests from other clubs not currently receiving similar support for their activities, which could impact on Council's future Capital and Operational budgets.

Appendices

Appendix 1 - Aspendale Seniors Club - Budget Submission (Ref 18/614226)



Author/s:	Leigh Stewart, Principal Maintenance Planning Officer
Reviewed and Approved By:	Steve Lewis, Manager Community Buildings Mauro Bolin, General Manager Community Sustainability

9.1

ASPENDALE SENIOR CITIZEN WORKS - FINANCIAL IMPACT

1	Aspendale Seniors Club - Budget Submission.....	287
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A WISH LIST FROM ASPENDALE SENIORS CLUB

Building and maintenance requests

- **To have the hall floor professionally treated.**-For the last 4 years the hall has been sanded and varnished by 2 committee members. (If possible we would appreciate if it could be done between mid December and mid January so the club does not have to close down)
- **Front Entrance.** At present, anyone waiting outside the front door, has no protection from the weather. Some sort of shelter would be a great improvement. Possibly need a security alarm as doors have been forced twice and front foyer is dark. Much rubbish gets blown in through the front doors.
- **Back Entrances** The 2 lights by the back door do not work and the security door in the kitchen does not lock properly.
- **Beautification of club** Outside and inside could do with a clean and possibly a new paint job. (our cleaner cannot reach the high points inside the club)
- **Roofing, Guttering and Down Pipes.** On windy days, the roof sheets bang up and down and guttering probably needs cleaning
- **Sealing the gap between windows and window frames.** On some windows facing the carpark there is a gap where much dust gets blown in.
- **Carpet in Broadband Room** Council officers have already inspected the carpet and stated it needed to be replaced

May 2018

A WISH LIST FROM ASPENDALE SENIORS CLUB

Equipment and Other Requests

- **Angle Parking** A request has been put to the council to consider angle parking as we are very short of parking space and with our members getting older, it is hard for them to walk any distance.
- **Mats for Gentle Exercises** This group needs about 10 mats for floor exercises.
- **Bowls Stand** Our bowls group have requested a new stand for their bowls mats.
- **Honour Board** We need an additional honour board to display our committee members and our life members. The current board has no more room for life members.
- **Internet access** Council to pay for internet access and connection
- **Contract Cleaner** Council to pay for cleaner
- **Extra Power Points** These are needed in the rear of the hall to facilitate the filming of performances. Currently a long lead has to be trailed from existing power points
- **New Printer/Copier** It would be good to have a new printer/copier to replace the present antiquated ones which are difficult to use and on their last legs.
- **Concrete slab for BBQ. (750mm x 1500mm)** It would be useful to have a concrete slab outside the back door to stand our BBQ on.
- **I Pad for music system.** The present system can be unreliable.

May 2018

10. City Assets and Environment Reports

Ordinary Meeting of Council

10 December 2018

Agenda Item No: 10.1

CON 18/50 - STORMWATER DRAINAGE MAINTENANCE

Contact Officer: Tim Scott, Team Leader Maintenance Contracts and Waste

Purpose of Report

This report seeks Council's approval to award Contract No.18/50 Drainage Inspection and Cleaning, to a panel of two contractors based on a schedule of rates contract for a total value up to \$650,000 for a three year period with the option of two further two year periods.

Disclosure of Officer / Contractor Direct or Indirect Interest

No Council officer/s and/or Contractor/s who have provided advice in relation to this report have declared a Conflict of Interest regarding the matter under consideration.

OFFICER RECOMMENDATION

That Council:

1. Award contract No.18/50 Drainage Inspection and Cleaning, to a panel of two companies, Veolia and GMA Waste Water Pty Ltd as a combination of a lump sum component and schedule of rates contract for three years with a further two, two-year options at Council's discretion as per pricing, for a total contract sum of \$650,000 per year; and
2. Authorise the Chief Executive Officer or their delegate to execute this contract and award a further one-year option subject to the contractors' satisfactory performance.

1. Executive Summary

This report seeks approval for the award of Contract No.18/50 Drainage Inspection and Cleaning to a panel of two companies, Veolia P/L and GMA Waste Water Pty Ltd. Officers have evaluated tender submissions received for Contract No.18/50 and have ranked tenderers against predetermined selection criteria of quality and financial factors.

GMA Waste Water P/L have provided Council with drain cleaning services for the last 6 years, while Veolia P/L undertake this role with the City of Port Phillip. Both organisations have a long history of successfully undertaking Council service contracts in the drainage maintenance field in Victoria.

The recommended tenderers have been assessed for financial viability and OHS checks and have been reference checked with other Melbourne Councils.

2. Background

The City of Kingston has an extensive drainage network for which it has statutory obligations under the Water Act and Local Government Act. Part of these obligations require the provision of an operational drainage network to safe service standards. This report allows for two contractors to be appointed to provide drainage inspection and cleaning services for both programmed and reactive works.

The existing Drain Cleaning Services contract is with Pipeline Services P/L, Rangedale P/L and GMA Waste Water P/L and was approved by Council on 28th May 2012 on a schedule of rates basis for a 3-year period with an option to extend for 1 year + 1 year + 1 year, and up to 6 months on a monthly basis.

3. Discussion

3.1 Council Plan Alignment

Goal 1 - Our well-planned, liveable city supported by infrastructure to meet future needs
Direction

1.3 - Infrastructure and property investment for a functional city now and into the future

The works undertaken as part of Contract No.18/50 are general drainage cleaning and inspection referred by Council to the Contractor. This type of service is required to effectively maintain Council's drainage network which assists in the prevention of flooding of private and public land.

3.2 Consultation/Internal Review

This Drainage cleaning and inspection service was consulted with the Engineering Design team and the Asset Management team. The Engineering Design team request underground pipe video reports to assist in condition assessments of assets. The Asset Management team will manage the data capture component of this contract which will assist internal staff with asset identification.

3.3 Operation and Strategic Issues

This contract is required to effectively maintain the 800km of Council managed underground drainage pipes and 33000 drainage pits that services the wider community. As such, providing a cleaning and inspection service to these assets will reduce future capital expenditure whilst still maintaining a satisfactory level of service.

Based on the number of customer requests received by Council for drain cleaning and the rates provided by the tenderers, Contract 18/50 should provide Council with an appropriate service. The appointment of two contractors will give Council the flexibility to provide an appropriate service when required.

The existing 6-year contract with three panelists for this type of service expires on July 30th 2018, with the option for a maximum 6 month extension.

3.4 Tender Evaluation

Prices were sought by Advertised Public Tender with tenders closing Thursday 20th September 2018, at 2:00pm.

One tender was received and evaluated by a Tender Evaluation Panel, consisting of Tim Scott, TL Maintenance Contracts & Waste, Robin Fisher, Maintenance Coordinator and Jeanne Roussow, Senior Drainage Engineer. Procurement procedures were followed in line with advice received from Council's procurement department for previous "standard" service contracts of a similar value.

Tender Evaluation for contract 18/50 was undertaken by an evaluation method which confirmed that two tenderers were appropriate. The tender prices were evaluated against the existing Drain Cleaning Services contract and confirmed that the rates were very similar and appropriate.

The detailed evaluation of the tender was confirmed through the following criterion:

- Schedule of Rates price comparison for each component;
- Company history in similar roles;
- Quality of works in similar roles;
- Responsiveness to customer requests for similar Councils;
- The provision of an electronic reporting system sympathetic to Councils Asset Management System;
- Access to suitable plant and equipment;
- Access to suitable sub-contractors and in-house trades where required;
- Relationship with contract principal and residents;
- Insurances;
- OH&S compliance (**renewals being updated**); and
- Financial viability.

Following the evaluation of the minimum criteria, non-price and price criteria, GMA Waste Water P/L and Veolia were invited to an interview. As a result, both GMA Waste Water and Veolia were selected as the preferred tenderers.

3.5 Occupational Health

As per the attached, Council's Procurement and Contracts team has approved GMA Waste Water P/L and Veolia as Rapid Compliant.

3.6 Options

3.6.1 Option 1

That Council:

1. Award contract No.18/50 Drainage Cleaning and Inspection to Veolia for the lump sum component of the contract and award the schedule of rates component to a panel comprising of GMA Waste Water P/L and Veolia for three years with a further two, two-year options at Council's discretion as per pricing schedules for a total contract sum of \$650,000 per year; and
2. Authorise the Chief Executive Officer or their delegate to execute this contract and award a further one-year option subject to the contractors' satisfactory performance.

3.6.2 Option 2

- 1.** Not award this contract.

4. Conclusion

GMA Waste Water P/L and Veolia are the preferred tenderers for the panel Contract 18/50 Drainage Cleaning and Inspection, representing the best value for Council in providing a service for drainage asset cleaning and electronic inspection to drains managed by Council.

4.1 Environmental Implications

The cleaning of the underground drainage network is required to optimise drainage outcomes on public and private land in Kingston. This ensures that roads remain trafficable in rainfall events and that storm water runoff is conveyed away from areas of high risk. The maintenance and inspection of drainage assets ensures that water quality is also prioritised in areas where still water may present odour or insect problems.

4.2 Social Implications

This contract supports continued maintenance of the drainage network within the municipality, which provides a level of confidence to residents that there will be minimal flooding of both public and private land. This aligns with the requirements of the Road Management Plan to provide a safe and efficient road network for the community.

4.3 Resource Implications

This contract is to be funded and managed from within the existing budget allocation within the Infrastructure Road Maintenance operational budget.

4.4 Legal / Risk Implications

Routine drainage maintenance ensures that service levels are maintained, a safe and efficient drainage network is provided, and defects are minimised that may otherwise expose Council to claims from potential flooding issues or incidents.

Appendices

Appendix 1 - CON 1850 Schedule of Items Analysis for Drain Cleaning and Inspection
(Ref 18/606932) - Confidential

Author/s: Tim Scott, Team Leader Maintenance Contracts and Waste
Reviewed and Approved By: Rachele Quattrocchi, Manager Infrastructure
Daniel Freer, General Manager City Assets and Environment

Ordinary Meeting of Council

10 December 2018

Agenda Item No: 10.2

KINGSTON DISC GOLF COURSE STAKEHOLDER CONSULTATION FINDINGS AND NEXT STEPS

Contact Officer:

Purpose of Report

The purpose of this report is to present details of the community and key stakeholder feedback regarding Council's proposal to establish a disc golf course at Bald Hill Park, Clarinda.

Disclosure of Officer / Contractor Direct or Indirect Interest

No Council officer/s and/or Contractor/s who have provided advice in relation to this report have declared a Conflict of Interest regarding the matter under consideration.

OFFICER RECOMMENDATION

1. That Council note the feedback received from the community in response to the establishment of the proposed disc golf course at Bald Hill Park;
2. That Council confirm Bald Hill Park, Clarinda as the preferred site for the establishment of a Disc Golf Course; and
3. That the establishment and activation of a disc golf course at Bald Hill Park be initiated in the current financial year's funding for the purchase of 'City of Kingston' branded discs for a six month trial loan program through Council's library services (primarily at Clarinda and Westall libraries), Council's aquatic centres and the Clayton Bowls Club.

1. Executive Summary

Following detailed investigations, specific site risk assessment and a community consultation process, there is strong support for the establishment of a disc golf course at Bald Hill Park, Clarinda.

Risk matters identified with the specific site risk assessment by Jardine Lloyd Thompson (JLT) can be accommodated as part of further design development and or addressed as part of management of the disc golf facility. A revised disc golf course design has been produced and is set out as Appendix 1. This design also responds to community concerns regarding potential tensions between disc golf users and dog off leash activities at the site.

In addition to the establishment of a disc golf course, it is recommended that Council provide operational funding to support the purchase of Kingston branded disc for a 6-month trial loan program through Council's library services (primarily at Clarinda and Westall libraries), Council's aquatic centres and the Clayton Bowls Club (partnership approach). This will assist to activate the new disc golf facility for access by local Kingston residents.

The disc golf facility is proposed to be open to the general public and is considered a low to no cost service provided by Council to the Kingston community.

2. Background

On the 24 July 2017 Council resolved by Notice of Motion 28/2017 - Disc Golf, the following:

1. *'That Council officers write a report on the best possible locations for a permanent Disc Golf Course within the City of Kingston and the requirements and costing for installing a course. Further that, this report is to be presented to the August Ordinary Council meeting.'*

This report was presented at the 28 August 2017 Ordinary Council meeting and as a result, Council endorsed as follows:

1. Council work with interested peak disc golf agencies to explore the further assessment, including preliminary site feasibility of preferred sites, appropriateness (design considerations) and risk Management, of the following preferred sites to support disc golf:

- *Bald Hill Park, Clarinda;*
- *Heatherton Park, Heatherton;*
- *Braeside Park, Braeside (Parks Victoria site).*

2. Council receive the findings from the preliminary site feasibility assessments no later than February 2018.

At its 26 February 2018 meeting, Council resolved to:

- 1. Provide in principle endorsement of Bald Hill Park as the preferred location for a disc golf course for the purpose of completing a specialist risk assessment of the suitability of this site; and*
- 2. Continues to work with the Melbourne Disc Golf Club on this location and receives a further report presenting the results of the specialist risk assessment no later than May 2018, and steps required to establish a facility at Bald Hill Park.*

In response to Council's consideration of this matter the following actions have been undertaken:

- Consultation with Melbourne Disc Golf Club and review of locations across the City to assess and determine 10 potential sites for use as a permanent disc golf venue. The Melbourne Disc Golf Club currently uses 2 temporary 'pop up' disc golf courses in Kingston on Melbourne Water land in Aspendale Gardens. It also hosts regular 'come and try' disc golf events at venues such as Braeside Park, Kingston Heath Reserve and Bald Hill Park. These events generated interest from new participants from within Kingston and surrounding areas. Whilst the Club has made good use of its temporary access in Aspendale Gardens, its long-term goal is to establish a permanent disc golf venue in Kingston that will provide for local and regional catchments i.e. southern bayside area of Melbourne;
- DiscGolfPark was engaged by Council in December 2017 to undertake concept design and site assessments at Braeside Park and Bald Hill Park. This work was completed and reviewed by officers, Councillors and representatives from the Melbourne Disc Golf Club. As a result of these investigations, Bald Hill Park was determined to be the preferred site;
- Specialist risk assessor, Jardine Lloyd Thompson (JLT), was engaged to provide specialist risk management advice in March 2018;

- On the 21 May 2018, Councillors considered the findings of the specialist risk assessment completed for Council by Jardine Lloyd Thompson as well as a revised disc golf course design. Following this discussion officers commenced community consultation activities, including distribution of an information bulletin to surrounding residents, on-site temporary signage, online survey and an on-site come and try/information session to seek feedback from residents and regular park users regarding the proposed disc golf course at Bald Hill Park.
- The original disc golf course design was modified in response to matters identified within JLT's risk assessment report (July/August 2018);
- Disc golf representatives have been engaged throughout the process and have provided support for the project via on-site meetings, Councillor briefings, sharing of sport specific information/resources, and support with on-site come and try activities;
- A 3.5-week resident and stakeholder consultation and engagement period has been completed adopting methods such as resident letterbox drop (approx. 1000 homes), online survey and information, on-site signage promoting details of the proposal; and an on-site come and try disc golf event held on 26 October 2018. The consultation period was completed between 15 October → 09 November 2018, including an on-site 'Show and Throw' event hosted on 26 October 2018. The consultation period was scheduled in Spring when the weather was expected to be more favourable for hosting events.

2.1 Council Plan Alignment

Goal 3 - Our connected, inclusive, healthy and learning community
Direction 3.4 - Promote an active, healthy and involved community life

The 2018 Kingston Sport and Recreation Strategy and Council's Public Health and Wellbeing Plan support the provision of unstructured sporting participation opportunities as well as social/family recreation activities and low cost physical activities. Installation of a disc golf course at Bald Hill Park, Clarinda is one means of providing less formal sport and recreation participation options in Kingston.

2.2 Consultation/Internal Review

Community consultation upon the establishment of a disc golf course at Bald Hill Park has included:

- 1000 homes adjacent to Bald Hill Park received an information bulletin regarding Council's disc golf proposal;
- Direct contact and conversation regarding the proposal was initiated with 4 local schools, 2 neighbouring LGAs, 2 peak sporting bodies, and Council's Sport and Recreation Advisory Group;
- An on-line platform including project information and electronic survey option was available *via yourkingstonyoursay.com.au* for 3.5 weeks; 52 responses were received in total (online and email); and
- An on-site come and try disc golf event and barbecue was hosted by Council officers and Melbourne Disc Golf Club on 26 October between 4 and 6pm – 51 people attended with 27 people participating in the come and try activities.

The following table summarises the feedback and input received through the engagement activities:

Consultation Method	Findings/Outcomes
Your Kingston Your Say	Of the 52 responses received online, 48 respondents indicated their support for the proposed disc golf course (refer Appendix 2 – summary report and Appendix 3 –full report).
Community Information Session	<p>An on-site community information/come and try session was held at Bald Hill Park on Friday 26 October 2018 between 4 and 6pm. The Cheltenham Rotary Club provided a BBQ for participants and Melbourne Disc Golf Club ran come and try activities.</p> <p>A total of 51 people were recorded as attendees to the information session with 27 people participating in the come and try activities.</p> <p>Comments received from the community information session are set out at Appendix 3.</p>
Individual responses	<p>Individual submissions were received from 3 occupants of three residential properties (Appendix 4).</p> <p>These submissions raised two main concerns regarding flying discs in a public space as well as conflict between dogs and discs. The risk assessment completed by JLT addressed both of these matters and measures have been taken to mitigate these risks including relocation of hole 7 from within the dog off-leash area to within the general park space, and an assessment of the risk of being hit by a flying disc as being ‘low risk’ – disc baskets and tees are positioned to enable clear sight lines to other park users, and it was determined through the assessment that less skilled participants are not likely to have the ability to fly a disc at great pace. Primary to any disc golf activities is the ‘rule’ for participants to ‘yield/give way to all other park users’ and signage will be used to remind participants of this at each tee, as well as alerting general park users to the presence of disc golf activities.</p>

2.3 Operation and Strategic Issues

2.3.1 Activating the facility for use by Kingston community and visitors

Feedback received via consultation with the community and stakeholders highlighted a variety of ways that Council can support the community’s use of a disc golf venue at Bald Hill Park.

Should a disc golf facility be established at Bald Hill Park, it is suggested that a 6-month trial disc loan program be undertaken; this would involve discs being made available to residents and visitors potentially through Council’s libraries and/or customer service centres/leisure centres. Officers are exploring this approach and have established that it is feasible and would also be of benefit to the libraries as a means of engaging residents with Council’s library services. Discs would be barcoded and borrowed and returned in the same manner as books/dvds/CDs etc.

The Clayton Bowls Club have also provided initial support via discussion with officers, to act as a disc loan venue, in a similar way to the libraries. The Club sees opportunities to engage new people in its activities as a result of supporting Council to loan discs to disc golf participants.

Other options for encouraging usage include partnering with local schools, Melbourne Disc Golf Club running a school holiday program on-site, hosting an annual come and try event and social media promotion.

3. Conclusion

3.1 Environmental Implications

The sport of disc golf is a low impact activity and requires no site specific environmental modifications. The use of Bald Hill Park for the purpose of disc golf will have limited impact on the environment i.e. no loss of vegetation.

The risk assessment undertaken by JLT (Appendix 5) makes reference to the former use of Bald Hill Park as a landfill venue and the need to complete an environmental/contamination assessment to inform the management of risks due to soil contamination and other latent soil condition issues. It is anticipated that any works would need to be strictly managed via an Environmental Management Plan

There is likely to be additional cost associated with the management of the soil contamination issues with the establishment of a disc golf course at Bald Hill Park.

3.2 Social Implications

The establishment of recreational and sporting facilities which respond to the interests of our community is likely to result in positive social and wellbeing outcomes in Kingston. Provision of a low cost, informal sporting activity for residents in the north of Kingston (and visitors) broadens opportunities for participation in physical activity.

3.3 Resource Implications

Costs associated with the establishment of a 9-hole dual tee disc golf course are estimated to be up to \$80,000, comprising:

- \$30,000 for final course design, disc golf baskets/equipment, signage design and supply, labour/installation and input from technical consultants;
- \$24,000 for site preparation including footing requirements for disc golf baskets at Bald Hill Park (contingency allowed for its former use as a landfill site);
- \$16,000 for supporting amenities and landscaping such as seating, site remediation, plantings; and
- \$10,000 to 'activate' the venue via a community launch event, school holiday program, promotional activities and purchase of disc golf equipment for loan via Kingston libraries, Kingston Active leisure centres, Kingston customer service centres and Clayton Bowls Club.

An estimated ongoing annual provision of \$5,000 from Council is recommended to fund repairs, equipment replacement and damage, which would be outside of the financial means of the Disc Golf Club. This funding of these repairs is similar to Council's current responsibility for the repair and maintenance of goal posts i.e. AFL and soccer in open space areas.

To progress with the timely establishment and activation of a disc golf course at Bald Hill Park, Clarinda it is recommended that funding is identified from within the 2018/19 budget.

3.4 Legal / Risk Implications

In response to the Jardine Lloyd Thompson's risk assessment at Bald Hill Park (Appendix 5), measures to mitigate risk have been incorporated into a revised disc golf design. Further, the provision of future management and maintenance of the facility will also be responsive to the risk assessments recommendations.

Appendices

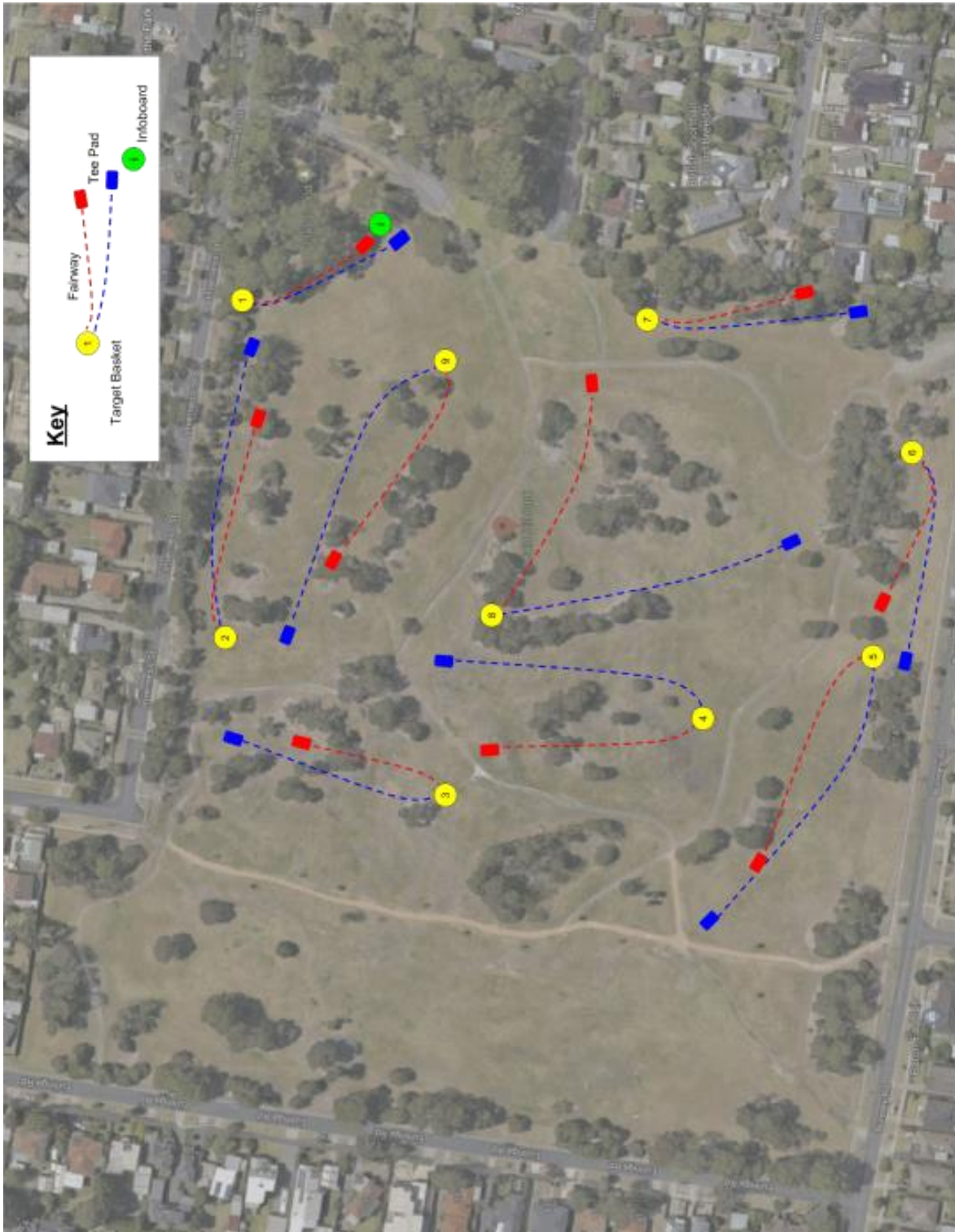
- Appendix 1 - Bald Hill Disc Golf Park Revised Draft Design (Ref 18/606482) [↓](#)
- Appendix 2 - Disc Golf Feedback Summary (Ref 18/605874) [↓](#)
- Appendix 3 - Disc Golf Online Survey Report (Ref 18/605952) [↓](#)
- Appendix 4 - Disc Golf Consultation Summary (Ref 18/606919) [↓](#)
- Appendix 5 - JLT Consulting Disc Golf Risk Assessment Bald Hill Report FINAL (Ref 18/69246) [↓](#)

Author/s: Mark Stockton, Team Leader Sport and Recreation
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Daniel Freer, General Manager City Assets and Environment

10.2

KINGSTON DISC GOLF COURSE STAKEHOLDER CONSULTATION FINDINGS AND NEXT STEPS

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4	Disc Golf Consultation Summary.....	325
5	JLT Consulting Disc Golf Risk Assessment Bald Hill Report FINAL.....	337



Bald Hill Park
Dual Tee 9 Target Course Concept Design Layout_v4

Do you support the introduction of a disc golf course at Bald Hill Park?

YES - 48 people NO – 4 people

Most common reasons for supporting the proposal:

- Disc golf is an inclusive activity
- Disc golf is fun/social
- Family friendly activity
- Low cost/affordable recreation activity
- Great use of the park, plenty of space
- It will bring more people to visit Kingston’s beautiful parks
- Encourages interaction between different users

Comments for not supporting the proposal related to:

- Safety concerns regarding flying discs
- Conflict between dogs off leash and disc golf participants

If a disc golf course is introduced at Bald Hill Park would you, your family or friends use it?

- 48 people answered Yes, 4 people answered No.
- Most common comments:
- Park is close to home
- It’s a beautiful park setting
- Local low cost recreation activity
- Whole family can be involved
- There are very few disc golf courses available in Melbourne
- One comment opposed use of the facility as it ‘will spoil the peace and tranquility of the Park’

How could Council encourage and/or support your participation in disc golf?

- Make discs available at local libraries
- Host events at the new facility
- Maintain a quality facility year round
- Include rules and how to play information on signage
- Work with local schools to encourage their use
- Partner with Melbourne Disc Golf Club to run Come and Try activities

Do you have any other comments you would like to make about Bald Hill Park?

- It’s a fantastic park
- Perfect setting for disc golf
- Friendly locals
- Needs more seats
- Well maintained park
- Concerned about rate increases as a result of installing a disc golf course
- Disc golf would encourage park visitation
- Need more shaded seating in off leash dog exercise area
- It’s common to see dogs off leash in on-leash areas of the Park

Survey Report

15 October 2018 - 12 November 2018

Bald Hill Park - Proposed Disc Golf Facility

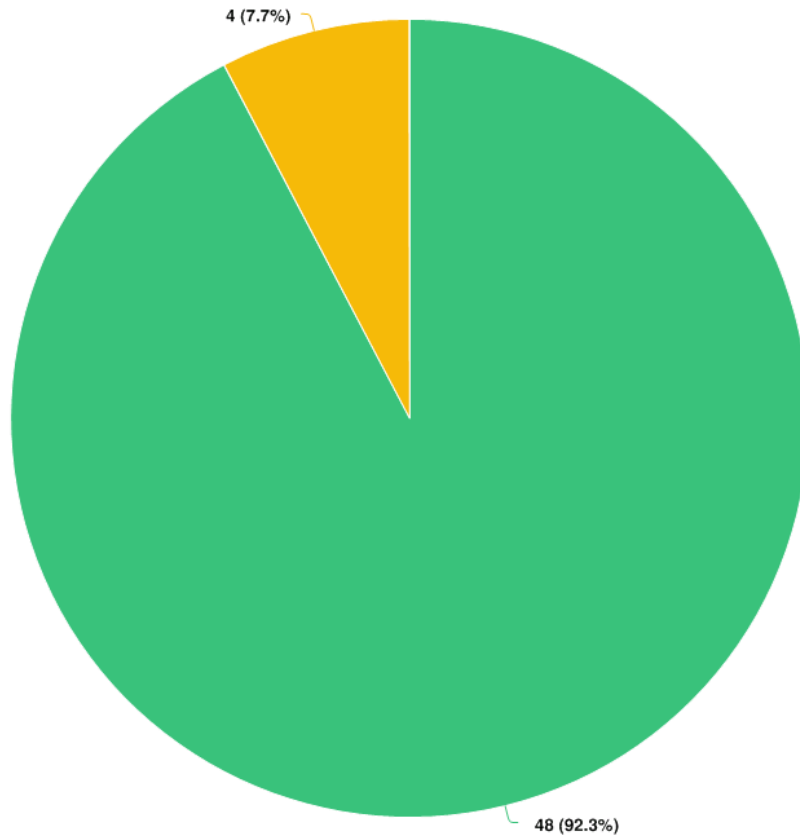
PROJECT: Bald Hill Park Clarinda - Disc Golf Facilities

Your Kingston Your Say



Bald Hill Park - Proposed Disc Golf Facility : Survey Report for 15 October 2018 to 12 November 2018

Q1 1. Do you support the introduction of a disc golf course at Bald Hill Park?



Question options

- Yes
 - No
- (52 responses, 0 skipped)

Bald Hill Park - Proposed Disc Golf Facility : Survey Report for 15 October 2018 to 12 November 2018

Q2 Why? Why not?

Darren Stace-Smith

10/17/2018 07:08 PM

Disc golf is an excellent and inclusive low impact sport, available to all social and economic members of the community

Jeff Brunsting

10/17/2018 09:47 PM

Disc Golf is a great sport that is easy to learn and can be played by people of all ages and abilities.

Krayz

10/18/2018 08:14 AM

It's a new exciting sport that provides the area for using a frisbee.

J.Crow

10/18/2018 08:39 AM

Disc golf is growing rapidly and there are very few facilities. It's a rewarding outdoor pursuit that will see more residents utilize existing parkspace.

Carol Heath

10/18/2018 09:04 AM

My family were introduced to disc golf a few years ago and it has provided our entire family with an opportunity to get outdoors and enjoy a sport which is accessible for all. I have limited mobility after a serious injury but still enjoy playing and even compete in tournaments. That is what I love about this sport. It does not discriminate on age, gender or ability The sport even took us to the US earlier this year where my daughter competed in the junior world disc golf championships. We desperately need more courses in Melbourne. People travel from far afield to play the course at Doncaster weekly and we would definitely do the same for Bald Hills. We played in a tournament there last year and absolutely loved the park.

Brian Holden

10/18/2018 09:12 AM

Get people outside and active.

Paul Arden

10/18/2018 09:46 AM

Disc Golf is a fantastic low impact, accessible sport which allows people of any age to stay active and have fun in the outdoors. Currently players on the south eastern side of Melbourne need to travel to Doncaster to play on at least a 9 hole Disc Golf course. The addition of the Disc Golf course will expose a much wider group of people and provide facilities for those already living in the area.

DavidHeath

10/18/2018 10:03 AM

Disc Golf is a great sport for all the family to participate in, it's one where someone can be as active (or inactive) as they choose. My family has played various temporary courses in the SE suburbs and has found the area to offer plenty of variety. We live in the Doncaster region, so tend to visit Ruffey Lake Park to play, but there's a lot to be said for playing a variety of courses to increase skills. Both my children are current national junior champions having learned to play the sport in the past few years. Further, my daughter is current world under-12s doubles champion and came 4th in singles... all because she was able to play on a local park - who knows how far residents of Kingston can take the sport once they have their own local course!

Pat U

10/18/2018 10:18 AM

I believe disc golf is a fantastic sport that people of all abilities can play. It is a low impact sport and has minimal effect on the surrounding environment however provides hours of enjoyable outdoor fun. It is a fast growing sport and would bring a significant number of visitors to the Kingston area!

Bald Hill Park - Proposed Disc Golf Facility : Survey Report for 15 October 2018 to 12 November 2018

<p>Kshick 10/18/2018 10:26 AM</p>	<p>Good to see people making use and having fun at the park. I live right at the front of the park</p>
<p>Tarron 10/18/2018 01:06 PM</p>	<p>I think DG is a great activity for all ages and putting a course in will get more people outside using our beautiful parks.</p>
<p>Oscar 10/18/2018 01:12 PM</p>	<p>Disc golf is an inclusive sport for every age and ability. It also works with existing infrastructure very well. It's healthy and gets you outdoors for some fun.</p>
<p>Jonno C 10/18/2018 05:02 PM</p>	<p>Disc golf is a sport for all ages and is a fun way to get some exercise. I'd like to see another course addition in Melbourne.</p>
<p>SK2 10/18/2018 05:46 PM</p>	<p>Need another fun activity the community can get involved in. Is a very inclusive sport</p>
<p>Jay Ros 10/19/2018 07:08 AM</p>	<p>Disc Golf is a great social and family sport and has created a friendly and diverse community at my local course at Ruffey Lake Park Doncaster. I've been practicing recently at Bald Hill and a number of friendly locals have stopped for a chat and shown keen interest. I invited them to the come and try event Fri 26 Oct and they seemed really keen. The community benefits are many: health, social, community , park utilisation, support Australian sport (and future champions amount the youth!)</p>
<p>Juha 10/19/2018 04:48 PM</p>	<p>Disc golf is growing around Victoria and Australia and the only hiccup for the growth to keep on happening is the lack of courses around. This would be a great addition to Melbourne's disc golf scene.</p>
<p>Kaia 10/19/2018 06:25 PM</p>	<p>I think it's a great idea to utilise all the space</p>
<p>Dimi 10/20/2018 11:28 AM</p>	<p>I</p>
<p>Alastair S 10/20/2018 06:12 PM</p>	<p>I play Ultimate Frisbee and i would love to be able to play disc golf casually as well</p>
<p>Chris E 10/21/2018 10:18 PM</p>	<p>Disc Golf is a great fun and very accessible outdoor sporting activity that can be enjoyed by all.</p>
<p>RBH 10/23/2018 09:44 PM</p>	<p>Flying frisbees or any other flying object in a public space that is used for walking is a dangerous idea that Kingston council would be foolish to implement. In the event of flying objects hitting people including vulnerable people like the disabled or the elderly or small children is just foolhardy and will leave the Kingston council open to being sued for personal and physical damage. People who are not experts at this form of sport and that would be the majority of users would be potentially a hazard to any person walking in Bald hill Park. I would object most strenuously to such an activity as any form of Disc golf facility at Bald Hill Park for safety reasons.</p>
<p>honey ant 10/24/2018 12:03 PM</p>	<p>encourages healthy activity in public space</p>
<p>Daniel Sizer</p>	<p>Disc golf is a fun and friendly game to play with little to no impact on other</p>

Bald Hill Park - Proposed Disc Golf Facility : Survey Report for 15 October 2018 to 12 November 2018

10/24/2018 02:12 PM	park users.
Jenko17 10/24/2018 07:02 PM	I love disc golf
Fordcam 10/26/2018 12:19 PM	An amazing family fun activity that promotes so many amazing things.
Margaret Irvine 10/26/2018 12:33 PM	Fear of being hit as I am disabled.
Charlie 10/26/2018 01:08 PM	Disc golf is an easy to play form of recreation for all ages. In Toronto, Canada as an example there are many permanent courses throughout the city which are heavily utilised in summer.
James123 10/28/2018 01:35 PM	I love frisbee golf
Paul Barbagallo 10/29/2018 12:27 PM	It's an affordable sport for people of all levels and ability. As an older male it's made great improvements to my mental well-being and connection to the community.
Margaret 10/31/2018 04:07 PM	1. The removal of ground cover plants has already taken place in readiness for an upcoming event. If permanent activity I am concerned that animals and birds will be affected by these changes. 2. This kind of activity will affect walkers and general public enjoying the park - disc golf is limited to some people not all.
Jennifer Pemberton 11/02/2018 10:30 AM	I think this is an excellent opportunity to provide facilities that promote health active lifestyles.
Jarrath 11/08/2018 05:41 PM	Great Sport! Would fit into this park perfectly.
Toosje 11/08/2018 05:51 PM	It's a fantastic sport , that caters for everyone's needs.
tahlia 11/08/2018 06:00 PM	It's a fun activity and low environmental impact.
Rhye 11/08/2018 06:01 PM	Great opportunity for another outdoor activity
Taz 11/08/2018 06:03 PM	its a great way to get people involved in the community and be active outdoors
Tbo 11/08/2018 06:58 PM	I recently played in the competition there and it reminded me of my local course at Barwon Valley. It was great to interact with the dog walkers on the day and I can see it really taking off in the area.
RyanBudge 11/08/2018 07:00 PM	Disc golf is a great sport for fun and to get out and be active. Perfect for any age and is cheap to play.
Cassie Sweetten 11/08/2018 07:34 PM	Disc Golf is a fantastic opportunity for the community to learn about. It's an amazing sport for people of all ages!

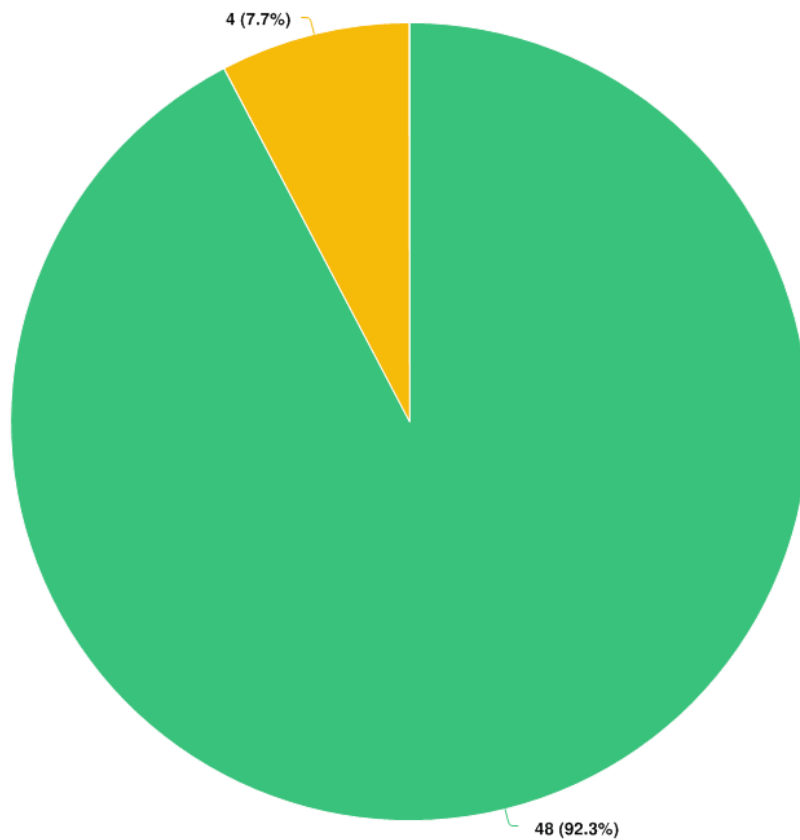
Bald Hill Park - Proposed Disc Golf Facility : Survey Report for 15 October 2018 to 12 November 2018

Mattellis 11/08/2018 07:41 PM	Fantastic sport that anyone can participate in. Promotes healthy living.
Kaspar 11/08/2018 07:44 PM	As a disc golf player for 6 years any new courses to play are great. It also allows people of all ages and abilities to get outdoors and enjoy some low impact exercise and social interaction.
Matt Snell 11/08/2018 08:58 PM	Great use of public space
Gizmo 11/08/2018 09:46 PM	Great community engagement idea Is fun, good exercise and very social
AmySS 11/08/2018 10:07 PM	It's a great initiative to get our community more active at a low cost to individuals!
blairu 11/09/2018 06:47 AM	Disc golf is a great activity for all ages, and has an extremely low price barrier to play!
Janet Abbott 11/09/2018 10:38 AM	Better facilities to get the family out and about
Lou van Beveren 11/09/2018 02:13 PM	Sounds like good fun, suitable for all ages
Mat Rowan 11/10/2018 08:56 PM	People need to spend more time outdoors and this is a great way to do that and have fun.
Billy 11/12/2018 10:26 AM	I have played before and it's a nice, fun activity.

Optional question (49 responses, 3 skipped)

Bald Hill Park - Proposed Disc Golf Facility : Survey Report for 15 October 2018 to 12 November 2018

Q3 If a disc golf course is introduced at Bald Hill Park would you, your family or friends use it?



Question options

- Yes
- No

Optional question (52 responses, 0 skipped)

Bald Hill Park - Proposed Disc Golf Facility : Survey Report for 15 October 2018 to 12 November 2018

Q4 Why? Why not?

Darren Stace-Smith 10/17/2018 07:08 PM	I would use it multiple times a week
Jeff Brunsting 10/17/2018 09:47 PM	I am an avid disc Golfer and my nearest course is 40 minutes away. Bald Hill is only 10 minutes.
Krayz 10/18/2018 08:14 AM	It will be a safe and monitored space to be able to play the sport.
J.Crow 10/18/2018 08:39 AM	Yes I would regularly use it and my children would usually join me.
Carol Heath 10/18/2018 09:04 AM	Absolutely. Love the Bald Hill park and another Melbourne course would enrich our opportunity to grow this amazing sport. Friends who live in that area would love it too as they would have a course a little closer to home.
Brian Holden 10/18/2018 09:12 AM	My 3 and 5 year olds love playing
Paul Arden 10/18/2018 09:46 AM	Having participated in the trial events in the park, as a Disc Golfer I find the location and suggest course great to play and like to have the option of more variety in the courses I play. It will also allow me to introduce more friends who live closer to the area to the sport.
DavidHeath 10/18/2018 10:03 AM	as I wrote previously, it's great to attempt a variety of courses to improve our skills.
Pat U 10/18/2018 10:18 AM	I have a range of family and friends that already play at a similar course in Doncaster it would be fantastic to have one that is closer proximity to the southern suburbs of Melbourne
Kshick 10/18/2018 10:26 AM	Good exercise
Ben Ayres 10/18/2018 11:06 AM	It's close to home and has other good facilities
Tarron 10/18/2018 01:06 PM	I think there should be a DG course in every suburb, we need our younger Generation outside and off netflix! And DG is cheap to play and great for all ages.
Oscar 10/18/2018 01:12 PM	I've played disc golf for a few years and love travelling to different courses
Jay Ros 10/19/2018 07:08 AM	Such a beautiful park. I live just 30 mins away and will possibly be moving to Benteigh East in the next couple of years.
Juha 10/19/2018 04:48 PM	It very well located.
Kaia	Something to do locally

Bald Hill Park - Proposed Disc Golf Facility : Survey Report for 15 October 2018 to 12 November 2018

10/19/2018 06:25 PM

Alastair S

It is close to where i live

10/20/2018 06:12 PM

Chris E

Yes I would travel to the area for casual play and events. Due to limited Disc Golf facilities in Victoria they are a great drawcard to the area

10/21/2018 10:18 PM

RBH

Its a hazardous activity to be playing a public park/space where people walk on a daily basis and could be injured by a flying object that they probably would not see flying towards them, irrespective of whether Council sets the targets away from the dedicated walking paths - Frisbees used by lay persons have the real potential to fly off course and hit the public. If this is implemented then I hope council has a very high Public Insurance policy. new and family friendly activity that is social and community building

10/23/2018 09:44 PM

honey ant

10/24/2018 12:03 PM

Jenko17

I love disc golf

10/24/2018 07:02 PM

Margaret Irvine

Prefer trees birds friends and dogs. Please put them in a larger more open area.

10/26/2018 12:33 PM

Charlie

There are not many facilities in Melbourne, so it would be great to have in the City of Kingston.

10/26/2018 01:08 PM

James123

We love frisbee golf. We got hooked on it in Queenstown, NZ

10/28/2018 01:35 PM

Paul Barbagallo

Definitely, already played two events their and look forward to many more.

10/29/2018 12:27 PM

Margaret

Because I do not want to see this kind of activity spoiling the peace and tranquility of this beautiful parkland.

10/31/2018 04:07 PM

Jennifer Pemberton

We love to play frisbee based activities. School groups could also utilise these facilities to promote access to recreational activities within the community.

11/02/2018 10:30 AM

Jarrath

I play a bit with friends.

11/08/2018 05:41 PM

Toosje

We have a sister in law that lives near the course

11/08/2018 05:51 PM

tahlia

It's local and a sport that we can all try.

11/08/2018 06:00 PM

Taz

myself and my Kids love Disc golf! mainly for the fact that anyone can play at any skill level at any time of the day and its FREE!

11/08/2018 06:03 PM

Tbo

Definitely. There's very few courses in Victoria at the moment so i can easily see some regular trips up with my dog for a throw

11/08/2018 06:58 PM

Bald Hill Park - Proposed Disc Golf Facility : Survey Report for 15 October 2018 to 12 November 2018

RyanBudge 11/08/2018 07:00 PM	I play disc golf in Geelong and would love to have another Melbourne course to play when I am up there visiting.
Mattellis 11/08/2018 07:41 PM	Regularly travel to Melbourne and always make time to play disc golf at Victoria's other courses
Kaspar 11/08/2018 07:44 PM	Because we enjoy playing
Gizmo 11/08/2018 09:46 PM	Fun for lots of ages, new skill for the kids to learn, safe for all ages
AmySS 11/08/2018 10:07 PM	It's a fun and social activity for all ages and I have my own discs!
blairu 11/09/2018 06:47 AM	disc golf is great to play with family or friends - more courses the better!
Janet Abbott 11/09/2018 10:38 AM	Our kids LOVE disc golf
Lou van Beveren 11/09/2018 02:13 PM	Same answer as q.1
Mat Rowan 11/10/2018 08:56 PM	Fun!

Optional question (41 responses, 11 skipped)

Bald Hill Park - Proposed Disc Golf Facility : Survey Report for 15 October 2018 to 12 November 2018

Q5 How could Council encourage and/or support your participation in disc golf?

Darren Stace-Smith

10/17/2018 07:08 PM

Make discs available to rent at libraries or community centres in the area.
Promote the sport via community inclusion events with local competitive disc golf players

Jeff Brunsting

10/17/2018 09:47 PM

It might be an idea to have rental discs at one of the local libraries as they do in Doncaster.

Krayz

10/18/2018 08:14 AM

Run competitions to get people together.

J.Crow

10/18/2018 08:39 AM

By installing the proposed course at Bald Hill in consultation with local representatives from disc golf sporting bodies, to ensure that the course integrates well with the existing park usage.

Carol Heath

10/18/2018 09:04 AM

Simply by having a permanent course. It has done wonders for our local community in Doncaster. So many families are out on the course every day.

Brian Holden

10/18/2018 09:12 AM

Make access to discs at Library and have good equipment

Paul Arden

10/18/2018 09:46 AM

Installing the course is the necessary first step. Once in the ground ensuring that it is promoted well in council publication and at council events would be helpful. Joint events with the Melbourne Disc Golf Club would also be a good way to encourage participation. In Doncaster the council has placed Disc Golf discs at the library which can be loaned out to players. This could work well at Bald Hill as well so players can try the sport without waiting for an organised event or discs to arrive. Clarinda Library for example is walking distance from the course.

DavidHeath

10/18/2018 10:03 AM

give us a permanent course!

Pat U

10/18/2018 10:18 AM

To supply and install baskets, signage and teepads to ensure the most fun/safety at the hopefully new course here.

Kshick

10/18/2018 10:26 AM

If it does go ahead it should be properly managed.

Tarron

10/18/2018 01:06 PM

Council could run some DG clinics using Melbourne DiscGolf club to get the word out and encourage people to give it a go.

Oscar

10/18/2018 01:12 PM

High quality signage. A local place to rent or buy cheap discs.

Jonno C

10/18/2018 05:02 PM

By installing and maintaining quality facilities.

SK2

10/18/2018 05:46 PM

Advertising. Maybe raise awareness of the course by eg taking school excursions there

Bald Hill Park - Proposed Disc Golf Facility : Survey Report for 15 October 2018 to 12 November 2018

<p>Jay Ros 10/19/2018 07:08 AM</p>	Get involved! The sport is growing exponentially elsewhere in the world because it is so available to people of all ages, abilities, etc. Potential for social events, clubs, etc.
<p>Juha 10/19/2018 04:48 PM</p>	Events or advertisement of the sport.
<p>Kaia 10/19/2018 06:25 PM</p>	Open day with Free frisbee giveaways
<p>jrc 10/19/2018 09:05 PM</p>	i like the idea..sold
<p>Alastair S 10/20/2018 06:12 PM</p>	Have them installed along with rules on how to play and throw.
<p>Chris E 10/21/2018 10:18 PM</p>	Work with Melbourne Disc Golf Club to design and Install a permanent course and be active with the club in developing leagues and events for the new course.
<p>RBH 10/23/2018 09:44 PM</p>	I would NOT participate - I am however I walker at Bald Hill Park and am very aware of the number of people who use the park to walk. I would have no hesitation suing the council for any injury if I was hit by a flying frisbee - I have kept a record of my objection to this proposal on the grounds of Public safety.
<p>honey ant 10/24/2018 12:03 PM</p>	maintaining the course to high standard. Supporting the user groups in first 12-24 months with public awareness and programs that get all ages and abilities to use course. kids and older people have similar levels and can play together as intergenerational activity.
<p>Daniel Sizer 10/24/2018 02:12 PM</p>	Build more courses and encourage local schools to participate in playing.
<p>Jenko17 10/24/2018 07:02 PM</p>	Build more courses
<p>Fordcam 10/26/2018 12:19 PM</p>	Putting more courses in
<p>Margaret Irvine 10/26/2018 12:33 PM</p>	Never
<p>Charlie 10/26/2018 01:08 PM</p>	Publicise on social media
<p>James123 10/28/2018 01:35 PM</p>	More courses in the community, competition days, food trucks, live music like a festival vibe
<p>Paul Barbagallo 10/29/2018 12:27 PM</p>	Approve course installation, promote events and support a club. Promote the course through all council networks, including schools etc. Have information available at council sites i.e. leisure centres, libraries...
<p>Margaret 10/31/2018 04:07 PM</p>	They won't.

Bald Hill Park - Proposed Disc Golf Facility : Survey Report for 15 October 2018 to 12 November 2018

Jennifer Pemberton 11/02/2018 10:30 AM	By putting the course into Bald Hill Park
Jarrath 11/08/2018 05:41 PM	Install the course! If you build it, they will come. Nothing promotes disc golf better than a permanent course. Visuals speak 1000 words.
Toosje 11/08/2018 05:51 PM	By installing baskets in park lands.
tahlia 11/08/2018 06:00 PM	Just advertise if it gets completed so we know it's available to come and try.
Rhye 11/08/2018 06:01 PM	Help with advertising or hold Council events At the venue
Taz 11/08/2018 06:03 PM	Join with Melbourne Disc golf club and run some Clinics and Learning sessions for the Community
Chris Finn 11/08/2018 06:56 PM	Run events at the course.
Tbo 11/08/2018 06:58 PM	Contribution towards signage, basket and teepad installation is much appreciated. Working with the Melbourne Disc Golf Club to grow the sport and promote getting outdoors and active is a good idea
RyanBudge 11/08/2018 07:00 PM	By installing an awesome course!
Cassie Sweetten 11/08/2018 07:34 PM	Hold come try days for the local community
Mattellis 11/08/2018 07:41 PM	Promotion of course and the game itself through council website.
Matt Snell 11/08/2018 08:58 PM	Availability of hire discs
Gizmo 11/08/2018 09:46 PM	Advertising, making events/community clinics
AmySS 11/08/2018 10:07 PM	Provide disc loans via a local library
blairu 11/09/2018 06:47 AM	no encouragement needed. I'm completely on board.
Janet Abbott 11/09/2018 10:38 AM	Provide the facilities and we'll be there
Lou van Beveren 11/09/2018 02:13 PM	I'll just visit and watch others, then have a go
Mat Rowan 11/10/2018 08:56 PM	Providing facilities

Bald Hill Park - Proposed Disc Golf Facility : Survey Report for 15 October 2018 to 12 November 2018

Billy

Provide an easy and low cost way to rent Discs.

11/12/2018 10:26 AM

Optional question (49 responses, 3 skipped)

Bald Hill Park - Proposed Disc Golf Facility : Survey Report for 15 October 2018 to 12 November 2018

Q6 Do you have any other comments you would like to make about Bald Hill Park?

- Jeff Brunsting
10/17/2018 09:47 PM
Its a fantastic park which will be ideal for disc golf.
- J.Crow
10/18/2018 08:39 AM
An ideal location for a disc golf course. Sure to generate lots of interest from locals as they discover the sport.
- Carol Heath
10/18/2018 09:04 AM
Simply that it is a great park and a perfect location for a disc golf course.
- Paul Arden
10/18/2018 09:46 AM
It's a great park and I have really enjoyed the trial events that have been played there. Disc Golf will make a fantastic addition and definitely draw me into the area more often.
- DavidHeath
10/18/2018 10:03 AM
talk to Manningham Council about the success of the course at Ruffey!
- Kshick
10/18/2018 10:26 AM
More people could mean more rubbish. Would be good to have regular clean ups.
- Tarron
10/18/2018 01:06 PM
DG is a sport that anyone at any skill level can participate in and putting in a course costs a fraction of what a tennis court soccer pitch or play ground does and it encourages friendly competition and fitness for all ages. Bald hill definitely needs this course installed.
- Jay Ros
10/19/2018 07:08 AM
All positive. Its beautiful, clean, feels safe, open and well maintained, good facilities, friendly locals.
- Kaia
10/19/2018 06:25 PM
Great space That could do with some improvements
- jrc
10/19/2018 09:05 PM
yes. although i dont mind having dogs of leash in leashed area. the proposal to have more people around unleashed dogs with more commotion will create a dangerous mixture. i see many big dogs off leash and having more kids and parents around concerns me. i have kids and a dog myself and am always cautious of big dogs of leash there. i would want more signs and enforcement of leash area if the disc golf will be placed in a leashed area...
- Dimi
10/20/2018 11:28 AM
I would be more inclined to want more of these services provided in our parks if our rates werent so high. Our rates keep going up and are very expensive. I do not want this as an added cost onto our rates because rate payers will end up paying for this under parks and maintenance. It is ridiculous and whilst i agree maintaining these will be needed. I do not think that they need to be at an added expense to the rate payer.
- Alastair S
10/20/2018 06:12 PM
Patrol the park as part of the park is a off leash dog area and many owners then choose to not clean up after their dogs.
- Chris E
10/21/2018 10:18 PM
Disc Golf is a perfect addition to Bald Hill Park and makes particular good use of the parks underutilized. It will draw Disc Golfers to the area, as well as introducing the sport to many new people in the area, while encouraging

Bald Hill Park - Proposed Disc Golf Facility : Survey Report for 15 October 2018 to 12 November 2018

<p>RBH 10/23/2018 09:44 PM</p>	<p>outdoor activity and healthy lifestyle for Kingston City residents.</p> <p>Bald Hill Park is a delightful natural environment of which is enjoyed by many people in Clarinda to walk and look at bird life and an open space for young people to run . It is not a Frisbee park and as such should not be used for such a potential dangerous activity. Disc Gold facilities could be provided at a specifically made enclosed park for such an activity and as there are many rubbish tips being redeveloped within Kingston perhaps this activity would be best suited to an enclosed purpose built area fenced off area in one of these reclaimed Rubbish tips.</p>
<p>Jenko17 10/24/2018 07:02 PM</p>	<p>Build it</p>
<p>Fordcam 10/26/2018 12:19 PM</p>	<p>Great venue for a disc golf course</p>
<p>Margaret Irvine 10/26/2018 12:33 PM</p>	<p>As an older disabled dog owner walked here daily for 15 years but NOT ENOUGH SEATS. I often cannot walk all the way round without sitting. Please do this before you introduce more hazards. Love this peaceful park.</p>
<p>James123 10/28/2018 01:35 PM</p>	<p>When will it be open? As we want to play.</p>
<p>Paul Barbagallo 10/29/2018 12:27 PM</p>	<p>Having just played an event over the weekend, I found that local park users were very open and interested in the activity. It was a really nice experience.</p>
<p>Margaret 10/31/2018 04:07 PM</p>	<p>Stop making changes that effect this lovely peaceful park. Leave it alone. I, and my children planted trees there when the park first opened.</p>
<p>Jarrath 11/08/2018 05:41 PM</p>	<p>Fantastic park!</p>
<p>tahlia 11/08/2018 06:00 PM</p>	<p>no thanks</p>
<p>Taz 11/08/2018 06:03 PM</p>	<p>Bald hill is a fantastic venue for Disc Golf, I recently Played the Kingston City Classic and it was amazing! the community came out in force to support the sport and the proposed course, I spoke to several locals who were there just to watch and they loved it!</p>
<p>Chris Finn 11/08/2018 06:56 PM</p>	<p>Would give me a reason to visit</p>
<p>Tbo 11/08/2018 06:58 PM</p>	<p>For a good comparison course have a look at the Barwon Valley Disc Golf course in Geelong. It's a similar park with many runners and dog walkers throughout during rounds. It's a great way to get people regularly visiting the park</p>
<p>RyanBudge 11/08/2018 07:00 PM</p>	<p>The park has great potential for disc golf and I think it would be a great success for Kingston</p>
<p>Cassie Sweetten 11/08/2018 07:34 PM</p>	<p>Bald Park is a fantastic park and I can't wait to see the development of this disc golf park if approved</p>
<p>Matt Snell</p>	<p>Make it happen</p>

Bald Hill Park - Proposed Disc Golf Facility : Survey Report for 15 October 2018 to 12 November 2018

11/08/2018 08:58 PM

Gizmo

Needs something like this, is a cheap to play sport, cheap to set up and safe sport to play

11/08/2018 09:46 PM

AmySS

Thanks for getting this far in the process - it's a really exciting project!

11/08/2018 10:07 PM

blairu

nope - hope this goes through!!

11/09/2018 06:47 AM

Janet Abbott

Yes... the off leash dog area needs more shaded seating in the agility area and water fountains (dog and human) in the "just running around" area.

11/09/2018 10:38 AM

Lou van Beveren

I need to go there to see it. I did not know it was there.

11/09/2018 02:13 PM

Mat Rowan

Great location for this.

11/10/2018 08:56 PM

Optional question (34 responses, 18 skipped)

BALD HILL DISC GOLF COURSE PROPOSAL CONSULTATION REPORT

CONSULTATION METHODS

A number of consultation methods have been implemented to give our community and key project stakeholders the opportunity to contribute to Council's decision making process regarding a disc golf course at Bald Hill Park in Clarinda.

The consultation and engagement period ran from 18 October to 9 November 2018; listed below are the methods which were used:

- Resident information bulletin, survey and Come and Try event invite distributed to approximately 1000 properties surrounding Bald Hill Park;
- On-line project information platform at yourkingstonyoursay.com.au;
- Electronic survey at yourkingstonyoursay.com.au;
- Community information session at Bald Hill Park including project information, BBQ and come and try disc golf activities;
- Facebook post;
- Park signage at four locations within Bald Hill Park;
- Kingston Moorabbin Leader article featuring a photo of Mayor Steve Staikos and Melbourne Disc Golf Club representative at Bald Hill Park, plus project proposal and come and try event information; and
- Direct contact with 10 key stakeholders i.e. neighbouring LGAs, local schools, peak sporting bodies, KCC Leisure Centres, KCC Libraries, and Clayton Bowls Club.

COMMUNITY AND STAKEHOLDER PARTICIPATION

Consultation Method	Findings/Outcomes
Your Kingston Your Say	Of the 52 responses received online, 48 respondents indicated their support for the proposed disc golf course.
Community Information Session	An on-site community information/come and try session was held at Bald Hill Park on Friday 26 October 2018 between 4 and 6pm. The Cheltenham Rotary Club provided a bbq for participants and Melbourne Disc Golf Club ran come and try activities.

	<p>A total of 51 people were recorded as attendees to the information session with 27 people participating in the come and try activities.</p>
Individual responses	<p>Individual submissions were received from 3 occupants of three residential properties.</p> <p>These submissions raised two main concerns regarding flying discs in a public space as well as conflict between dogs and discs. The risk assessment completed by JLT addressed both of these matters and measures have been taken to mitigate these risks including relocation of hole 7 from within the dog off-leash area to within the general park space, and an assessment of the risk of being hit by a flying disc as being 'low risk' – disc baskets and tees are positioned to enable clear sight lines to other park users, and it was determined through the assessment that less skilled participants are not likely to have the ability to fly a disc at great pace. Primary to any disc golf activities is the 'rule' for participants to 'yield/give way to all other park users' and signage will be used to remind participants of this at each tee, as well as alerting general park users to the presence of disc golf activities.</p>

CONSULTATION OUTCOMES

Community Information Session - Comments/Suggestions

Did you enjoy disc golf?

Yes No

Would you like a disc golf course at BALD HILL PARK?

Yes No

Please go ahead with this NOT JUST A PROPOSAL

Shelter + Shade for on lookers another couple of water fountains benches for elderly - and Not enough parking space

Comments or Suggestions? Fantastic idea for community park/pool SWANSDALE ABOUT TIME DOG POO BAGS IN THE PARK - LIMITED PARK PARKING. Rental desk with holes 5+6 Basket + Tee Fantastic!!! Sounds perfect! Can't wait!!! Awesome addition to the park! Think it will be a great addition to our community! CANT WAIT!!!

Written Submissions received via email to info@kingston.vic.gov.au

Mark Stockton

From: Janet Ford-Adams <[REDACTED]>
Sent: Wednesday, 17 October 2018 9:30 PM
To: info@kingston.vic.gov.au
Subject: Both Bald Hill Park frisbee golf, and fence required to separate dog on and off leash areas.

Hi Hannah,

Thanks for the flyer advertising the disc golf proposal at Bald Hill Park.

I'm a local resident who uses the park to walk through sometimes and sometimes our family uses the playground there too.

While I support the idea of the council delivering ways for the community to be more active, I can't say that I agree with the frisbee golf at Bald Hill Park.

From a safety perspective, what happens when a frisbee hits someone (possibly on the head or in the face) walking nearby. Will that area of the park be shut down to the local residents to use for walks, throwing balls around etc. If so, this reduces the space available for people to do other active things in the park. If not, then it is probably a safety issue.

If the frisbee golf club shut down that area of the park for their events, then the public won't be able to use that area any time they want.

What happens when the rented equipment gets stuck up one of the many trees in the park. Does the equipment then have to be paid for. People will risk climbing trees up high in order not to receive a fine for loss of equipment stuck up trees. Or people will lose their own frisbees up some of the very high trees in Bald Hill Park. (And risk their safety climbing high trees to retrieve their belongings).

What happens when the dogs that are supposed to be in the dogs off lead area come over to the on lead area without being on their leads (as countless dogs in that park do) and try to chase the frisbee and run off and destroy either a rented or an owner's frisbee, or scares someone concentrating on their frisbee golf.

Finally, the top thing I'd like to say about this, is that if something like this goes ahead, Council needs to be aware that there are many dogs in the on-lead area, who are constantly off their lead (most likely having come across from the off-lead area). More people using the park will mean more people at risk of dog attacks from the dogs off leads. In saying this, a fence needs to be installed to separate the dogs off leash area and the on leash area, (completely enclosing the off-leash area), regardless of whether the frisbee course is developed or not. Too many dogs are off lead as they don't know that the gravel path is the border and the owners unfortunately don't care enough to obey the rules. The gravel path being the border really doesn't separate one area of the park from the other.

So I'd say, that unless Council wants to constantly monitor the dogs being off lead and issue fines to the owners, (Park is especially busy around 5.00 pm and 9.00am daily with dogs off leads), and unless a fence is built to completely enclose the dogs off leash area, then I wouldn't consider it to be safe for the public to be playing their frisbee golf, and aside from the dogs, the fact that the general public walking/playing in the frisbee golf area at the same time as frisbee golfers playing also concerns me safety wise. And if the frisbee golf area becomes blocked off to people to generally walk and play, then a large amount of open space is lost to the public's general use.

I'd really love to see a fence in place and more signage about where the on-leash area is and starts, and the expectations of all dog owners who use the park.

Thanks for the chance to provide feedback.

Regards

Janet
[REDACTED]

11

[REDACTED]

2

[REDACTED]

Mark Stockton

From: Roz Hamilton [redacted]
Sent: Tuesday, 23 October 2018 9:56 PM
To: info@kingston.vic.gov.au
Subject: Attention - Ms Hannah Croughan, Leisure Planner, City of Kingston - Objections to Disc Golf facilities at Bald Hill Park
Attachments: Objection for Disc Gold at Bald Hill Park 23Oct 2018 - Kingston City Council.pdf

Dear City of Kingston

Please find attached PDF file of a copy of my Objection and reasons for same, as entered on the online platform on 23 October 2018.

I wish to ensure that my objections are duly noted and do not go astray via any online Platform.

Please acknowledge receipt.

With thanks

Mrs R Hamilton

[REDACTED]

https://www.yourkingstonyoursay.com.au/bald-hill-park-clarinda-disc-golf-facilities/survey_tools/bald-hill-park-clarinda-disc-golf-facilities 

Bald Hill Park - Proposed Disc Golf Facility

Council is keen to hear community views on the proposed Disc Golf Course at Bald Hill Park in Clarinda.

Question 1:

1. Do you support the introduction of a disc golf course at Bald Hill Park?

*

==

Yes No

Why? Why not?

Flying frisbees or any other flying object in a public space that is used for walking is a dangerous idea that Kingston council would be foolish to implement.

In the event of flying objects hitting people including vulnerable people like the disabled or the elderly or small children is just foolhardy and will leave the Kingston council open to being sued for personal and physical damage. People who are not experts at this form of sport and that would be the majority of users would be potentially a hazard to any person walking in Bald hill Park. I would object most strenuously to such an activity as any form of Disc golf facility at Bald Hill Park for safety reasons.

Question 2:

If a disc golf course is introduced at Bald Hill Park would you, your family or friends use it?

Yes

No

Why? Why not?

Its a hazardous activity to be playing a public park/space where people walk on a daily basis and could be injured by a flying object that they probably would not see flying towards them, irrespective of whether Council sets the targets away from the dedicated walking paths - Frisbees used by lay persons have the real potential to fly off course and hit the public. If this is implemented then I hope council has a very high Public Insurance policy.

Question 3:**How could Council encourage and/or support your participation in disc golf?**

I would not participate - I am however I walker at Bald Hill Park and am very aware of the number of people who use the park to walk. I would have no hesitation suing the council for any injury if I was hit by a flying frisbee - I have kept a record of my objection to this proposal on the grounds of Public safety.

Question 4:**Do you have any other comments you would like to make about Bald Hill Park?**

Bald Hill Park is a delightful natural environment of which is enjoyed by many people in Clarinda to walk and look at bird life and an open space for young people to run . It is not a Frisbee park and as such should not be used for such a potential dangerous activity. Disc Gold facilities could be provided at a specifically made enclosed park for such an activity and as there are many rubbish tips being redeveloped within Kingston perhaps this activity would be best suited to an enclosed purpose built area fenced off area in one of these reclaimed Rubbish tips.

Submitted online 23October 2018 / sent as PDF file on 23 October 2019 to:

Hannah Croughan

Leisure Planner

City of Kingston

Phone 1300 653 356

Email info@kingston.vic.gov.au

Submitted by a Resident and Rate payer of Kingston Council – Mrs R B Hamilton

Mark Stockton

From: Norris Miles [REDACTED]
Sent: Thursday, 1 November 2018 9:19 AM
To: Corporate Information
Subject: bald hill park (Disc throwing)

Dear sirs /madam you ask for feedback re the installation of disc golf unit at Bald hill park--
You say you have been approached by a number of local activists for this - the first question is (WHERE) in Bald hill park are these people suggesting we should pay to install an orange coloured box , amongst otherwise natural surroundings ?

(1) Why do these people not simply (probably once every 6 months) set up their own "washing " basket at the appropriate distance and use their own Frisbees as you rightly suggest they could, and may well do anyway ???

(2) Why should our beautiful and natural Bald hill Park have more of its natural greenery and beauty subjected to a coloured box as a disc dispenser , located , if at all , anywhere other than just beside the current children's playground ?

(3) We all know that any mechanical apparatus standing alone will undoubtedly be graffitied and or damaged at best , / hardly ever used ! and the tax payer will again pick up the tab--

Who and how many are these (number) of individuals who want this ? are they locals who will use it regularly , ? are they locals who will service and clean it regularly ? of course not !!!

I for one, would ask that Council puts this time and money into deterring males who upset us locals with their late night frequenting of the toilets in the park, for private uses other than their intended use -/ this would be the money put to good use -

Regards
Norris Miles [REDACTED]

[REDACTED]

[REDACTED]



DISC GOLF RISK ASSESSMENT BALD HILL PARK CITY OF KINGSTON

Version FINAL

May 2018



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1. OVERVIEW

1.1 BACKGROUND

City of Kingston [Council] seeks risk management advice in relation to a proposal to establish a Disc Golf Course within Bald Hill Park, a public-access open space reserve owned and managed by Council.



Figure 1.1 Bald Hill Park, Clarinda with key points of interest noted

Bald Hill Park currently consists of general grassed open space areas, including a mixture of mostly flat areas as well as landscaped hills and mounds. Small native copses of mostly small to medium trees are scattered throughout the reserve.

A significant, mostly flat section of the reserve on the western side has been designated as an "Off-leash Dog" area, and a children's playground, picnic/BBQ and toilet facilities are located in the north-eastern



corner. Two car parking areas are available for use, the main, sealed car park located adjacent to the playground and a secondary gravel car park located in the south-eastern corner. A mixture of sealed (concrete) and gravel meandering pathways are provided throughout the park.

Disc Golf is a sport in which players throw a disc (or Frisbee) at a target, and is played using rules similar to golf. Similar to golf, it is often played on a course of 9 or 18 holes, with each hole consisting of a tee (flat concrete pad), a set route through open space, and a target or hole. In the case of disc golf the target or hole is a round ring with a chain net attached designed to catch the flying discs.

As part of establishing the disc golf course, Council has engaged the services of an external contractor Disc Golf Park to firstly assess a selection of Council-owned reserves in terms of their suitability (based on a range of factors) for Disc Golf. From this assessment, Bald Hill was selected as the most suitable. Disc Golf were then engaged to plan the layout of the course within Bald Hill Park. This effectively entails identifying the most ideal locations and arrangement for the number of holes – in this case a 9 hole course – taking into consideration a number of factors such as length, location of trees and other infrastructure, etc.

Once the course has been planned out and approved, concrete tees are to be constructed, targets are installed, and associated rule/safety/route finding signage installed where needed.

It is proposed that the course be open for public use at all times, as well as possibly being utilised for various events and competition.

Council have requested a site inspection (of the existing park) and risk assessment in relation to the proposal, with particular consideration given to the following:

- Potential risk of disc golf as an activity causing personal injuries to other users or the park, and or damage to nearby private property such as parked cars.
- Potential risk of disc golf as an activity negatively impacting the experience of existing park users (other than directly causing injuries), in particular children/families using the playground and dog owners using the off-leash area.

Council's legal duty of care and obligations in regards to the operation of such public facilities and the management of such risks is specified in the Wrongs Act, 1958.

In short, Council as an *Occupier* owe a Duty of Care to ensure that their land and the activities conducted therein do not pose an unreasonable risk to other persons and their property. Where a risk is identified (that is unreasonable and not obvious), Council must be able to demonstrate they have taken all reasonable steps to mitigate said risk.

Effectively this requires Council to assess, analyse and evaluate risks when they are identified and/or are reported to them, then implement mitigation measures that are *reasonable* in the given circumstances. If Council does so, and documents their processes (i.e. their justification for their decisions), they should then be in a good position to defend any claims of negligence for incidents/injuries/property damage.



1.2 OBJECTIVES AND SCOPE

The objectives of the engagement were to:

- Liaise with relevant Council staff to confirm the scope and objectives of the review;
- Review any relevant information (to be provided by Council) relating to the proposal including the proposed layout of the course/holes, existing risk/suitability assessments, etc.
- Conduct a site inspection to confirm existing park conditions/activities and key public facilities / locations within the park and/or within proximity of the park, as well as any existing relevant risk mitigation strategies such as signage, tree plantings that may act as barriers, etc.
- Identify strategies which may address the risk exposures related to the proposal, in particular any recommended re-alignment of holes, relocating tee positions, and the like to minimise Council's risk exposure where possible;
- Consider and make reference to:
 - Likely flight trajectory of flying discs;
 - Existing activities conducted within the park;
 - Liability exposure of Council; and
- Delivery of a risk management report

1.3 APPROACH AND METHODOLOGY

On April 16th, 2018, a site assessment of Bald Hill Park was conducted by Daniel Brooks, Senior Consultant in conjunction with Council Leisure Planner Hannah Croughan and a representative of Disc Golf Australia.

The report that follows is based on findings from the site assessment, JLT's extensive knowledge and experience of public liability risk and incidents/claims, and the review of the following documentation/information provided by Council:

- *3_Bald Hill Park_Disc Golf Concept Design Layout.pdf* – Satellite/aerial image of the park on which is indicated the proposed layout of the course, including the alignment of each hole, location of two tee pads for each hole (Beginner and Expert), and the location of each respective net. (Refer Fig. 3.1). Prepared by *DiscGolfPark* (installation contractors/consultants) on behalf of Council.
- *18 18005 Site Suitability Analysis – Disc Golf – City of Kingston* – The results of a quantitative analysis of the suitability of two potential Council reserves/parks including Bald Hill Park for Disc Golf, conducted by *DiscGolfPark* on behalf of Council as a way of selecting the most appropriate location to establish a Disc Golf Course. This analysis involves the rating of various factors, such as Amenities and Parking, Accessibility, Environment/Layout, as well as Safety and Risk, and resulted in Bald Hill Park being assessed as the most suitable of the two locations.
- *18 11178 Disc Golf Site Suitability Assessment Criteria – City of Kingston* – guidance in relation to the methodology used to complete the abovementioned *Suitability Analysis*.
- *Chichester Park Disc Golf Park_Infoboard* – an example of standard signage installed at or nearby the first tee, which includes a diagram of the overall course layout, hole numbers/alignments/par scores, rules of the game, etc.
- *TeeSign* – example of typical signage placed at or nearby the tee pad(s) of each hole, which includes an overview diagram of the respective hole and any points of interest/hazards nearby (e.g. out of bounds areas), the par score for the hole and the hole distance.



2. KEY FINDINGS & RECOMMENDATIONS

2.1 Summary of Recommendations

In terms of potential liability exposures (Claims against Council for personal injury and/or property damage as a result of Council's negligence) the overall risk posed by the proposal to establish a Disc Golf Course within Bald Hill Park is seen as very low.

Using a typical risk management assessment matrix (example matrix used is provided in Appendix 1 – Council may wish to re-evaluate using their own matrix), the following risks were identified and assessed accordingly:

Risk	Likelihood	Consequence	Risk Rating
Errant disc causing personal injury and/or property damage through direct impact.	Rare	Minor	Low
Conflict arising between Disc Golf players and existing park users.	Unlikely	Major	Moderate
User of park sustaining personal injury as result of unsafe disc golf infrastructure/equipment.	Possible	Moderate	Moderate
Council / Park reputational damage associated with introduction of disc golf course.	Unlikely	Minor	Low



Falling tree / limb impacting with park user – particularly disc golf players.	Unlikely	Major	Moderate
--	----------	-------	-----------------

A high level summary of the recommendations are provided below:


Risk	Rating	Recommended Mitigation Measures
Errant disc causing personal injury and/or property damage through direct impact.	Low	<p>Council should consider ensuring that appropriate messages in relation to <i>sharing the park with other users</i> and <i>having consideration for the safety and enjoyment of other park users</i> are raised through park signage and/or other means such as information on Council’s website.</p> <p>Council should also consider what measures may be implemented to monitor how the new users (Disc Golf Players) interact with the various existing user groups, with particular consideration for dog owners and users of the playground/picnic space.</p> <p>Such measures may include conducting inspections during busy periods to directly observe activities, consultation with regular users of the park (including all key user types), recording an investigating any reported incidents, etc.</p>
Conflict arising between Disc Golf players and existing park users.	Moderate	<p>Council should give consideration to changing the layout of the course such that the location of Hole 7 is moved outside the “Off-leash dog” area.</p> <p>Council should also consider installing signage in relation to parental supervision of children at or nearby the existing children’s playground. Refer figure 3.10 for further information.</p> <p>Given these two locations are the only ones of note on the course where such conflict is seen as even slightly possible, taking these actions will reduce the likelihood from Unlikely to Rare.</p>




Risk	Rating	Recommended Mitigation Measures
User of park sustaining personal injury as result of unsafe disc golf infrastructure/equipment.	Moderate	<p>Council should consider conducting an inspection of the course following construction; to both assess the risk of trip/falls, as well as assess the visibility of infrastructure/equipment during the day and at night. It may be necessary to take measures such as changing the colour of some equipment, or possibly attaching reflective tape to improve visibility at night.</p> <p>Council should consider ensuring all such infrastructure / equipment installed as part of the proposal is integrated into Council's existing inspection and maintenance systems (i.e. proactive inspections of Councils assets), to ensure they do not present a risk to park users. Refer to Figure 3.9 for further information.</p>
Council / Park reputational damage associated with introduction of disc golf course.	Low	<p>Council should ensure that reasonable measures are taken to ensure adequate consultation with existing users and nearby residents in relation to the proposal.</p> <p>This should ideally take place not only prior to establishing the course, but also following to gain feedback on how it is being used.</p>
Falling tree / limb impacting with park user – particularly disc golf players.	Moderate	<p>Council should consider conducting a thorough review of their inspection and maintenance of the trees located within this reserve. Refer Fig. 3.8 for further information.</p>

3. DETAILED OBSERVATIONS AND FINDINGS


3.1 SITE INSPECTION


3.1 Site Inspection	
Observations / Findings	Recommendations
 <p>Figure 3.1 The proposed course layout as prepared by <i>DiscGolfPark</i> on behalf of Council.</p>	<p>Hole 1 – Refer Fig. 3.3 below.</p> <p>Hole 7 – While it is noted that the risk is likely to be quite low, Council should consider relocating Hole 7 to a location <i>outside</i> the “Off-leash Dog” area.</p> <p>This will help lower the likelihood for conflict between the different user groups, such as a dog chasing / taking a players disc, leading to confrontation with owners. Refer Fig. 3.4 below for further details.</p> <p>Hole 8 – Refer Fig. 3.5 below.</p> <p>Car parking – Council should consider monitoring current usage of the existing car parking in and around the reserve, to determine the likely impact additional users the Disc Golf Course may have. While this is not seen as a liability exposure by any means, lack of parking facilities may lead to loss of Council reputation as well as the attractiveness and success of the park itself.</p>



3.1 Site Inspection	
Observations / Findings	Recommendations
<p>Overall the layout of the holes appears to be well thought out, holes being strategically placed to avoid existing pathways and flight directions appearing to be mostly away from any existing infrastructure. The following high level observations are made of this layout design:</p> <ul style="list-style-type: none"> • Hole 1 – is located in what appears to be quite close proximity to the children’s playground, however trees appear to offer some physical separation. • Hole 7 – unlike all other holes, is located within the “Off-leash dog” area. This may increase the potential for conflict between the dog-owners and disc golf participants, and while it is noted that one of the rules of the game is to “always give other park users right of way”, much relies on the behaviour of animals which can be unpredictable. • Hole 8 – the alignment of this hole appears to bring players within close proximity to Bunney Street, on the southern boundary of the park. • Car Parking for the reserve (other than kerb-side) appears somewhat limited. This may not present a problem during most periods of the day, however it is likely the primary car park can be quite busy during certain periods just from existing users of the playground. 	

3.1 Site Inspection	
Observations / Findings	Recommendations
 <p data-bbox="365 997 869 1023">Figure 3.2 Examples of the discs used in the sport.</p> <p data-bbox="365 1061 1182 1145">Council particularly requested advice regarding the risk of such discs causing personal injury/property damage through impacting with other park users or their property (e.g. parked cars).</p> <p data-bbox="365 1184 1182 1240">After testing the discs, and noting the reasonably soft rubbery compound they are made from, their light weight, the reasonably slow speed at which they fly</p>	<p data-bbox="1193 432 1926 547">Council should consider ensuring that appropriate messages in relation to <i>sharing the park with other users and having consideration for the safety and enjoyment of other park users</i> are raised through park signage and/or other means such as information on Council’s website.</p> <p data-bbox="1193 579 1926 694">Council should also consider what measures may be implemented to monitor how the new users (Disc Golf Players) interact with the various existing user groups, with particular consideration for dog owners and users of the playground/picnic space.</p> <p data-bbox="1193 726 1926 841">Such measures may include conducting inspections during busy periods to directly observe activities, consultation with regular users of the park (including all key user types), recording an investigating any reported incidents, etc.</p>


3.1 Site Inspection	
Observations / Findings	Recommendations
<p>and their typical flight characteristics, it is the observation of JLT that they are <i>extremely</i> unlikely to cause any injury or property damage of note.</p> <p>It is seen as <i>more</i> likely (albeit still very unlikely) that a player themselves may sustain an injury through attempting to retrieve a disc from a dog, or perhaps recklessly chasing a disc onto an adjacent road.</p>	
 <p>Figure 3.3 Hole 1 looking from the approx. location of the Expert tee with the intended flight trajectory indicated by the red arrow, and approx. location of the net/target indicated by the yellow circle.</p> <p>While the children’s playground is located just to the right of this image, given the direction of the target, the trees lining the edge of the playground and the other risk factors noted in Fig. 3.2 (e.g. speed and flight characteristics of discs,</p>	<p>Refer observations and recommendations provided for Fig. # regarding parental supervision signage at the playground.</p> <p>Otherwise, Council should consider using existing systems and processes to monitor the use of the park, behaviour of users and any ongoing incidents.</p>

3.1 Site Inspection	
Observations / Findings	Recommendations
<p>etc.) the risk of a child suffering an injury resulting from an errant throw is assessed as Low.</p>	
 <p>Figure 3.4 Hole 7 looking from the approx. location of the Expert tee with the intended flight trajectory indicated by the red arrow. The proposed location of the net/target is just on the other side of the copse of trees.</p> <p>Given this hole is located within the “Off-leash Dog” area, there is an increased potential for conflict to arise between the different user groups (relative to other holes).</p> <p>As mentioned in Fig. 3.2, the risk of thrown discs directly causing injury or property damage by impacting with people and/or property is seen as very low. Rather, conflict is more likely to arise through the sometimes unpredictable</p>	<p>Council should consider reviewing the currently proposed course layout, such that this hole can be relocated elsewhere within the reserve outside the “Off-leash dog” area.</p> <p>This will help lower the likelihood for conflict between the different user groups.</p>


3.1 Site Inspection	
Observations / Findings	Recommendations
<p>nature of animals when they are not on-leash (e.g. dogs chasing discs, confrontation between dogs/dog owners and disc golf players, etc.).</p>	
<div style="display: flex; flex-direction: column; align-items: center;">   </div> <p>Figure 3.5 Hole 8: Top image looking from the approx. location of the Expert tee with the intended flight trajectory indicated by the red arrow. Bottom image</p>	<p>Council may consider establishing some low level tree/shrub plantings along the south (road/footpath) side of this hole, even if only at its most narrow pint (indicated by green circles).</p> <p>This will lower the likelihood of errant throws landing onto the road/footpath, by creating an obstacle which will encourage disc golf players to aim their throws closer to the trees on the left, increasing their margin of error to towards the road/footpath.</p> <p>Otherwise, Council should consider using existing systems and processes to monitor the use of the park, behaviour of users and any ongoing incidents.</p>

3.1 Site Inspection	
Observations / Findings	Recommendations
<p>looking from approx. half way along the length of the hole, with the approx. location of the net/target indicated by the yellow circle.</p> <p>While the footpath is located reasonably close to this hole, creating some potential for errant discs to make their way onto the footpath or road, given the both are elevated above the level of the park, this is still assessed as quite unlikely with very low consequences in any event.</p>	
 	<p>Council should consider reviewing existing usage of the car parking spaces, particularly during busier periods, and assess the likely impact of the addition of a new user group to the reserve.</p> <p>While not considered a safety/liability risk exposure, ensuring the reserve remains suitable and accessible to existing users as well as additional users associated with the proposal will be an important factor in the success of the overall proposal and the reserve as a whole.</p>

3.1 Site Inspection	
Observations / Findings	Recommendations
<p>Figure 3.6 Main sealed car parking area (top) located nearby children’s playground, and the secondary car parking area (bottom) located in the South East corner of the park.</p> <p>It is noted that while the provision of car parking appeared more than sufficient at the time of the site inspection, it is likely there are specific time periods (e.g. mornings of weekends and public/school holidays) when the playground is particularly busy. The addition of more reserve users – particularly if their popular periods coincide with already busy periods – may have a negative impact on Council reputation and also the accessibility/popularity of the reserve as a whole.</p>	
 <p>Figure 3.7 In addition to the two car parking areas, there are a number of pedestrian access points to the reserve/park.</p> <p>Signage located at these entry points was found to be very good given the existing park activities and/or risk factors; with clearly legible information and appropriate use of Australian Standard symbols for warnings/prohibitions where</p>	<p>Council should consider upgrading / modifying existing signage at key access points to include the following:</p> <ul style="list-style-type: none"> Warning (using the Australian Standard AS 1319:1994 generic Warning symbol – Sign No. 441) that disc golf is played in the park and that visitors should aware of flying discs. <p>Eg.</p> <div data-bbox="1229 1018 1877 1142" style="border: 1px solid black; padding: 5px;">  <p>Warning – Disc Golf played in this park. Be aware of flying discs.</p> </div> <ul style="list-style-type: none"> Appropriate and simple messages in relation to <i>sharing the park with other users and having consideration for the safety and enjoyment of other park users.</i>

3.1 Site Inspection	
Observations / Findings	Recommendations
<p>necessary and no evidence of damage/vandalism. It is noted however, that none of these sign include any content/information in relation to disc golf of course (e.g. warning of flying objects, message regarding sharing the park with others, etc.).</p>	
 <p>Figure 3.8 At various locations through the park there were a number of fallen trees and limbs.</p>	<p>Council should consider conducting a thorough review of their inspection and maintenance of the trees located within this reserve.</p> <p>It is not known if these trees are subject to a proactive inspection program, however it may be reasonable, given the potential for increased usage of the park and the fact that the disc golf course actively encourages park users into these otherwise rarely used areas of the park, that the level of respective Council inspection and maintenance activities be increased.</p>

3.1 Site Inspection	
Observations / Findings	Recommendations
<p>While most of these were relatively small, and may well have been the result of a storm event that occurring the weekend prior to the site inspection, there is some potential for park users to suffer significant injuries if they were to be hit by such.</p>	
<div data-bbox="360 584 1115 963">  </div> <p>Figure 3.9 Examples of the Disc Golf nets/targets (left) and tee pads (right) to be installed to facilitate the activity/sport.</p> <p>While relatively little constructed infrastructure is required to be introduced to the park to facilitate the sport of disc golf, anything that is installed may present a risk, either through creating a risk of personal injury (e.g. trip and fall) or simply a financial risk to Council due to the need to maintenance and repair costs (particularly if vandalism is prevalent in the local area).</p>	<p>Council should consider conducting an inspection of the course once all has been installed, to both assess the risk of trip/falls, but also to assess the visibility during the day and at night. It may be necessary to take measures such as changing the colour of some equipment, or possibly attaching reflective tape to improve visibility at night.</p> <p>Council should consider ensuring that any infrastructure / equipment installed as part of the proposal is integrated into Council’s existing inspection and maintenance systems (i.e. proactive inspections of Councils assets), to ensure they do not present a risk to park users.</p> <p>Such infrastructure/equipment should include:</p> <ul style="list-style-type: none"> • Concrete tee pads and associated covering (e.g. artificial turf), • Disc Golf nets/targets, • Associated signage, including the main sign at the start of the course, individual hole signs, as well as information/warning signs located park access points. • General inspection of the grassed surface of each hole to identify any significant (yet not obvious) trip/fall hazards.

3.1 Site Inspection	
Observations / Findings	Recommendations
<p>Specifications for the tee pads have been provided by Council and if constructed and maintained accordingly they should present little if any risk.</p> <p>One key factor will be the visibility of both the tee pads and the nets/targets, both during the day (ensuring they effectively contrast with background/surrounding surface) and at night.</p>	
 <p>Figure 3.10 The children’s playground located in the north eastern corner of the park.</p> <p>While not a <i>requirement</i> under legislation by any means, it is noted that there does not appear to be any signage near this playground in relation to the importance of parental supervision children.</p>	<p>Council should consider installing signage at or nearby this playground highlighting the importance of effective parental supervision of children.</p> <p>The following is an example of signage developed in partnership with a number of local Councils as well playground industry specialists. While not supported by Australian Standards (or similar) yet, it is the intention of the respective standards committee that this will integrated into the respective standards at some stage in the future.</p> <div data-bbox="1182 917 1715 1059" style="border: 1px solid black; padding: 10px; margin: 10px 0;">  <p>Please supervise your children</p> </div>

APPENDIX 1 – RISK ASSESSMENT MATRIX

LIKELIHOOD - determine the likelihood of an event occurring		CONSEQUENCE - assess the most likely consequence of the event (not absolute worst case)		DETERMINE - the level of risk 1-6 = LOW 7-16 = MED 17-22 = HIGH 22-25= EXTREME					
					Insignificant	Minor	Moderate	Major	Catastrophic
RARE	The event may occur in exceptional circumstances, no record of previous occurrence.	INSIGNIFICANT	No injury or damage < \$10k.						
UNLIKELY	The event could occur at some time, has rarely happened in the past.	MINOR	First aid attention, minor financial loss \$10-20k.	ALMOST CERTAIN	M	S	S	H	H
POSSIBLE	The event will probably occur at some time, has happened occasionally in the past.	MODERATE	Injury requiring medical treatment, moderate financial loss \$50-200k.	LIKELY	M	M	S	S	H
LIKELY	The event could occur in most circumstances, has occurred at regular intervals in the past.	MAJOR	Serious long term injury, major financial loss \$200-500k.	POSSIBLE	L	M	M	S	S
ALMOST CERTAIN	The event is expected to occur in all circumstances.	CATASTROPHIC	Major injury, disablement or death. Significant financial loss >\$500k.	UNLIKELY	L	L	M	M	S
				RARE	L	L	L	M	M

**Disclaimer**

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Ordinary Meeting of Council

10 December 2018

Agenda Item No: 10.3

PARKING MANAGEMENT POLICY - ONLINE PERMIT PROCESS

Contact Officer: Ross Gregory, Manager Traffic and Transport

Purpose of Report

To seek Council's endorsement of a variation to the Parking Management Policy to facilitate online payments and introduce a 12-month renewal from date of issue.

Disclosure of Officer / Contractor Direct or Indirect Interest

No Council officer/s and/or Contractor/s who have provided advice in relation to this report have declared a Conflict of Interest regarding the matter under consideration.

OFFICER RECOMMENDATION

That Council:

1. Endorse an amendment to the Parking Management Policy under *Section 4.1 Residential Parking Permits: Duration of Residential Parking Permits* to read: "Residential parking permits are valid for 12 months and must be renewed annually, including any applicable fees";
2. Endorse an amendment to the Parking Management Policy under *Section 4.1 Residential Parking Permits: Fees* to read: "Where fees for a residential parking permit apply, the cost will be in accordance with the *User Fees and Charges* in Council's annual budget to cover the administration cost of issuing the permit and managing the resident parking permit schemes";
3. Note the transition from a paper-based to online application and processing of forms; and
4. Receive a further report in 2019 following a full review of the Parking Management Policy to be undertaken following the completion of the Kingston Parking Study to inform the strategic direction of the Policy.

1. Executive Summary

As part of the Customer First Transformation the Traffic and Transport department is proposing to move the purchase of residential parking permits online. This is one of the higher volume transactions provided by Council that is not currently available on line. This change creates a simpler transaction for the customer and reduces manual handling by internal staff: especially in the finance and corporate records departments, who will no longer need to manually process payments and paper forms.

The move to online forms means that customers can immediately check their eligibility for residential parking permits. This can be difficult to interpret due to differing rules for eligibility based on the number of dwellings on a block and the age of the property. As a result many customers ring through to Council's customer care team to confirm their status prior to making an application.

A further enhancement is proposed to the durations for renewal. The Policy presently requires all renewals to be processed annually on 1 September, and it proposed is to amend the Policy to provide the full 12-month benefit for the first year of their permit. This will also balance the renewals throughout the year, reducing pressure on internal systems that arise with the single renewal date.

Finally, the online system is not presently capable of processing the free permits for customers without adding significant cost and delay to the roll-out of the online system for further system development. Those customers of single dwellings that choose to receive a single free permit, as allowed under the Parking Management Policy, will still need to complete the paper-based form. The paper-based form will still also be available for those that wish to use it for paid transactions whilst the development work is conducted to move this transaction online.

Additionally, for consistency with other fees and charges, it is proposed to reword the fees section of the Policy to reference the User Fees and Charges section of Council's Annual Budget for the residential parking permit fee.

It is recommended that Council endorse these changes to the Parking Management Policy.

2. Background

The Parking Management Policy was adopted by Council in April 2016 and introduced the payment of fees for residential parking permits for the first time in Kingston. At the time of adoption Council did not offer online payments for any transaction, so the wording reflected the manual systems in place.

A full review of the Parking Management Policy is due by 31 December 2019. Officers are currently consulting the community on the Kingston Parking Study. Feedback from the community through this Study will inform the full review of the Parking Management Policy, this is an interim review to make one small change to the Policy to facilitate online transactions.

3. Discussion

3.1 Council Plan Alignment

Goal 4 - Our free-moving safe, prosperous and dynamic city
Direction 4.4 - Integrated accessible transport and free moving city

Residential Parking Permits are valued by the community that live in areas of high on-street car parking demand. The move to online payments for permits reduces the hassle for residents in making this transaction.

3.2 Consultation/Internal Review

The Customer and Community Relations and Information Services and Strategy departments have been consulted through this process. The Business Applications team within Information Services and Strategy have conducted the development work for the online process and have developed a robust system for processing these payments. The Customer Care team has provided feedback on the process from the customer's perspective.

3.3 Operation and Strategic Issues

3.3.1 Customer enhancements offered by online permits

A mechanism has been built into the process to automatically assess the eligibility of a customer based on their residential address. Presently customers must rely on interpreting the Parking Management Policy rules to determine whether they are eligible, and as a result many customers ring through to Council's customer care team to confirm their status. This eligibility test allows us to provide 'conditional' approval on the spot and issue a temporary permit for the customer which is valid for 14 days until the permanent pass is posted to them.

This is in addition to convenience of being able to apply for a permit through an online form rather than having to print out and complete a paper form then scan and send back.

3.3.2 Organisational savings from online permits

The current process for residential parking permits requires three internal departments, Traffic and Transport, Finance and Corporate Records, and an external contractor, Tenix Solutions, to process an application.

An application made online will remove Finance and Corporate Records' involvement in the process. Additionally, Council is in negotiation with Tenix Solutions to take back the processing of permits, which provide a saving to Council.

3.3.3 Limitations of payment systems

The current online system is limited to being able to process payments only and requires additional development in order to be able offer a single free permit to eligible customers. Officers have been advised that it will cost approximately \$15,000 and take several months for this feature to be built into the system.

It is proposed to conduct this development work in parallel with the launch of the online system for paid permits so as not to delay the roll out of paid version, which accounts for the majority of transactions. In the meantime, a simple web-form will be provided for those customers wishing to obtain a free permit. This form will still require manual processing by staff but will be straightforward from a customer perspective and reduce the confusion that some customers experience on whether there are eligible for a free permit.

3.3.4 Changing wording for fees to reference User Fees and Charges

The Policy currently states: "*Where fees for a residential parking permit apply, the cost will be \$20 per annum to cover the administration cost of issuing the permit and managing the resident parking permit schemes.*" It is usual for a Policy to refer to the "User Fees and Charges" section of the Annual Budget when stating fees for Council services.

Accordingly, it is proposed to reword this section of the Policy to read: *“Where fees for a residential parking permit apply, the cost will be in accordance with the User Fees and Charges in Council’s annual budget to cover the administration cost of issuing the permit and managing the resident parking permit schemes”*

4. Conclusion

4.1 Environmental Implications

Nil

4.2 Social Implications

This change enhances the process for customers to obtain residential car parking permits by offering an online payment option. Additionally, customers receive the full value of their permit in the first year.

4.3 Resource Implications

This change reduces workload for the Finance and Corporate Records teams who will not be required to process online permits. There is expected to be fewer calls through to Customer Care, as eligibility information will be available directly to customers. Additionally, permits can be managed internally, removing the need for permits to be processed by Tenix. This will be negotiated out of Council’s contract with Tenix, which is worth approximately \$20,000 per annum.

4.4 Legal / Risk Implications

Customers currently provide credit card details on the paper form, which is then either emailed or posted to Council. This presents a risk for their details to be intercepted. The move to an online system provides a secure portal for processing of payments, reducing exposure for customers.

Appendices

Appendix 1 - City of Kingston Parking Management Policy 2016 (Ref 16/48808)



Author/s: Ross Gregory, Manager Traffic and Transport
Reviewed and Approved By: Daniel Freer, General Manager City Assets and Environment

10.3

PARKING MANAGEMENT POLICY - ONLINE PERMIT PROCESS

1	City of Kingston Parking Management Policy 2016	367
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Parking Management Policy 2016

APPROVAL	Council / CEO adoption date
VERSION NO:	Version 4
TRIM REF:	
REVIEW	31/12/2019
RESPONSIBLE EXECUTIVE	General Manager City Assets and Environment
POLICY OWNER	Manager Traffic and Transport

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1. Purpose of the Policy

Purpose

The purpose of this document is to provide Kingston City Council with a framework to manage parking across the municipality for the benefit of the whole community and road users. It provides guidance for the introduction of new parking restrictions or changes to existing parking for road safety reasons, where parking is in high demand or where traffic congestion has increased

This document sets out the approach, tools, principles and procedures that Council will follow in order to manage parking and its Parking Permit Scheme to ensure consistent, equitable and transparent outcomes.

Parking is a growing issue for the community and requires a whole-of-community response. Everyone can play a positive part by:

- parking within your own property to minimise on-street parking
- walking children to and from school
- considering walking, cycling or using public transport for short neighbourhood trips
- avoiding parking recreational vehicles, such as boats and caravans, on public streets
- catching a bus to the train station, rather than parking your car in local streets surrounding train stations.

Objectives

The objectives of this document are to:

- provide a framework to manage parking to satisfy the needs of the community
- promote a safe, accessible and sustainable road environment for all users
- protect residential amenity while providing equitable access to on-street parking, access for vehicles (including emergency services), cyclists and pedestrians
- facilitate access to commercial activities and other community facilities while balancing the needs of residents
- ensure parking improves community safety, amenity and economic activity.

2. Definitions

Area of interest	Road link or collection of roads with the same parking issue; a section of road 150m or greater in length; or a section of road with the same existing parking restrictions.
Abutting Property	A property on the same side of the road as the parking spaces. This does not include properties on the opposite side of the road.
Mixed-use	A property that has more than one use, such as retail on the ground floor and residential on the upper floors, including 'shop-top' living
Multi-dwelling property	A property with 3 or more dwellings and includes subdivided sites.
Occupation rate	This is the percentage of parking spaces that are occupied in the area of interest, as an average for the area.
Parking permit	A parking permit issued by the City of Kingston.
Parking turnover rate	The actual parking usage over the theoretical parking availability, in an area of interest.
Residential property	A property that is solely for residential purposes, excluding multi-dwelling properties.
Response rate	The number of properties who responded, as a percentage of the total number of properties consulted.

3. Parking Management Policy

Policy Statement

As competition for parking spaces in public areas increases due to changes in population density, visitor profiles and the average number of vehicles per household, Council will balance the competing requirements of residential amenity, road safety, road usage and economic prosperity through the implementation of managed parking restrictions using approved guidelines.

Council will enforce parking restrictions and permits through the Kingston Local Law.

Scope

This policy applies to all Council roads, public parks in Kingston and private parking areas where agreements exist between Kingston City Council and the property owner. To group the parking needs of similar areas, the policy refers to specific parking needs in the following precincts:

- residential areas
- commercial areas including major and minor activity centres
- foreshore areas
- school and safety areas

- industrial areas
- narrow roads and laneways.

This document outlines the parking restrictions to be used and any exemptions.

The types of Parking Permits issued are listed in section 4: Parking Permits.

4. Parking Permits

A range of permits have been established to meet and balance the different needs of residents, visitors and businesses in Kingston. Parking permit types include:

- Residential Parking Permit
- Disabled Parking Permit (Blue and Green types)
- Trader Parking Permit
- Foreshore Parking Permit
- Car Share Permit.
- Work Zone Permit

Typically, a permit allows the permit holder's vehicle to remain parked for longer than the displayed time restrictions or to park in designated spaces. The application and the design requirements for each permit type are described in the Parking Management Guidelines.

The rules under which each permit type is issued, and the conditions associated with the continued use of the permit, are described separately in the Parking Permit Conditions

4.1. Residential Parking Permits

Residential permits are available to residents of the City of Kingston. Residential permits exempt the permit holder from time-based parking restrictions or allow them to park in residential permit zones. Permits allow residents greater opportunity to park near their property, but do not guarantee a parking space nearby.

A Residential Parking Permit Area can be established by the Traffic and Transport Team in consultation with Local Laws. These areas may cover a street, part of a street or a number of streets and any permit issued will be valid for all streets or locations in the specified area.

Duration of Residential Parking Permits

Residential parking permits are valid for 12 months and renewed on 1 September annually.

Number permits available

Housing Type (Dwellings per lot)	Permits		
	Free	Fee applies	Total
Single Dwelling	1	1	2
Dual Dwellings*	0	1	1
Three or more Dwellings	0	0	0

*Addresses with 2 dwellings per lot granted planning advertisement after 28 July 2015 will not be eligible for a parking permit.

Eligibility

Residential Parking Permits will be made available under the following conditions:

- A large number of existing properties do not have access to off-street parking, for example a series of terrace houses built prior to statutory planning requirements for off-street parking, in this case an exception may be granted from the Manager, Traffic and Transport.
- Shop-top apartments within residential areas will be subject to the same eligibility requirements as all residential dwellings, shop-top apartments in commercial or activities centre zones are not eligible for parking permits.

Residential Parking Permits will not apply to some time-based restricted spaces:

- Residential Parking Permits will not apply to spaces with parking restrictions sign-posted 1P or less.
- Properties granted planning advertisement after 28 July 2015 exists incorporating a net increase in total dwellings in the following zones:
 - Activity Centre Zone
 - Comprehensive Development Zone
 - Commercial 1 Zone
 - Commercial 2 Zone
 - Mixed Use Zone
 - Residential Growth Zone
 - General Residential Zone Schedules 1 and 2

These restrictions ensure that all future occupants in new developments in Kingston cannot park on time-based on-street car parking. The need for on-site parking is assessed through the Kingston Planning Scheme and is expected to be provided on-site for these developments.

Council acknowledges it is unsuitable to implement the new parking policy measures and guidelines in areas where residents already have parking permits. As such existing permit holders will be permitted to continue to hold valid permits until such a time as the resident vacates the property. All new applications will be subject to Kingston Parking Management Policy and Parking Guidelines.

Fees

Where fees for a residential parking permit apply, the cost will be \$20 per annum to cover the administration cost of issuing the permit and managing the resident parking permit schemes.

4.2. Parking for People with Disabilities

People with disabilities have access to two types of parking permits – Blue Permits and Green Permits.

Blue Permit holders may use the wider accessible spaces. When providing these wider parking spaces for people with disabilities, a number of issues must be considered including Australian Standard design requirements, ramp/footpath access to the parking space and that the driver may be the person with the disability (rather than the passenger). It is therefore generally easier to meet all requirements for such spaces in off-street parking situations.

Where on-street angle parking is available, providing safe parking spaces for people with disabilities will generally be possible. Where on-street parallel parking is the only parking available, it is generally not safe to provide a parking space for people with disabilities, as the driver (who may be disabled) will be getting out of the vehicle in the path of approaching traffic. Parallel on-street parking spaces for people with disabilities will only be provided on roads wide enough to ensure the full width wider parking space, clear of any traffic or cycling running lanes and where ramps can be provided.

4.3. Foreshore Parking Permit

Foreshore Parking Permits are issued to all ratepayers within Kingston enabling them to park free of charge in designated foreshore areas.

Foreshore parking permits are renewed every three (3) years from 1 September 2015. Parking restrictions will be implemented in car parks and streets near the foreshore. In these cases the parking restrictions will:

- focus on a balance between residential, local business and visitor parking
- be analysed, designed and implemented using the methodology for technical assessment and community engagement detailed in the Parking Management Guidelines.

4.4. Share Car Permit

The car share scheme is designed to reduce on-street car parking pressures by encouraging people to use shared vehicles to meet their car travel needs. Car sharing supports sustainable travel because the booking process encourages people to consider travel alternatives before choosing to drive.

Duration Share Car Permit

Car share bays are provided to the operator for a period 2 years, with the option to renew.

Eligibility

Applies to Council land and Council-managed roads:

- Car share bays are subject to approval by Council.
- Council will work with the operator to identify appropriate sites and consult with nearby land owners and occupants prior to installing on-street car share bays. Council will consider submissions received and reserve the right to decline the application.
- Car share operators must bear the costs associated with the installation of a car share facility which must include the supply and installation of two generic car share parking signs and line marking of the bay (as a minimum).
- Operators are responsible for submitting planning permit applications for all promotional and information signs associated with the car share bay (if required).
- Preference will be given to operators who use fuel-efficient, low-emission vehicles to further promote sustainable transport options.
- Operators will be required to provide Council with an annual report outlining car share use patterns within the municipality.

Fees

Car share operators will be charged a fee for exclusive access to a car share bay for a 12-month period to be determined by Council's property services team.

Application

Applications must be made in writing and received by the Property Services department in line with Council's existing Commercial use of Council Land Policy.

The application must include the following:

- completed Commercial Use of Council Land Application Form
- a detailed site specific plan
- public liability insurance with cover of no less than \$10 million, indemnifying council

- detailed operational overview of Car Share scheme and operator.

Cancellation

If, due to unforeseen circumstances, Council needs to relocate or suspend a car share bay – Council will work with the relevant car share operator to find a suitable, alternative location. The costs of new signage and line marking will be covered by Council in such circumstances.

4.5. Trader Parking Permits

Parking restrictions will normally be implemented in commercial areas or activity centres. In these cases the parking restrictions will:

- focus on a balance of high-turnover, short-term time restrictions limited to normal business hours and longer-term parking for staff
- focus on reducing road congestion
- focus on supporting local business prosperity
- be analysed, designed and implemented using the methodology for technical assessment and community engagement detailed in the Parking Management Guidelines.

Trader Parking Permits may be made available under the following conditions:

- there is a demand from local businesses as determined by parking studies and the Economic Development Department; taking into account existing on-site provisions
- suitable parking sites are available within the commercial zone.

4.6. Work Zone Permit

A work zone permit is intended to permit the occupation of on-street car parking spaces, where appropriate, for the exclusive use by entities undertaking construction or works within an adjacent site.

Duration

As per application, this will be assessed on a case-by-case basis.

Eligibility

As per application, this will be assessed on a case-by-case basis with regards to existing street parking provisions and operational need.

Fees

An occupation rate will be charged as per the Fees and Charges schedule adopted in Council's Annual Budget.

Application

A Work Zone Permit application will be made as part of a traffic management plan for construction purposes and assessed by the Transport and Traffic Department.

5. Decision Guidelines

There are no exemptions to this Policy except where provided under the Road Safety Rules 2009.

6. Policy owner

The position responsible for writing and/or reviewing the policy and ensuring it is kept up-to-date with any legislative or operational changes is the Manager, Traffic and Transport. This position can be contacted for assistance and information about this Policy.

7. Delegation Authority

Delegations under the following Acts and Regulations that apply to this Policy:

- *Local Government Act 1989*

8. Responsible Executive

The General Manager, City Assets and Environment has responsibility for this policy.

9. Review

This Policy is to be reviewed by 31 December 2019.

10. Related Documents

This policy refers to the following State legislation and local laws:

- Local Government Act 1989*
- Road Safety (Traffic Management) Regulations 2005
- Road Safety Road Rules 2009
- Community Local Laws 2015

This policy refers to the following internal plans and strategies:

- Living Kingston 2035
- One Vision Council Plan 2013 – 2017
- Parking Management Guidelines 2014

- d) Parking Permit Conditions 2014
- e) Commercial Use of Council Land Policy 2014

11. Parking Policy Framework

The purpose of the parking policy framework is to describe the processes which Kingston City Council manages parking across the municipality for the benefit of the whole community and road users.

It provides guidance for the introduction of new parking restrictions or the modification of existing conditions where a road safety hazard has developed, where parking spaces are in high demand or where there has been an increase in traffic congestion.

11.1. Background

The Kingston municipality has a number of issues related to parking. In some cases this is caused by the general population change in Melbourne which sees increased housing density and a general increase in the number of cars present on local roads. In other areas parking issues have been long-standing, for example where the roads are narrow or near the beach where the seasonal demand is high.

In a number of areas in Kingston, the demand for parking is already greater than the supply of parking spaces.

This framework is designed to:

- demonstrate a commitment and process to managing parking to best satisfy the needs of the community
- promote a safe, accessible and sustainable road environment for all users
- protect residential amenity while providing equitable access to on-street parking, access for vehicles (including emergency services), cyclists and pedestrians
- facilitate access to commercial and activity centres and other community facilities while balancing the needs of residents
- ensure parking infrastructure design, construction and maintenance improves community safety, amenity and increased economic activity.

12. Parking Management Principles and Procedures

Parking management and restrictions are to be designed to provide the best and highest use of the parking resources within the municipality.

The framework seeks to provide guidance on the relative priority of user groups within each parking zone.

12.1. User Priority

Each parking zone has a unique set of users with competing interests. In order to plan for the best and highest use of the limited parking supply we must prioritise the user as shown in the tables below. Priority is shown across three categories of A through to C, where A is deemed to be the most important user set and C is least important.

These user priorities help inform traffic studies to determine the most appropriate parking restrictions for the area.

Residential Parking

Residential parking restrictions will be implemented through time-based parking restrictions suitable for the adjacent high-demand use. The parking changes will be implemented following the community engagement model described in this document in part 12.4. In these cases the parking restrictions will focus on:

- the needs of the residents as the principal priority
- improving road safety around these locations, as pedestrian activity is likely to be higher
- reducing traffic congestion at peak usage periods.

Residential Parking Permits will usually be made available under the following conditions:

- time restrictions have been implemented in the street or area, however, Residential Parking Permits will not apply to spaces with parking restrictions of 1 hour and less – such as 5-minute parking spaces near schools – which are designed as safe pick-up and drop-off points for students
- there is a shortage of on-street parking for residential properties because either:
 - a. a large number of residential properties do not have access to off-street parking and/or
 - b. there are significantly more residential properties than available parking spaces
- residential properties are within 400 metres of a commercial area or a railway station
- Resident Parking Permits do not permit parking within off-street public car parks, in these areas all visitors must obey parking restrictions.

Residential Areas		
A	B	C
Residents	Traders	Loading Zones
Bus and Taxi stops	Commuters	Short Term Parking
	Foreshore	School
	Disabled	

Commercial Areas and Activity Centres

Kingston City Council is committed to supporting our many local businesses and the efficient and effective application of parking management can contribute to the prosperity of our Activity Centres and small shopping strips. In these cases the parking management will focus on:

- time limited restrictions such as 1/2P, 1P or 2P should be used within an Activity Centre or be considered within 200m of a commercial area and will generally need only apply during business operating hours.
- to increase parking opportunities for customers and visitors, the occupancy rate should not exceed 95%. The balance between short and medium-term parking restrictions should be reviewed when this limit is reached.

Activity / Commercial Areas		
A	B	C
Disabled	Traders	Residents
Short-term Parking	Commuters	Schools
Bus and Taxi stops	Foreshore	
	Loading Zones	

Streets near Railway Stations

Where appropriate, Council should encourage local residents to walk, cycle or use public transport in their journey to railway stations. However, it is still important to provide some parking for commuters, while maintaining residential amenity.

- All residential streets within 400m distance of a railway station should be considered in each parking review.
- 2P or 4P, 9 AM – 6 PM, Mon – Fri restrictions should be used along one side of the residential street only.
- A Residential Parking Permit scheme can be implemented for selected streets or the area within the study zone.
- Where the railway line is the abutting property, parking is to be made available to encourage public transport usage or for commuter parking. Exceptions will be where there may be conflict such as with a cycling lane, where parking would significantly delay traffic in an otherwise uncongested environment or other safety issues.
- Where the railway line is on the opposite side of the road, parking should be primarily for the use of the abutting properties. Any under-used parking should be made available for commuters.

Rail Stations		
A	B	C
Disabled	Short-term Parking	Residents
Bus and Taxi stops	Loading Zones	Foreshore
Commuters		Schools
		Traders

Foreshore Areas

Parking restrictions will be implemented in car parks and streets near the foreshore. In these cases the parking restrictions will:

- focus on a balance between residential, local business and visitor parking
- be analyzed, designed and implemented using the methodology for technical assessment and community engagement detailed in the Parking Policy Framework.

Foreshore Areas		
A	B	C
Foreshore	Trader Parking	Commuter Parking
Bus and Taxi stops	Residents	Loading Zones
Short-term Parking	Disabled Parking	Schools

Streets with Schools, Kindergartens, Hospitals

The safety of pedestrians and cyclists around schools is the prime concern when investigating parking restrictions near schools, kindergartens and child care centres.

The whole school community needs to be involved in road safety improvements including changes to parking arrangements. It is generally members of the school community who create safety issues around schools.

It is expected that all school staff parking is provided off-street or in appropriately restricted streets. The streets adjacent to the school are for pick-up and drop-off purposes only.

The special process developed by Kingston to engage the school community in a holistic review of the transport needs of the community is the Kingston Schools Audit Program.

In a similar way, care needs to be taken to minimise any impact of parked cars on safe travel for pedestrians and cyclists near medical facilities.

Schools		
A	B	C
Schools	Residents	Teachers
Bus and Taxi stops	Commuter Parking	Foreshore
Short-term Parking	Disabled Parking	Traders

12.2. Parking Study Triggers

The table below outlines the trigger points that Kingston will use to guide when parking changes are instigated. These triggers are used when assessing the differing demand

profiles and relative user priorities for parking within the area to determine the most appropriate parking change.

Current Condition	Change Triggers
No parking restrictions	<ol style="list-style-type: none"> 1. Safety issues including pedestrian, cyclist and vehicle access issues (based on VicRoads CrashStats or Victoria Police data) 2. Access and delay issues for public transport services 3. Actual occupancy of parking spaces is greater than 85% for the survey period* 4. Road configuration changes require the installation of signage 5. Required post Kingston Schools Audit Program
Time-restrictions	<ol style="list-style-type: none"> 1. Actual occupancy of parking spaces is greater than 85% for the survey period* 2. Seasonal usage requires protection of resident amenity (for example near foreshore or near sporting facility), restrictions would only apply for that time of the year when protection is needed 3. Actual parking turnover in Activity Centres or shopping strips is less than 0.8 or greater than 1.2 for the survey period
Time & use restrictions (resident parking or trader parking)	<ol style="list-style-type: none"> 1. Actual occupancy of parking spaces is greater than 85% for the survey period* 2. Seasonal usage requires protection of resident amenity (For example near foreshore or near sporting facility) 3. Actual parking turnover in Activity Centres or shopping strips is less than 0.8 or greater than 1.2 for the survey period 4. Trader requirements for allocated parking
Time restrictions with paid restrictions	As above

*Actual occupancy is measured over a 150 metre segment on both sides of the road for a continuous period of ≥ 5 hours.

It is important to note that not all triggers need to be met to instigate change. However if either the occupancy rate or the parking turnover rates are not met, then it is unlikely that new parking restrictions will be considered.

Once a street or area has been investigated and a decision made to implement change or retain the parking status quo, no additional review of the area should be undertaken for at least twelve (12) months, unless some significant and sustained change in circumstances has been identified.

12.3. Parking Study Methodology

When the Traffic & Transport Team receives or identifies an issue with the current parking arrangements the following steps need to be taken:

- a) Determine whether or not a change is warranted
 - are the current parking restrictions being complied with? If not then enforcement should be the first action undertaken.
 - who is having parking issues? Council will require the initiator of the request to provide evidence of general community support, before any investigation proceeds.
 - what are the current parking arrangements and usage patterns?
 - has a 'change trigger' been met?
- b) Determine if a change would allow the objectives to be met
 - different restrictions times and/or types
 - more parking spaces restricted.
- c) Consult the community to consult on whether proposed changes are supported (if changes are warranted).
- d) Implement the change, if the change is supported.
- e) Advise the consulted community of the outcome.

12.4. Community Consultation

It is important that the impacted community has an opportunity to provide feedback on proposed changes to parking restrictions, before any changes are introduced.

The general exception to this is where a no-stopping ban is required to ensure safety for one or more road user group, at which time the impacted community will be advised of the new ban and the reason for it.

The following steps are to be undertaken when consulting on parking restriction changes:

- To determine who to consult first requires a clear appreciation of the group that the restrictions are aimed at assisting. If, for example, residents near a commercial area have difficulty locating parking near their homes, then the proposed parking restrictions are intended to assist residents and not the commercial area. It would therefore be appropriate that only the residents are consulted on the proposed parking changes, noting that the changes will only be abutting the residential properties.
- The area to be consulted should generally include every property, on both sides of the road, with a frontage to the area where the parking restriction changes are proposed. It may also be appropriate to include the corner properties that have a side fence to the street or area. It will generally not be necessary to consider properties with a back fence to the street or area. If the road is a divided road or has service roads, then only the abutting properties need to be consulted.
- If the proposed changes will impact on more than one abutting property type, each group should be consulted separately and their feedback considered independently. An example of this is where commuters are parking near a railway station, limiting access for customers to shops and residents to homes. The businesses and residents have different parking needs and thus the consultation should be undertaken independently to ensure that the needs of each group is fully considered.
- Property occupiers will be consulted, rather than owners, as parking directly affects the occupiers and residential parking permits are only issued to occupiers.

The following points will be considered, when undertaking consultation:

- A minimum response rate of 25% will be required to authenticate the survey. If this response rate is not met, it will be assumed the changes are of little importance to the community and will be implemented as necessary
- It will be deemed not appropriate to proceed with the changes if a majority of the responders are opposed to the proposed changes.
- Where a community is relatively evenly divided on the need for the changes, consideration of additional factors may be necessary to determine whether or not to proceed. Such factors include the detailed comments provided in the community feedback, impact on neighbouring area and consistency with other similar situations within Kingston.
- In a situation where the majority of community responses do not support the parking changes but Council still wants to instigate the changes, it may be done by the authority of Council. At such a time a detailed report will be provided to the community explaining the reasons behind the decision.

12.5. Kingston Schools Audit Program

Kingston has a Schools Audit Program, which is a holistic approach to improving safety around schools. During an audit it may be determined that changes to parking restrictions are needed to change parking habits and improve safe access to and from the school for students. The program involves working in collaboration with the school community. The types of changes may be to:

- encourage more active travel to school
- encourage parking to ensure children cross fewer roads to access the school
- provide a safe drop-off and pick-up zone for children
- ensure neighbouring properties have adequate on-street parking opportunities.

Once the types of changes needed are determined through the audit program, the local community is then consulted and changes implemented in the same way as other parking studies.

13. Road Use Hierarchy and Parking Restrictions

The aim of this document is to provide a framework for an equitable balance between the parking needs of residents and other users such as customers in a shopping precinct, public transport users, school communities and staff and visitors at hospitals.

The road network within Kingston is divided into a hierarchy of roads based on use, geometry and construction standards. Parking measures must be compatible with the road's primary function. The following guidelines describe the principles and options available for each road type. Please note that during detailed analysis of the issues a number of other options may be considered for implementation. The comments below discuss the key principles which will be considered for each road type.

13.1. Narrow Roads and Lanes

Kingston has a number of narrow roads and laneways that are used to access properties or connect to other access roads. To ensure public safety, access for emergency vehicles must be provided on all roads. Therefore on some narrow roads parking can only be permitted on one side of the road, while on very narrow roads no parking can be permitted.

Local Roads	
Trafficable width	Signage Treatment
Less than 5.0m	No parking allowed on either side. Signage is not required unless compliance issues are significant. The entrance to lanes should be cleared of parking to allow a 7.7m emergency vehicle to maneuver without obstruction.
Between 5.0 & 7.0m	Parking allowed on one side only. No stopping signs may be required for one side, where compliance is proven to be an issue. Parking may be provided in a staggered formation, to discourage speeding. Parking will not be allowed in the areas required for an emergency vehicle to turn safely.
7.0m or more	Parking may be allowed on both sides of the road, including opposite driveways. Alternatively, where it is warranted, angle parking may be provided on one side of a road, with or without parking on the other side, depending on the road width.

Major & Collector Roads (includes roads with public transport services)	
Trafficable width	Signage Treatment
Less than 5.5m	No parking allowed on either side. Signage may be required, if there is the potential for non-compliance.
Between 5.5 & 7.3m	Parking allowed on one side only. No stopping signs may be required for one side, which may be provided in a staggered formation. Parking will not be allowed in the areas required for an emergency vehicle to turn safely.
Greater than 7.3m	Parking may be allowed on both sides of the road. Alternatively, where it is warranted, angle parking may be provided on one side of a road, with or without parking on the other side, depending on the road width.

13.2. Local Access Roads / Local Collector Roads

Parking restrictions are not usually required unless triggered by demand from a school, commercial area, industrial area or sporting facility etc. The exceptions which may trigger parking restrictions are:

- When demand is greater than supply due to the number of vehicles regularly parked in the street. The trigger point is greater than 85% occupation. This includes streets with a mix of single dwellings and multi-dwelling buildings.
- When a significant number of properties in the street do not have vehicle crossovers or on-site parking.
- When the geometry of the road displaces available parking, for example when the street has numerous bends or traffic calming elements which restrict available parking spaces.

A Residential Parking Permit scheme may be implemented for the street or local area.

Parking restrictions should not be implemented where it adversely changes the safety profile of the road, for example by allowing speeding.

Kingston will consider indented parking bays or approved hard standing verge parking bays in some cases if:

- the width of the nature strip is sufficient to accommodate parking and not compromise footpath safety and access
- sight lines are not compromised
- existing infrastructure and landscaping is not impacted
- it fits into the existing streetscape design
- construction is for a series of properties, not just for an individual location
- a fair proportion of the cost of installation is apportioned to the benefiting properties, in accordance with the Local Government Act.

The asset will remain under the care and maintenance of Council and will not be for the sole use of any individual property. Parking restrictions may apply to these indented parking spaces.

13.3. Main Roads

Parking bans may be necessary on main roads, to ensure suitable road capacity. This usually takes the form of:

- Clearway restrictions, which are determined by VicRoads
- No stopping restrictions on the approach and departure to a major intersection
- No stopping restrictions to ensure traffic is not impeded by parked vehicles (for example on the main carriageway in Nepean Highway and White Street).

Outside of capacity and safety needs, all the available on-street parking may be treated in the same way as for a local access or collector road.

13.4. Freeways

All freeways are under the full control of VicRoads. There is a statutory ban on parking on any freeway, with the only exception an emergency situation.

14. Parking Issues not already covered

14.1. Industrial Areas

Parking restrictions may be necessary in industrial areas. In these cases the parking restrictions will focus on:

- a balance of high-turnover, short-term time restrictions limited to normal business times and longer term parking
- reducing road congestion
- ensuring safe access to business including access by heavy vehicles
- supporting local business prosperity
- will be analyzed, designed and implemented using the methodology for technical assessment and community engagement detailed in the Parking Management Guidelines.

Access to a permit equivalent to the Trader Parking Permit may be made available under the following conditions:

- there is a demand from the local businesses as determined by parking studies and the Economic Development Department
- suitable off-street parking sites are available within the industrial area.

14.2. Loading Facilities

It is a Planning Scheme requirement for new commercial and industrial properties to provide adequate on-site loading facilities. Council does not need to provide or retain existing on-street loading facilities for any new commercial or industrial developments.

However, there are a large number of existing commercial and industrial properties that were built before this requirement. Where existing established businesses are having issues with deliveries/ collection of goods, on-street loading facilities may be installed or retained, following consultation with relevant affected properties.

In residential areas, loading and unloading on-street can be in significant conflict with pedestrian and cyclist safety. Getting enough on-road space to safely park a truck in a residential area can also be very difficult. Generally there are limited requirements for loading facilities in residential areas. However, when the property is a very large multi-unit complex, loading and unloading by trucks can occur regularly. It is therefore incumbent on all new residential developments to adequately provide for loading and unloading on-site and not to rely on valuable on-street parking.

14.3. Private Parking Areas

Currently there are a number of private parking areas where Council has an agreement with the land owner to enforce parking restrictions. It is expected that, generally, the parking provisions in these private parking areas will be consistent with this policy and the Parking Management Guidelines.

14.4. Streets designated as cycling routes

Kingston has a cycling network which consists of a mix of shared-use paths on-road lanes.

- Parking should be removed near intersections to reduce conflict points between cyclists and other vehicles.
- If designated as a high-usage cycling route, clearway/timed parking restrictions could be considered.
- Parking may be removed where the road is too narrow to accommodate parking while ensuring the safety of travel for cyclists.
- Alternative solutions may include changing lane widths or implementing indented parking.
- Reviewing parking conditions as the bicycle network expands.

14.5. Boats, Trailers & Caravans

Boats, caravans and trailers are permitted to park on local roads within Kingston subject to the following conditions:

- the boat, trailer or caravan is parked in an area subject to a Resident Parking Scheme
- the boat, trailer or caravan is not greater than 7.5m in length
- Council does not consider the boat, trailer or caravan to be abandoned.

Any vehicle found on Council land or a road and considered by an Authorised/ Delegated Officer to be abandoned, derelict or unregistered may be dealt with under the provisions of Schedule 11 of the Community Local Law.

14.6. Nature Strip Parking

Nature strip parking within Victoria is prohibited as stated in the Road Safety Road Rules 2009.

Whilst giving due regard to the road rules, Council has determined that parking on nature strips is not a high priority for enforcement. Therefore, City of Kingston's Statutory Education and Compliance Department uses the following approach to enforcing nature strip parking:

Upon receipt of a complaint about specific vehicles parked on a nature strip enforcement action may be taken if one of more of the following conditions is met:

- The vehicle is presenting an immediate danger by causing a vision hazard
- The vehicle is parked too close to an intersection (within 10m)
- The nature strip curb or channel is being damaged by the vehicle when driving on or off the nature strip
- The nature strip and/or vegetation is being damaged by the vehicle
- The vehicle is parked not facing the direction of travel
- The vehicle does not belong to the property outside where it is parked
- The vehicle is parking in a dangerous manner

If the vehicle does not meet any of the criteria set out about, no action is taken. A warning will be issued on the first occasion.

Officers do not need to take enforcement action against other vehicles parking on nature strips on the immediate areas unless a specific complaint has been received and the offending vehicle meets the required criteria.

15. Parking Related Infrastructure

All parking-related infrastructure will comply with the Road Safety (Traffic Management) Regulations 2005 and conform to the designs specified in the AUSTRROAD standards.

Ordinary Meeting of Council

10 December 2018

Agenda Item No: 10.4

OUTCOME OF DRAFT FENCING POLICY CONSULTATION

Contact Officer: Michelle Hawker, Senior Administration Officer
Angela Granter, Acting Team Leader Property Services

Purpose of Report

The purpose of this report is to advise Council on the outcome of the community consultation regarding the draft Fencing Policy and to recommend that Council adopt the revised Fencing Policy attached at appendix 1.

Disclosure of Officer / Contractor Direct or Indirect Interest

No Council officer/s and/or Contractor/s who have provided advice in relation to this report have declared a Conflict of Interest regarding the matter under consideration.

OFFICER RECOMMENDATION

That Council adopt the revised Fencing Policy.

1. Executive Summary

Council resolved at the July 2018 Ordinary Meeting to publicly advertise the draft Fencing Policy. The community was invited to provide comments on the draft Policy through public notices placed on 8 August 2018 in the Moorabbin and Mordialloc Leaders and Chelsea Mordialloc Mentone News on and also Council's *yourkingstonyoursay* website. The draft policy was available on Council's Website.

Submissions closed on 15 September 2018 and no submissions were received.

The report was originally listed for consideration at the Ordinary Meeting of 26 November 2018, however it was deferred to the Ordinary Meeting of 10 December 2018. There is one change to the report from the November meeting. In clause 4.2 the sentence '*All requests for fence repairs are to be carried out by a registered fencing contractor*' has been removed. Community feedback received (after the agenda for the 26 November was published) identified that as there is no statutory requirement for fencing works to be undertaken by a registered builder or other type of contractor, this policy requirement is unenforceable and accordingly it has been removed. Additionally recognition is made of shared fencing adjacent to open space areas that perform a pathway or connectivity function to parks/reserves.

2. Background

At the July 2018 Ordinary Meeting, Council resolved the following:

10.6 Draft Fencing Policy

RECOMMENDATION

That Council:

1. Adopt the Draft Fencing Policy 2018 (Appendix 1) for the purpose of consultation with the community, and
2. Receive a further report including consultation findings and for further consideration.

Note: Refer to page 5 of the Minutes where this item was block resolved

Council's current Fencing Policy was adopted in 1997 (see appendix 2). This policy has worked well over the past 20 years however concerns have been raised internally and externally regarding when Council will contribute to the cost of fences adjoining Council owned and managed land.

The purpose of the draft Fencing Policy is to clearly outline Council's obligations and willingness to share fencing costs with adjoining property owners. The policy will also provide consistency in the information provided to both internal and external parties.

Appendix 3 highlights the changes between the current policy and the proposed policy. In summary these changes are:

- The *Fencing Act 2014* prescribed that Councils are no longer required to contribute to fencing of properties that adjoin Council reserves however it is proposed that Council will continue to contribute 50% of cost of a standard fence up to 1.9 meters.
- Exclusions have always been included in the Policy however these have been expanded to clearly include discontinued roads, walkways and other unoccupied spaces.

The report was originally listed for consideration at the Ordinary Meeting of 26 November 2018, however it was deferred to the Ordinary Meeting of 10 December 2018. There is one change to the report from the November meeting. In clause 4.2, the sentence '*All requests for fence repairs are to be carried out by a registered fencing contractor*' has been removed. Community feedback received (after the agenda for the 26 November was published) identified that as there is no statutory requirement for fencing works to be undertaken by a registered builder or other type of contractor, this policy requirement is unenforceable and accordingly it has been removed.

Reference has also been made to fencing against open space areas that provide a connective function as linkages to open spaces such as parks or reserves.

3. Discussion

3.1 Council Plan Alignment

Goal 1 - Our well-planned, liveable city supported by infrastructure to meet future needs
Direction 1.1 - Intergenerational land use planning for a sustainable community

3.2 Consultation/Internal Review

Community consultation was via Council's *yourkingstonyoursay* website and advertising in the local newspapers for a period of 28 days. No submissions were received.

In drafting the revised Fencing Policy officers had previously engaged with internal departments. The key internal stakeholders were:

- Community Buildings Programmed Maintenance.
- Community Buildings Reactive Maintenance.
- Parks.
- Customer Care.

3.3 Operation and Strategic Issues

3.3.1 Extent of requests

Email and phone queries from the community regarding fencing responsibilities will be reduced as with the improved clarity concerning which fences Council will equally share costs. The Policy will be published on Council's website, providing accessible information which will provide ratepayers with an opportunity to seek their own advice.

3.3.2 Sector review

Officers conducted a desktop review of other Local Government Authorities' Fencing Policies. In summary the policies reflected a similar position and contents to Council's draft Fencing Policy.

3.3.3 Contribution responsibilities

The review of the existing policy revealed a need to be explicit on those circumstances where Council does not contribute to boundary fencing. Council does not contribute to the cost of fences on roads, drains and Crown Land. However where an unused road or walkway forms part of a recreation reserve, it has been officers practice to contribute to fencing costs and this practice has been clarified and confirmed in the draft Fencing Policy.

3.4 Options

3.4.1 Option 1 – Officer recommendation

Adopt the updated Fencing Policy.

3.4.2 Option 2

Do not adopt the updated Fencing Policy.

4. Conclusion

4.1 Environmental Implications

Nil

4.2 Social Implications

To assist property owners, a fact sheet will be available on Council's website.

4.3 Resource Implications

The revised policy will have a positive impact on Council's resources. The number of queries on shared fencing costs from internal and external sources should reduce due to policy accessibility and improved clarity of information.

4.4 Legal / Risk Implications

The *Fencing Act 2014* does not require Council to make contributions towards the construction and maintenance of a common boundary fence with an adjoining owner in respect of land owned by, vested in or under the care, control or management of Council for the purpose of a public park.

Under the proposed Policy, Council will continue to contribute 50% of the cost of a standard fence adjoining Council owned land including parks.

Appendices

Appendix 1 - Draft Fencing Policy 2018 (Ref 18/90243) [↓](#)

Appendix 2 - Fencing Policy 1997 (Ref 12/23790[v2]) [↓](#)

Appendix 3 - Marked up copy Draft Fencing Policy 2018 (Ref 18/586914) [↓](#)

Author/s: Michelle Hawker, Senior Administration Officer
Angela Granter, Acting Team Leader Property Services

Reviewed and Approved By: Julian Harvey, Manager Property and Arts
Daniel Freer, General Manager City Assets and Environment

10.4

OUTCOME OF DRAFT FENCING POLICY CONSULTATION

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2	Fencing Policy 1997	403
3	Marked up copy Draft Fencing Policy 2018.....	405



Fencing Policy

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1 Document Control

The electronic version of this document is the controlled version. Printed copies are considered uncontrolled. Before using a printed copy, verify that it is the current version.

RESPONSIBLE GENERAL MANAGER	General Manager City Assets and Environment
POLICY OWNER	Manager Property & Arts
APPROVED/ADOPTED BY	Council on XX XXX 2018
EFFECTIVE DATE (If different from approval date)	
REVIEW DATE	30/06/2022
CM REF AND VERSION	18/90243
VERSION HISTORY	Version 1 adopted by Council 24 November 1997 Version 2 adopted by Council

2 Purpose

The purpose of this policy is to set out clear responsibilities for the construction, maintenance or replacement of boundary fencing abutting Council owned land, which includes compliance with the *Fencing Act 1968*.

3 Scope

This policy applies to land owners where their property abuts Council owned or managed property who seek a Council contribution to the cost of construction, maintenance or repair of a boundary fence.

4 Policy Details

The *Fencing Act 1968* does not require Council to make contributions towards the construction and/or maintenance of a common boundary fence with an adjoining owner in respect of land owned by, vested in or under the care, control or management of the Council for the purpose of a public park.

Council is willing to contribute 50% of the cost for a standard fence in cases where the owner adjoins a Council owned public park.

4.1 Standard Fence

The standard fence for properties adjoining Council owned land will be a timber paling fence of no more than 1.9 metres in finished height (palings, posts and plinth). Construction standard and materials will be in accordance with the accepted industry standard.

The standard fence does not include enhancements such as trellis, fence toppings, gates or decorative timbers, this is at full cost to the adjoining owner.

4.2 Half Cost Share

If a fence adjoining a Council owned property requires replacement, Council will contribute 50% of the cost of replacement of a standard fence. Council will determine at its discretion if the fence requires replacement.

Council does not carry out any repairs or maintenance to adjoining boundary fences.

Construction of a new fence on a previously undefined boundary will require the adjoining owner to engage a qualified surveyor to prepare a boundary establishment plan at the owner's cost. Council will then contribute 50% of the cost of a standard fence.

Should there be any discrepancies with existing fence lines in regards to the correct title boundary; the owner will be requested to carry out a title re-establishment survey at their cost to define and set the boundary / fence line.

Pool fences / gates that lead onto council land are required to comply with the Australian Standards AS1926.1

4.3 Exclusions

Council does not accept responsibility for half cost fencing for the following:

- Land not in direct Council ownership or maintained under Council management.
- Crown Land and foreshore reservations.
- Road reservations.
- Right of ways, laneways, discontinued roads, walkways and other unoccupied spaces except where a walkway or road forms part of a recreation reserve.
- Buffer to a road, for example, plantation and tree reserves or other land except where the land is used as a public open space, provides connection to adjoining recreation reserve or walkway / shared path network.
- Drainage reservations.
- Bicycle path.
- Non Council owned open space.
- Gates, trellis, enhancement, painting, decorative and other features.
- Pedestrian gates.
- Pool compliance fencing.

5 Delegation Authority and Decision Guidelines

5.1 Delegations/Authorisations

Financial delegation applies to authorise reimbursement of the fencing costs.

5.2 Exemptions

The policy does not apply to emergency works where public safety is of concern.

5.3 Human Rights Charter

This policy has been reviewed against and complies with the Charter of Human Rights and Responsibilities Act 2006. Property rights are a human right under the charter which states that a person must not be deprived of his or her property other than in accordance with law.

6 Related Documents and Resources

Legislation

- *Fencing Act 1968*
- *Australian Standard AS1926.1*

7 Definitions

Fence "fence" means a structure, ditch or embankment, or hedge or similar vegetative barrier, that encloses or bounds land.

POLICY

FENCING

INTRODUCTION

A policy to acknowledge Council's obligations under the Fencing Act 1968

1 Standard Fence

- 1.1 The "Standard" fence for properties adjoining Council owned lands will be 1.67m height (5 foot 6 inches) timber paling. Construction standard and materials will be in accordance with the accepted industry standard.

2 Half Cost Share

- 2.1 Should a fence adjoining a Council property require replacement, Council in accordance with the Fencing Act 1968 will be responsible for "Half cost share" of the replacement of a "Standard" fence.
- 2.2 Should one of the adjoining owners require a fence of a higher standard, that owner shall be required to pay in addition to their half cost share the full cost difference over that of a "Standard" fence.
- 2.3 Property boundary fences at walkways directly leading to Council reserves held in freehold title

3 Exclusions

- 3.1 Council does not accept responsibility for fences adjoining and that are not under direct ownership of Council, for example:
- Foreshore Reservations
 - Road Reservations
 - Right of Ways & Laneways
 - Tree Reserves, which act as a buffer to Main Roads
 - Drainage reservations.



Fencing Policy

Changes marked up

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RESPONSIBLE GENERAL MANAGER	General Manager City Assets and Environment
POLICY OWNER	Manager Property & Arts & Leisure Services
APPROVED/ADOPTED BY	Council on 23 July XX XXXX 2018
EFFECTIVE DATE (If different from approval date)	
REVIEW DATE	30/06/2022
CM REF AND VERSION	18/90243
VERSION HISTORY	Version 1 adopted by Council 24 November 1997 Version 2 adopted by Council

2 Purpose

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The purpose of this policy is to set out clear responsibilities for the construction, maintenance or replacement of boundary fencing abutting Council owned land, which includes compliance with the *Fencing Act 2014*.

3 Scope

This policy applies to land owners where their property abuts Council owned or managed property who seek a Council contribution to the cost of construction, maintenance or repair of a boundary fence.

4 Policy Details

The *Fencing Act 2014* does not require Council to make contributions towards the construction and/or maintenance of a common boundary fence with an adjoining owner in respect of land owned by, vested in or under the care, control or management of the Council for the purpose of a public park.

Council is willing to contribute 50% of the cost for a standard fence in cases where the owner adjoins a Council owned public park.

4.1 Standard Fence

The ~~standard~~ fence for properties adjoining Council owned lands will be a ~~timber paling fence of no more than 1.9 metres in finished height (palings, posts and plinth)~~ 1.67m height (5-foot-6 inches) timber paling. Construction standard and materials will be in accordance with the accepted industry standard

The standard fence does not include enhancements such as trellis, fence toppings, gates or decorative timbers, this is at full cost to the adjoining owner.

4.2 Half Cost Share

~~Should~~ If a fence adjoining a Council owned property requires replacement, Council ~~in accordance with the Fencing Act 1968 will be responsible for "Half cost share" of the replacement of a "Standard" fence.~~ will contribute 50% of the cost of replacement of a standard fence. Council will determine at its discretion if the fence requires replacement.

~~Should one of the adjoining owners require a fence of a higher standard, that owner shall be required to pay in addition to their half cost share the full cost difference over that of a "Standard" fence.~~

~~Property boundary fences at walkways directly leading to Council reserves held in freehold title~~

Council does not carry out any repairs or maintenance to adjoining boundary fences.

Construction of a new fence on a previously undefined boundary will require the adjoining owner to engage a qualified surveyor to prepare a boundary establishment plan at the owner's cost. Council will then contribute 50% of the cost of a standard fence.

Should there be any discrepancies with existing fence lines in regards to the correct title boundary; the owner will be requested to carry out a title re-establishment survey at their cost to define and set the boundary / fence line.

Pool fences / gates that lead onto council land are required to comply with the Australian Standards AS1926.1

4.3 **Exclusions**

Council does not accept responsibility for ~~fences adjoining and that are not under direct ownership of Council, for example:~~ half cost fencing for the following:

- Land not in direct Council ownership or maintained under Council management.
- Crown Land and foreshore reservations.
- Road reservations.
- Right of ways, laneways, discontinued roads, walkways and other unoccupied spaces, except where a walkway or road forms part of a recreation reserve.
- Buffer to a Roads, for example plantation and tree reserves or other land except where the land is used as public open space, provides connection to adjoining recreation reserve or walkway / shared path network.
- Drainage reservations.
- Bicycle path.
- Non Council owned open space.
- Gates, trellis, enhancement, painting, decorative and other features.
- Pedestrian gates.
- Pool compliance fencing.

5 Delegation Authority and Decision Guidelines

5.1 Delegations/Authorisations

n/a

5.2 Exemptions

The policy does not apply to emergency works where public safety is of concern.

5.3 Human Rights Charter

This policy has been reviewed against and complies with the Charter of Human Rights and Responsibilities Act 2006. Property rights are a human right under the charter which states that a person must not be deprived of his or her property other than in accordance with law.

6 Related Documents and Resources

Legislation

- *Fencing Act 1968*
- *Australian Standard AS1926.1*

7 Definitions

Fence

"fence" means a structure, ditch or embankment, or hedge or similar vegetative barrier, that encloses or bounds land.

Ordinary Meeting of Council

10 December 2018

Agenda Item No: 10.5

SPORTS FIELDS FEASIBILITY CONSULTATION FINDINGS AND NEXT STEPS - ASPENDALE GARDENS

Contact Officer: Mark Stockton, Team Leader Sport and Recreation

Purpose of Report

To present community feedback and input in response to the three design development options to develop additional outdoor sports facilities at Kerr Crescent, Aspendale Gardens.

Disclosure of Officer / Contractor Direct or Indirect Interest

No Council officer/s and/or Contractor/s who have provided advice in relation to this report have declared a Conflict of Interest regarding the matter under consideration.

OFFICER RECOMMENDATION

That Council:

1. Notes the findings of the consultation process for proposed development of additional sports field facilities at Kerr Crescent, Aspendale Gardens;
2. Notes that the proposed site for the proposed additional sports field facilities at Kerr Crescent, Aspendale Gardens is subject to Amendment C149, which is currently under consideration by Council;
3. Receive a further report with a revised design development option that responds to the key feedback themes identified from the consultation findings and presents next steps for the development of additional outdoor sports facilities at Kerr Crescent, Aspendale Gardens; and
4. Continues to engage with stakeholders and interested local residents with the planning for the proposed development of additional sports field facilities at Kerr Crescent, Aspendale Gardens through the posting of project updates on Council's website and onsite signage.

1. Executive Summary

Council is seeking to provide additional sporting facilities to meet strong growing demand in our community. The Kingston Sport and Recreation Strategy, endorsed by Council at the 24 September 2018 Ordinary Council Meeting, identifies a clear need for additional outdoor sporting facilities.

Kerr Crescent Reserve in Aspendale Gardens has been identified as a preferred potential site as it is Council-owned land, has ample space, already has some (but minimal) sporting use at the site, and the land has low ecological value. Aspendale Gardens currently has no formal sporting facilities to serve its community.

Other sites such as Yammerbrook Reserve, Browns Reserve and land along the Mornington Peninsula Freeway extension are not owned by Council and would require costly acquisition by Council. These other sites were considered as alternatives and assessed less favourably in comparison with Kerr Crescent, which already has an existing AFL/cricket field, established in 2012/13.

Development of the site would be designed to meet the needs of a range of sports (multisport), including four soccer pitches OR two AFL ovals for the winter season and two cricket ovals for the summer season.

While the design of the site would allow for flexibility, it is proposed that the site would primarily be allocated to the Aspendale Stingray Soccer Club (ASSC) for winter use and for cricket activity during summer.

A recent six-week consultation process to seek community feedback on the proposed development of additional outdoor multisport fields and pavilion at Kerr Crescent, Aspendale Gardens has been held. In addition to seeking the level of support for the overall concept, three initial potential layouts for the site were also developed to seek community views on how the site could be developed. Further details on the range of consultation methods, feedback received, key themes and potential responses are listed in the report below.

Council received strong response to its community engagement activities. In total, 364 people took part in an online survey via www.yourkingstonyoursay.com.au; a further 19 submissions were received, the Aspendale Gardens Residents Association (AGRA) provided feedback; and a number of community members attended the drop-in information sessions (Appendix 3).

289 participants of the online survey and five submissions supported providing improved sporting facilities at Kerr Cres Reserve.

“Soccer facilities desperately needed in the area. Stingrays is a family focussed club who wants kids to get outside and play sport.”

“It should be considered an asset to the local area.”

“Think about the future, think about the kids = they need to get out of the house.”

“Sports changes lives, better facilities can increase the support to make it happen to a higher number of people”

75 participants of the online survey and 14 submissions, including a Petition with 12 signatures opposed the proposal. At the community drop-in sessions the first event attracted a number of immediate neighbouring residents who do not support the proposal, while the second event attracted a mix of feedback;

“We want more vegetation – create a park (instead of a sporting facility)”

“What is the benefit to local residents?”

“This is a great local community asset (we don’t want it to change)”

“Is there anywhere else it can go?”

While the submission from AGRA did not explicitly support or oppose the proposal, it did raise a number of concerns with the development.

The neighbouring Aspendale Gardens Primary School supported the proposal and would welcome the extra car parking facility on Kerr Crescent. The need to ensure pedestrian/cycling safety as the reserve is used as a key transport route for school children travelling to and from school was highlighted.

The site is also subject to Amendment C149, which is currently under consideration by Council. This amendment seeks the rezoning of the Council owned land to accurately reflect its existing use. Amendment C149 is subject to separate consideration by Council.

The next steps for the proposed development of additional sports field facilities at Kerr Crescent, Aspendale Gardens is to determine a revised design development option that responds to key theme raised by stakeholders and interested local residents. As part of determining a revised design development option it is important that Council continue to provide updated information to stakeholders and interested local residents

2. Background

2.1 Opportunities for Additional Sporting Fields

Council is seeking to provide additional sporting facilities to meet strong growing community demand. The Kingston Sport and Recreation Strategy, endorsed by Council at the 24 September 2018 Ordinary Council Meeting, identifies a clear need for additional outdoor sporting facilities.

During the past 2 years Council has been investigating a number of opportunities for the development of additional sporting fields within the City. These include:

- Spring Road Reserve, Dingley – unsuitable due to complication with its former use as a landfill site;
- Chadwick Reserve, Dingley – subject to further feasibility planning involving the development of the site for junior AFL/community cricket use;
- Delta Site, off Kingston Road Heatherton – Council is working with the State Government to identify the suitability of this site to accommodate sporting activities. This site is considered a longer term prospect to provide outdoor sporting facilities;
- Hawthorn Football Club site, Dingley – Council continues to engage with the Hawthorn Football Club to support the potential development of facilities on its site that can provide broader community benefits; and
- Kerr Crescent, Aspendale Gardens – Preliminary investigations support the future development of this site to include additional multi-sport facilities, potentially achieving 2 AFL/cricket ovals or four soccer fields.

As one of Council's largest sporting clubs, the Aspendale Stingrays Soccer Club (ASSC), it is in strong need for a home facility to meet the needs of its growing membership. Details of the ASSC are listed in Appendix 1.

This report presents findings from the engagement activities upon a number of proposed design development options for additional outdoor sporting facilities at Kerr Crescent, Aspendale Gardens.

2.2 Kerr Crescent, Aspendale Gardens

Kerr Crescent Reserve is Council's most recently established sports ground, developed in 2012/13 in response to a need for additional outdoor sporting fields. However, the lack of a pavilion, toilets, floodlights and car parking significantly restricts community access to and use of this site. The inclusion of built facilities to support the sports field was tested as part of its development in 2012/13 with the Aspendale Gardens community. Feedback received via consultation undertaken highlighted concerns from residents at the time (including the Aspendale Gardens Resident's Association) for any built facility such as a pavilion to accompany the sportsground. There was also opposition from residents to the planned formalised car parking being provided on site, which did not proceed.

As the site does not currently provide pavilion, toilet or car parking facilities it is only regularly used during summer by the Aspendale Cricket Club. For the 2017/18 season Council provided a temporary toilet facility, in response to requests from the Club. There are other irregular users of the reserve, particularly to accommodate preseason training requirement of local clubs.

While Aspendale Gardens has an excellent, in comparison to other Kingston suburbs, supply of recreation facilities such as walking/cycling paths, open space reserves, wetland areas, a multipurpose youth centre and distribution of playgrounds, the supply of sporting infrastructure is low.

Both the suburbs of Aspendale Gardens and adjoining Waterways have a very low supply of sporting facilities. This matter has been raised to the attention of Council by residents asking "Why doesn't Aspendale Gardens have a locally based sports club?"

To inform, guide feedback and input, three possible development options were prepared and presented for community feedback. These options are set out in the Appendix 2.

3. Discussion

3.1 Council Plan Alignment

Goal 2 - Our sustainable green environment with accessible open spaces.
Outcome 2.5 - Provide for a variety of sport and recreation opportunities across Kingston through the Sport and Leisure Strategy

3.2 Consultation/Internal Review (refer to separate Consultation report 18/592387)

Significant community consultation was undertaken regarding the project including:

- Information bulletin sent to 800+ homes in surrounding area;
- 2 community information sessions hosted by Kingston Council (Thursday evening and Saturday morning sessions);
- Kingston Council officers attended 2 Aspendale Gardens Resident Association (AGRA) Meetings to discuss the project;
- Kingston Council officers met directly with Aspendale Gardens Primary School Principal and the School President to seek their feedback on the plans;
- Further bulletin (extending the consultation period in response to AGRA request) sent to 1600 plus homes;
- A Petition with 12 signatures objecting to the project was also received';
- Online survey on www.yourkingstonyoursay.com.au completed by 364 participants; and
- 20 submissions received via mail/email.

Copies of "kingstonhaveyoursay" responses, written submissions and submission from the Aspendale Gardens Residents Association (AGRA) are set out as Appendices 3 and 4, respectively.

3.3 Operation and Strategic Issues

3.3.1 Key Feedback Themes

Concerns raised by respondents are categorised as key themes. They include parking and traffic management, pavilion facilities, safety, environmental impacts, loss of open space and rise in anti-social behaviour. These key themes are discussed below:

3.3.1.1 *Parking and Traffic Management*

Issues concerning on-site parking, off-site parking and traffic management were the most common concerns expressed by respondents.

While 246 participants (71%) in the online-survey preferred that car parking is provided largely onsite within the reserve – a key concern raised by neighbouring residents was the loss of green ‘open space’ for car parking and the impacts on local traffic, particularly in Nurten Parade.

“Nurten Parade has blind spots, additional traffic will be a nightmare and potential for serious accidents.”

A traffic study produced by the Traffix Group and set out as Appendix 4, recommends the need for 120 onsite car parks to meet current and forecast future sporting needs associated with the proposed sports fields. Preliminary advice provided by Council’s Town Planning Department also supports the need for a number of onsite car parks, as identified by the Traffix Group.

Given the size of Kerr Crescent, its proximity to the Aspendale Gardens Shopping Centre and existing on street car parking along the Aspendale Gardens Primary School, the preferred point for access, egress and principle location for car parking has been determined by the Traffix Group is from Kerr Crescent (located north west of the primary school).

Providing separate car parking off Nurten Parade, was also strongly opposed during community feedback sessions. From a traffic engineering perspective, any car parking off Nurten Parade would be best considered and used only as a secondary/overflow site i.e. game day car parking. Respondents also cite issues with parking areas split across the site.

A number of respondents also had concerns that reserve users may park in Tarongo Drive, Aspendale and walk across the reserve.

“We don’t have the street to support traffic. Plan A is absolutely out of the question, people with park in Taronga drive in Aspendale, block up the roads and walk through to Aspendale Gardens.”

3.3.1.2 *Pavilion Facilities*

Feedback received regarding the development and use of a pavilion facility are listed below:

- A centrally located pavilion works best; “I like the clubhouse in the middle (away from residents)”;
- Concerns with the use of the pavilion to host late night events; “No club function room club can hire venues elsewhere for functions”; and
- Concerns with the consumption of alcohol.; “The community will strongly oppose any liquor license application.”

- 3.3.1.3 *Safety*
Many respondents have raised the importance of the retention and enhancement of existing pathways and avoidance of conflicts between vehicles and pedestrians.

“Walking tracks must be safely maintained and no shared paths (roads and kids don’t mix)”
- 3.3.1.4 *Environmental Impacts.*
Concerns were raised by some community members regarding the environmental impact of the development.

“(This project would result in) a loss of naturalised vegetation and habitat (and) loss of feeding grounds for flocks of birdlife.”

Some other comments raised relate to preserving the flooding mitigation purpose of the site.

Advice has been sought from ecology experts Biosis regarding ecological impacts upon the site from the proposed additional outdoor sports facilities. A preliminary report from Biosis has assessed the conservation significance of the site as low (refer Appendix 5).

Whilst some local residents may view this area as a wetland, Biosis has confirmed that it has not been developed nor does it function for this purpose.

As part of any development at the site, plantings would be provided to offset vegetation removal and enhance the site.

As the distance of this site to the Edithvale/Seaford Wetlands is approximately 300m, it is not considered to have an adverse impact upon the birds or other wildlife.

Preliminary advice from Council’s hydrologist indicates that the current flood mitigation properties for the site and overall Mordialloc drain can be maintained with all options.
- 3.3.1.5 *Loss of Passive Open Space Area*
Concerns were raised about a loss of a passive open space area if the area is used for organised sport.

“There are fewer and fewer open spaces like this available for everyone to enjoy. And people do already enjoy this lovely peaceful and quiet space every day some on their own, some with their children, and others with their pets.”
- 3.3.1.6 *Rise in Anti-Social Behaviour*
Concerns were raised that the development may attract anti-social behaviour.

“The plans proposed generate a large carpark, likely to be used as a common meeting ground for late night youth gatherings like burnouts, littering, substance use, graffiti and noise pollution. This would significantly impact on the family environment created in Aspendale”

3.3.2 Other Concerns

3.3.2.1 *Potential Other Sites:*

A number of immediately abutting residents do not support the development and questioned why other sites could not be selected.

“With the green wedge all around us, it is time council started planning a larger facility like Casey Fields for our ratepayers.

Sports Lighting Impacts

Some neighbouring residents raised concerns about the impact of sports lighting on their amenity.

“Ground lighting will impact on residents”

3.3.2.2 *Proximity to residential properties*

A number of immediate neighbours to the site raised concerns regarding the proximity of the pavilion to residences.

“It’s too close to houses (particularly option A)”

3.3.2.3 *AFL Usage*

A number of respondents highlighted the need for additional sporting fields to accommodate AFL activities.

“Why hasn’t AFL even been considered? Why are cricket and soccer the only sports benefitting from the upgrade? Where is the evidence that these sports have the greatest need?”

3.3.2.4 *Potential impact on Stacey MacInnes Memorial Seat*

A memorial seat honoring local youth Stacey MacInnes was installed at the site, near the Nurten Parade entrance, a number of years ago. Stacey’s family attended a community drop-in session to request that consideration be given to ensuring the memorial seat is retained in its current position and would prefer that car parking not be located nearby.

3.3.3 Next Steps

3.3.3.1 *Zoning*

Officers are currently reviewing a number of sites (258) for rezoning within the City. These sites are recommended for rezoning as part of Amendment C149 to resolve anomalies in the Kingston Planning Scheme. The Kerr Crescent site is included in this amendment which will result in the zoning of the land being Public Park and Recreation Zone changing from the existing General Residential Zone and Public Use Zone – Service and Utility neither of which reflect the existing use of the land.

The identification of sites to be included as part of Amendment C149 commenced back in 2012.

3.3.3.2 *Further Site Investigations*

The following future steps are proposed with the project:

- Further assessment and review of the design development option to identify a design response that mitigates concerns raised by residents; and
- Confirm regulatory requirements associated with development of additional outdoor sports facilities at Kerr Crescent, Aspendale Gardens.

It is intended that the outcomes of the next steps are report back to Council for its consideration.

4. Conclusion

The development of additional outdoor multisport fields at Kerr Crescent, Aspendale Gardens will assist Council to meet existing and future demands for sporting reserve facilities.

Whilst there is strong support for the development of the proposed additional outdoor multisport fields at Kerr Crescent, Aspendale Gardens there a number of key themes expressed by residents and stakeholders. For Council to progress with the development of additional outdoor multisport fields at Kerr Crescent, Aspendale Gardens, future design development processes must carefully consider and respond to the key feedback themes raised through the consultation process.

With over 80% of online respondents supporting the proposed additional outdoor multisport fields at Kerr Crescent, Aspendale Gardens, it is recommended that Council endorse a design development option for the purposes of commencing the associated statutory processes for the future development of the site.

4.1 Environmental Implications

Vegetation and habitat assessments are currently being prepared for both the Kerr Crescent, Aspendale Gardens and Chadwick Reserve sites. These will inform Council as to associated implications and opportunities to improve environmental outcomes.

There is a risk of encountering acid sulphate soils at Kerr Crescent, which will require careful consideration and management.

4.2 Social Implications

Well planned, high-quality community facilities are likely to encourage use by residents contributing towards social, health and wellbeing benefits.

4.3 Resource Implications

The estimated cost for the development of design development Option is estimated at between \$12 and \$14 million dollars, based upon 29 June 2018 cost plan. A new cost plan is currently under development.

The development of additional outdoor multisport fields at Kerr Crescent, Aspendale Gardens has also attracted the attention of the Victorian Labour Party with \$4.5M pledge made as part of the forthcoming Victorian State election.

Council has provision with the Strategic Resource Plan 2017 – 2021 to contribute towards the development of additional outdoor multisport facilities at Kerr Crescent, Aspendale Gardens and Chadwick Reserve.

4.4 Legal / Risk Implications

There have been no risk implications identified at this time with the development options.

Appendices

Appendix 1 - Factsheet Stingrays Aspendale Soccer Club (Ref 18/591886) [↓](#)

Appendix 2 - Kerr Crescent Development Options A - C (Ref 18/587734) [↓](#)

Appendix 3 - AGRA submission Kerr Crescent (Ref 18/590393) [↓](#)

Appendix 4 - Traffix Report Kerr Crescent, Aspendale Gardens (Ref 18/590448)
[↓](#)

Appendix 5 - Aspendale Gardens Sports Facility vegetation assessment DRAFT
28.5.18 Kerr Crescent Biosis (Ref 18/96164) [↓](#)

Author/s: Mark Stockton, Team Leader Sport and Recreation
Reviewed and Approved By: Daniel Freer, General Manager City Assets and Environment
Bridget Draper, Manager Kingston Active

10.5

SPORTS FIELDS FEASIBILITY CONSULTATION FINDINGS AND NEXT STEPS - ASPENDALE GARDENS

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2	Kerr Crescent Development Options A - C.....	423
3	AGRA submission Kerr Crescent	427
4	Traffic Report Kerr Crescent, Aspendale Gardens.....	429
5	Aspendale Gardens Sports Facility vegetation assessment DRAFT 28.5.18 Kerr Crescent Biosis	471



Aspendale Stingrays Soccer Club



KEY STATS SHEET



Players

- 450** members
- 36** junior teams
- 2** senior teams
- 70** female players



Facilities

- 7** different sites
- spread across municipality** at Aspendale, Parkdale, Cheltenham, Mordialloc & Dingley Village
- School and Council owned grounds



Local trends

- junior rates** increased 59% since 2011
- Strong local growth** expected to 2036 due to growth in the sport, increasing female participation and strong junior participation
- High use of venues** by NPL clubs severely restricts community use at sites and cements need for additional pitches



Future needs

- +** **Single site** with 4 pitches, floodlighting + pavillion
- 📍** **Local home base in Aspendale** to meet growing demand, build club and create community focal point



SEARCH FOR A HOME BASE

1. Council has assessed a number of sites across Kingston including Mordialloc, Parkdale and Aspendale to identify potential suitable locations.
2. With limited opportunities locally, Kerr Crescent Reserve was identified as most suitable local site.
3. A number of potential basic concept designs for the site have been prepared, including options for location of a pavillion.
4. Council is now seeking community feedback on the proposed options.



Have your say

We're keen to hear about how the community currently uses the site:

- What activities are important to be accommodated in any future plans? (e.g. paths for walking/jogging/cycling or a space for dogs to exercise).
- Do you have a preference for the location of a pavillion?
- How could we make sure any impacts on neighbouring residents are minimised?
- Would any changes be needed to local roads and parking?

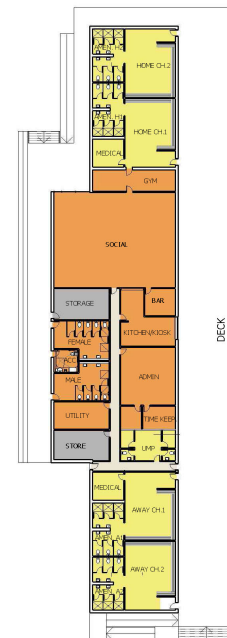
View the plans and let us know your thoughts at:

yourkingstonyoursay.com.au



OPTION A - NORTH WESTERN LOCATION

CAR PARK NUMBER 280



AREA SCHEDULE

CHANGE & SHOWERS	251 M ²
AMENITIES	76 M ²
CHANGE ROOMS	160 M ²
UMPIRE ROOMS	15 M ²
MEDICAL	
(MASSAGE / STRAPPING / MEDICAL ROOM)	20 M ²
KITCHEN, KIOSK & STORAGE	
KITCHEN & KIOSK	25 M ²
KIOSK	0M ²
STORAGE (INT./EXT.)	45 M ²
SOCIAL	150 M ²
UTILITY	24 M ²
PUBLIC TOILETS	45 M ²
VIEWING, MEETING & SOCCER	
GYM / FITNESS ROOM	20 M ²
OFFICE / ADMIN / MEETING	41 M ²
THIRD UMPIRE	0 M ²
TIME KEEPER	9 M ²
ADDITIONAL FACILITIES	
LAUNDRY	0 M ²
STORAGE	0 M ²
COOL ROOM	0 M ²
BAR FACILITIES	10 M ²
EXTERNAL COVERED SPACE	68 M ²
CIRCULATION	32 M ²
GROSS FLOOR AREA:	3570 M²

Issue	Description	Date	CH	ALT.
A	FOR INFORMATION	10/10/18	GS	MM



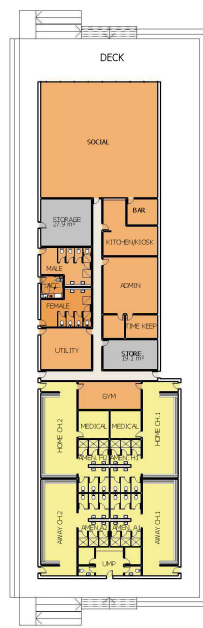
GROUND FLOOR - OPTION A
KERR CRESCENT RESERVE

AA1201



OPTION B - CENTRAL LOCATION - PERPENDICULAR

CAR PARK NUMBER 168



AREA SCHEDULE

CHANGE & SHOWERS	251 M ²
AMENITIES	76 M ²
CHANGE ROOMS	160 M ²
UMPIRE ROOMS	15 M ²
MEDICAL	
(MESSAGE / STRAPPING / MEDICAL ROOM)	20 M ²
KITCHEN, KIOSK & STORAGE	
KITCHEN & KIOSK	25 M ²
KIOSK	0 M ²
STORAGE (INT./EXT.)	45 M ²
SOCIAL	130 M ²
UTILITY	24 M ²
PUBLIC TOILETS	45 M ²
VIEWING, MEETING & SOCCER TO	
GYM / FITNESS ROOM	20 M ²
OFFICE / ADMIN / MEETING	41 M ²
THIRD UMPIRE	0 M ²
TIME KEEPER	9 M ²
ADDITIONAL FACILITIES	
LAUNDRY	0 M ²
STORAGE	0 M ²
COOL ROOM	0 M ²
BAR FACILITIES	10 M ²
EXTERNAL COVERED SPACE	68 M ²
CIRCULATION	32 M ²
GROSS FLOOR AREA: 3570 M ²	

A FOR INFORMATION 1000 00 00 MM
 DATE: 08/08/2024



GROUND FLOOR - OPTION B
 KERR CRESCENT RESERVE AA1202



OPTION C - CENTRAL LOCATION - NORTH SOUTH

CAR PARK NUMBER 165



AREA SCHEDULE

CHANGE & SHOWERS	251 M ²
AMENITIES	76 M ²
CHANGE ROOMS	160 M ²
UMPIRE ROOMS	15 M ²
MEDICAL	20 M²
(MASSAGE / STRAPPING / MEDICAL ROOM)	20 M ²
KITCHEN, KIOSK & STORAGE	289 M²
KITCHEN & KIOSK	25 M ²
KIOSK	0 M ²
STORAGE (INT./EXT.)	45 M ²
SOCIAL	150 M ²
UTILITY	24 M ²
PUBLIC TOILETS	45 M ²
VIEWING, MEETING & SOCCER	70 M²
GYM / FITNESS ROOM	20 M ²
OFFICE / ADMIN / MEETING	41 M ²
THIRD UMPIRE	0 M ²
TIME KEEPER	9 M ²
ADDITIONAL FACILITIES	110 M²
LAUNDRY	0 M ²
STORAGE	0 M ²
COOL ROOM	0 M ²
BAR FACILITIES	10 M ²
EXTERNAL COVERED SPACE	68 M ²
CIRCULATION	32 M ²
GROSS FLOOR AREA:	3570 M²



GROUND FLOOR - OPTION C
KERR CRESCENT RESERVE AA1203



NO.	DESCRIPTION	DATE	BY	CHK.	APP.



Aspendale Gardens Residents' Association

8 August 2018

**John Nevins
CEO
City of Kingston**

Re: Aspendale Gardens Sportsground Development

Dear John,

We refer to plans to add a second oval, pavilion and car parking to the existing Aspendale Gardens sportsground.

In principal AGRA supports the development of a second oval, change rooms and toilet facilities at Aspendale Gardens Sportsground.

However the proposed council costings of between \$8.9M and \$12.9M seems excessive for the job at hand. AGRA would like to see a smaller development at Aspendale Gardens in favour of funds being used for a "Casey Fields" style facility in the Green Wedge rather than in areas like Aspendale Gardens.

AGRA supports:

- Development of an appropriate sized pavilion located between the two ovals. A building style like the one recently constructed at Jack Grut Reserve is envisaged
- Retention of the existing walking paths
- Retention of the existing AFL goal posts

AGRA does not support:

- Large scale car parks in the reserves in Kerr Crescent and/or Nurten Parade
- Removal of vegetation in reserves in Kerr Crescent and/or Nurten Parade
- Vehicle access beyond reserve fences in Kerr Crescent and Nurten Parade
- An oversized pavilion
- A pavilion with social rooms used after hours

- Ground Lighting

AGRA recommends:

- Designs that work utilising the existing oval footprint without significant rework
- Designs that do not require the wholesale rework of the existing oval drainage

AGRA would like to see:

- Continued planting of trees around Aspendale Gardens sportsground
- Replacement of the existing grass surface with new turf (as the existing turf is made up of many grass species and is very patchy and uneven)
- Fencing of the main oval for use as a dogs off lead reserve
- Solar pathway lighting between Nurten Parade to Kerr Crescent to Tarongo Drive for safe access to Aspendale train station in winter months
- Replacement of the broken concrete paving on the pathway from Aspendale Gardens to Tarongo Drive near the timber bridge

Once the plans are available for community consultation AGRA will call a community meeting to obtain feedback from residents.

AGRA will also work actively with Aspendale Stingrays and Council to ensure the best outcome for the Aspendale Gardens community

ANDREW HIGHAM
PRESIDENT
ASPENDALE GARDENS RESIDENTS ASSOCIATION



Traffic Engineering Assessment

Kearney Drive Reserve, Aspendale Gardens

Prepared for
Kingston City Council

Traffix Group Pty Ltd ABN 32 100 481 570
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Email: admin@traffixgroup.com.au



Traffic Engineering Assessment
Kearney Drive Reserve, Aspendale Gardens

Traffic Engineering Assessment

Kearney Drive Reserve, Aspendale Gardens

Document Control

Issue No.	Type	Date	Prepared By	Approved By
A	Draft	6/08/2018	B Hodges	W de Waard
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Our Reference: G25017R-01B

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G25017R-01B



Traffic Engineering Assessment
Kearney Drive Reserve, Aspendale Gardens

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Appendix A	Kearney Drive Reserve Parking Surveys
Appendix B	Activity Profile Trip Generations

Traffic Engineering Assessment

Kearney Drive Reserve, Aspendale Gardens

1 Introduction

Traffix Group has been engaged by Kingston City Council to undertake a traffic impact assessment for the introduction of additional formal sporting activities at Kearney Drive Reserve, Aspendale Gardens.

This traffic engineering assessment reviews the proposed sporting uses, identifies the likely traffic and parking demands and provides recommendations regarding the provision of carparking.

2 Proposal

Kingston City Council is investigating the introduction of additional formal sporting activities at Kearney Drive Reserve, Aspendale Gardens. Under the existing conditions the reserve primarily caters for informal recreation, with 1 cricket oval utilised in the summer period. The current proposal seeks to expand the existing summer cricket use and introduce winter soccer competition as follows:

- **Summer** – 2 x Cricket Ovals (current 1 x cricket oval), and
- **Winter** – 4 x Soccer pitches (new use).

During the summer period, the reserve will continue to be utilised by the Chelsea Heights/Aspendale Gardens Cricket Club for both junior and senior competition (with expanded activity), whilst the winter soccer usage would accommodate the Aspendale Stingrays Soccer Club for both junior and senior competition (who current play at a range of other facilities throughout Kingston). A summary of the proposed broad activity is provided in Table 1 below.

Table 1: Anticipated Activity – Kearney Drive Reserve

Use	Day	Time	Daily Peak Teams / Games	Fields Used
Winter Usage – Soccer – Aspendale Stingrays Soccer Club				
Training	Mon-Fri	5:00pm-6:00pm	8 x Under 10 teams	2 x fields
		5:00pm-9:00pm	8 x Under 11-18 teams	2 x fields
		6:30pm-8:30pm	2 x Seniors teams	1 x field
Matches	Sat	9:00am-1:00pm	8 x Under 10 games	2 x fields
		1:00pm-5:00pm	2 x Seniors games	1 x field
	Sun	9:00am-10:00am	4 x Under 10 games	2 x fields
		11:00am-5:00pm	6 x Under 11-18 games	2 x fields
Summer Usage – Cricket – Chelsea Heights/Aspendale Gardens Cricket Club				
Training	Tue & Thurs	4:30pm-6:00pm	4 x Junior teams	Cricket nets
		6:00pm-8:00pm	4 x Senior teams	Cricket nets
Matches	Sat	9:00am-1:00pm	2 x Junior games	2 x fields
		1:00pm-6:00pm	2 x Senior games	2 x fields

The critical operation time for the proposed reserve usage will occur during the winter soccer competition on weekends when several games occur at the same time. It is noted that the senior competition operates on a home and away schedule, therefore seniors matches at Kearney Drive reserve would occur every second weekend.

Traffic Engineering Assessment

Kearney Drive Reserve, Aspendale Gardens

3 Existing Conditions

3.1 Subject Site

Kearney Drive Reserve is located to the south of Kerr Crescent and Nurten Parade in Aspendale Gardens. The reserve is bounded by a drainage channel along the south-western edge.

The subject site currently provides for a single oval and temporary toilets, which is currently utilised for cricket games in the summer and ad-hoc AFL training (i.e. pre-season).

No off-street carparking is currently provided for Kearney Drive Reserve, with all existing parking demands occurring on-street in the nearby area. Parking demands would be likely to occur in Kerr Crescent and Nurten Parade, given the reserve frontage and pedestrian access.

Kearney Drive Reserve is predominantly zoned as 'Public Use Zone – Service and Utility'. The broad area is predominantly occupied by residential properties with the following key non-residential land uses in the vicinity of the subject site include:

- Aspendale Gardens Primary School located adjacent to the site on the north-east boundary,
- Yammerbook Nature Reserve to the northwest,
- Edithvale Wetlands to the southeast, and
- Rossdale Golf Course to the south.

A locality plan, aerial photograph and Planning Zone map of the site are presented below in Figure 1, Figure 2 and Figure 3.

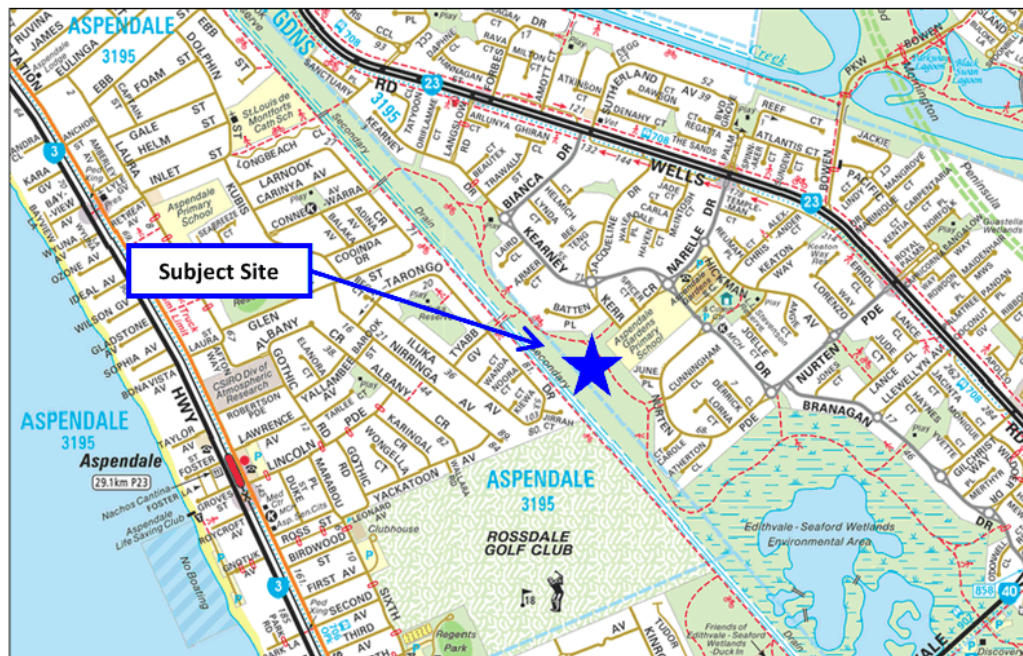


Figure 1: Locality Map

Traffic Engineering Assessment
Kearney Drive Reserve, Aspendale Gardens



Source: Nearmap

Figure 2: Aerial Photograph



Source: www.land.vic.gov.au

Figure 3: Planning Zone Map (Kingston Planning Scheme)

Traffic Engineering Assessment

Kearney Drive Reserve, Aspendale Gardens

3.2 Road Network

Kearney Drive broadly operates as a collector road that extends from a cul de sac north of Tatyoon Close in the northwest to Nurten Parade in the southeast. In the vicinity of the subject site Kearney Drive provides for a 9.5m single carriageway which accommodates a single traffic lane in each direction. Parallel kerbside parking is permitted on both sides of Kearney Drive.

Kearney Drive is subject to a posted speed limit of 40km/h in the vicinity of Aspendale Primary School and 50km/h in the remaining sections.



Figure 4: Kearney Drive – View North-West

Kerr Crescent is a local street that extends as a 'loop' to the south of Kearney. Kerr Crescent generally provides for a 7.3m wide carriageway, which allows for parking on one side and two lanes of traffic or parking on both sides and a single lane of traffic. In the section in the vicinity of the Aspendale Primary School, indented parking (mixture of 90-degree and parallel parking) is provided the southeast/southwest side.

Kerr Crescent is subject to a posted speed limit of 40km/h in the vicinity of Aspendale Primary School and 50km/h in the northern section.



Figure 5: Kerr Crescent – View South-West

Traffic Engineering Assessment

Kearney Drive Reserve, Aspendale Gardens

Nurten Parade extends from Wells Road in the north and forms a 'loop' to the south of Kearney Drive. Nurten Drive operates as a collector road to the north of Kearney Drive and a local road to the south of Kearney Drive.

In the vicinity of the subject site, Nurten Parade provides approximately a 8.5m wide carriageway with a single traffic lane in each direction. Nurten Parade permits parallel kerbside parking on both sides of the road.

In the vicinity of the subject site Nurten Parade is subject to the default urban speed limit of 50km/h.



Figure 6: Nurten Parade – View North

3.3 Cyclist and Pedestrian Facilities

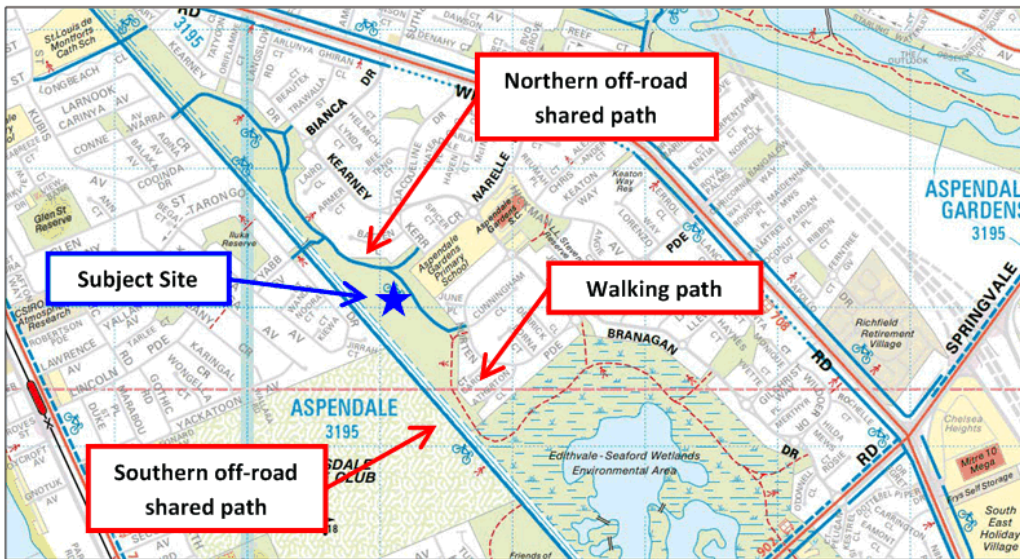
Formal bicycle and pedestrian facilities in the vicinity of the subject site include 2.0m wide off-road shared paths which extend along the north and south boundary of the subject site.

The southern off-road shared path forms part of VicRoads Principal Bicycle Network. The path extends along Edithvale Wetlands from Mordialloc Creek near Mordialloc Pier in the northwest to Thames Promenade in the southeast.

The northern off-road shared path extends parallel to the southern path. The northern off-road shared path extends from a connection with to the southern off-road shared path in Yammerbook Nature Reserve to Nurten Parade in the southeast. Two bridge connections are provided between the northern and southern paths over the main drainage channel.

A walking track runs along Edithvale Wetlands. The track extends from a connection with the northern off-road shared path near Nurten Parade in the northwest to a connection with the southern off-road shared path in Chelsea Heights.

A summary of the key pedestrian and cyclist facilities in the vicinity of Kearney Drive Reserve are shown in Figure 7 below.



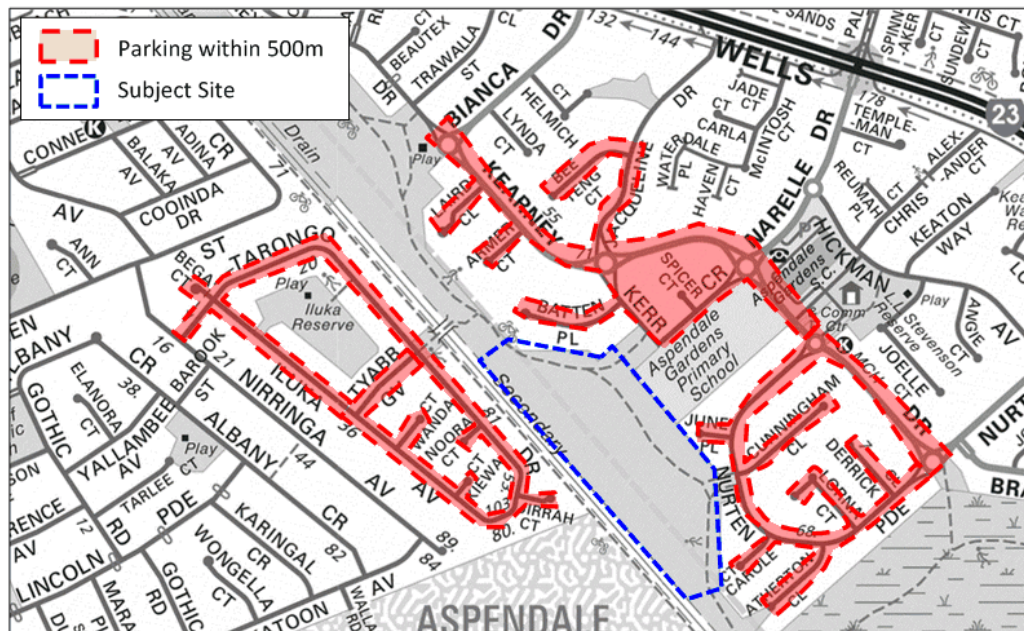
Source: TravelSmart Map

Figure 7: 'TravelSmart' Transport Map

4 Existing Carparking Conditions

4.1 Existing Supply and Restrictions

There is a total of 827 public carparking spaces within approximately 500m walking distance of Kearney Drive Reserve, as shown in Figure 8 below.



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Figure 8: Existing Parking Within 500m Walking Distance of Kearney Drive Reserve

Traffic Engineering Assessment

Kearney Drive Reserve, Aspendale Gardens

The car parking in the vicinity of Kearney Drive Reserve is largely 'unrestricted' with the only exceptions being on Kearney Drive, between Nurten Parade and Kerr Crescent and Kerr Crescent, opposite Spicer Court. A summary of the parking within 500m of Kearney Drive Reserve is provided in Table 2 below.

Table 2: Existing Parking Supply within 500m of Kearney Drive Reserve

Street	Unrestricted	No Parking (8-9:30am & 2:30-4pm on School Days)	No stopping (8:15- 9:15am & 3-4pm on School Days)	Capacity
North of Drainage Channel				
Kearney Drive	138	-	5	143
Nurten Parade	104	-	-	104
Derrick Close	18	-	-	18
Lorna Court	29	-	-	29
Atherton Close	28	-	-	28
Carole Court	15	-	-	15
Cunningham Close	32	-	-	32
June Place	7	-	-	7
Kerr Crescent	66	4	-	70
Spicer Court	10	-	-	10
Batten Place	35	-	-	35
Armer Court	18	-	-	18
Laird Close	12	-	-	12
Jacqueline Drive	32	-	-	32
Bee Teng Court	29	-	-	29
Sub-Total	573	4	5	582
South of Drainage Channel				
Tarongo Drive	103	-	-	103
Tyabb Grove	24	-	-	24
Iluka Avenue	76	-	-	76
Wanda Court	11	-	-	11
Kiewa Court	6	-	-	6
Jirrah Court	9	-	-	9
Noora Court	8	-	-	8
Bega Court	8	-	-	8
Sub-Total	245	0	0	245
Total	818	4	5	827

Traffic Engineering Assessment

Kearney Drive Reserve, Aspendale Gardens

4.2 Existing Parking Demand

A series of parking occupancy surveys were undertaken for the parking in the vicinity of Kearney Drive Reserve at the following times:

- Saturday, 23rd June, 2018 – 9am-5pm at hourly intervals
- Sunday, 24th June, 2018 – 9am-5pm at hourly intervals
- Tuesday, 26th June, 2018 – 5pm-9pm at hourly intervals
- Wednesday, 27th June, 2018 – 5pm-9pm at hourly intervals
- Thursday, 21st June, 2018 – 5pm-9pm at hourly intervals

It is noted that no formal activity (i.e. sports) was occurring at Kearney Drive Reserve during the time of the surveys. Therefore, all observed parking demands are associated with the existing surrounding land uses (primarily residential at the times of the surveys).

A summary of the overall parking occupancies observed is provided in Figure 9 below.

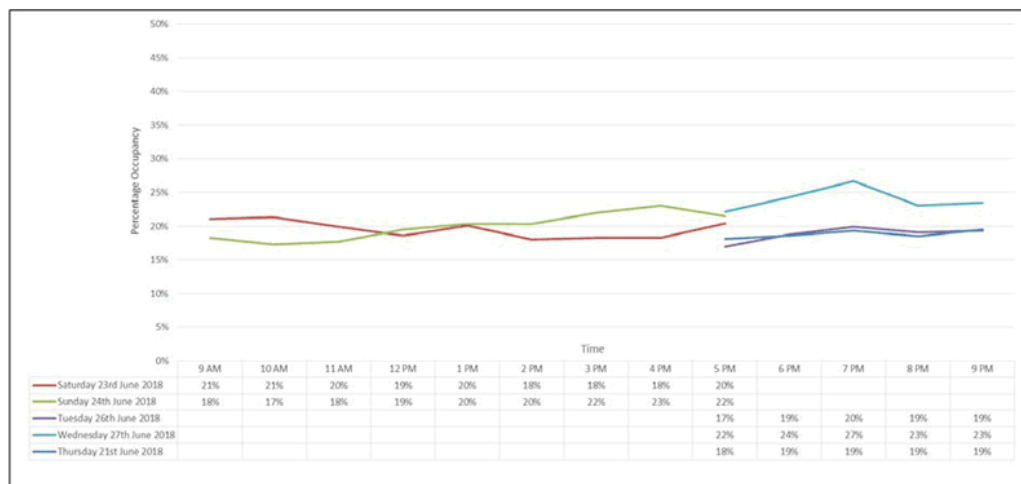


Figure 9: Parking Occupancy Surveys – Overall Parking Occupancies

The peak parking demands recorded on each day are as follows:

- **Saturday, 23rd June:** 10am - 176 vehicles parked (21% Occupancy) and 651 spaces available.
- **Sunday, 24th June:** 4pm - 191 vehicles parked (23% Occupancy) and 636 spaces available.
- **Tuesday, 26th June:** 7pm - 165 vehicles parked (20% Occupancy) and 662 spaces available.
- **Wednesday, 27th June:** 7pm - 221 vehicles parked (27% Occupancy) and 606 spaces available.
- **Thursday, 21st June:** 7pm - 160 vehicles parked (19% Occupancy) and 667 spaces available.

In general, the parking occupancy survey results show that parking demands were steady across each day, form a minor peak in the evenings on weekdays and late afternoon on weekends. This profile is typical of residential streets, where residents are out during the day (i.e. work, shopping, etc.) and return home during the evening. Furthermore, we would expect these low parking demands in typical

Traffic Engineering Assessment

Kearney Drive Reserve, Aspendale Gardens

residential streets, where a large portion of the residential parking demands are catered for within private off-street parking (i.e. driveways / garages).

A summary of the parking conditions during the Sunday peak (4pm) and the Wednesday peak (7pm) are provided in Table 3 and Table 4 below.

Table 3: On-Street Parking Availability – Peak Period (Sunday 24th June, 2018 – 4pm)

Street	Capacity	No. Cars Parked	% Occupancy	Spaces Available
North of Drainage Channel				
Kearney Drive	143	26	18%	117
Nurten Parade	104	14	13%	90
Derrick Close	18	5	28%	13
Lorna Court	29	7	24%	22
Atherton Close	28	4	14%	24
Carole Court	15	3	20%	12
Cunningham Close	32	13	41%	19
June Place	7	3	43%	4
Kerr Crescent	70	7	10%	63
Spicer Court	10	2	20%	8
Batten Place	35	10	29%	25
Armer Court	18	6	33%	12
Laird Close	12	5	42%	7
Jacqueline Drive	32	3	9%	29
Bee Teng Court	29	7	24%	22
Sub-Total	582	115	20%	467
South of Drainage Channel				
Tarongo Drive	103	32	31%	71
Tyabb Grove	24	6	25%	18
Iluka Avenue	76	27	36%	49
Wanda Court	11	1	9%	10
Kiewa Court	6	0	0%	6
Jirrah Court	9	2	22%	7
Noora Court	8	3	38%	5
Bega Court	8	5	63%	3
Sub-Total	245	76	31%	169
Total	827	191	23%	636

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Table 4: On-Street Parking Availability – Peak Period (Wednesday 27th June, 2018 – 7pm)

Street	Capacity	No. Cars Parked	Percentage Occupancy	Spaces Available
North of Drainage Channel				
Kearney Drive	143	40	28%	103
Nurten Parade	104	21	20%	83
Derrick Close	18	6	33%	12
Lorna Court	29	7	24%	22
Atherton Close	28	5	18%	23
Carole Court	15	3	20%	12
Cunningham Close	32	10	31%	22
June Place	7	6	86%	1
Kerr Crescent	70	34	49%	36
Spicer Court	10	2	20%	8
Batten Place	35	11	31%	24
Armer Court	18	3	17%	15
Laird Close	12	4	33%	8
Jacqueline Drive	32	3	9%	29
Bee Teng Court	29	7	24%	22
Sub-Total	582	162	28%	420
South of Drainage Channel				
Tarongo Drive	103	18	17%	85
Tyabb Grove	24	7	29%	17
Iluka Avenue	76	28	37%	48
Wanda Court	11	2	18%	9
Kiewa Court	6	1	17%	5
Jirrah Court	9	1	11%	8
Noora Court	8	2	25%	6
Bega Court	8	0	0%	8
Sub-Total	245	59	24%	186
Total	827	221	27%	606

A full summary of the parking occupancy survey results are provided at Appendix A.

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5 Traffic Engineering Assessment

5.1 Parking Demands

The following section details the parking demands associated with the existing and potential future sporting uses of Kearney Drive Reserve. While cricket currently occurs the single oval at Kearney Drive Reserve, Council's proposal expands the cricket usage and includes the introduction of soccer for the winter season.

5.1.1 Winter Soccer Usage – Development of Parking Demands

Parking Demands Per Team / Game:

The expected parking demands for the winter soccer use have been determined based on likely numbers of players, coaches, officials and spectators for each team / game. For each team / game the parking demands have been considered during the training session / game and during the drop-off / pick-up period as shown in Table 5 below.

Table 5: Anticipated Soccer Parking Demands Per Team / Game

Team			Parking Rate	During Session	Pick-up / Drop-off	Comments / Assumptions
Training						
U10	Players	10	0.8 spaces/player	6	8	<ul style="list-style-type: none"> 20% players carpool / walk / cycle 80% parents stay during session Coaches are parents of players
	Coaches	2	Parents of players			
	Total	12	-	6	8	
U11-U18	Players	15	0.8 spaces/player	2	12	<ul style="list-style-type: none"> 20% players carpool / walk / cycle 20% parents stay during session Coaches are parents of players
	Coaches	2	Parents of players			
	Total	17	-	2	12	
Seniors	Players	15	1 space/player	15	15	<ul style="list-style-type: none"> No players carpool / walk / cycle Coaches are independent of players
	Coaches	2	1 space/coach	2	2	
	Total	17	-	17	17	
Games						
U10	Players	20	1 space/player	18	20	<ul style="list-style-type: none"> No players carpool / walk / cycle 90% of parents stay to watch game Coaches and spectators are parents of players – no additional parking demands
	Coaches	4	Parents of players			
	Spectators	20				
	Officials	1	1 space/official	1	1	
	Total	45	-	19	21	
U11-U18	Players	30	1 space/player	21	30	<ul style="list-style-type: none"> No players carpool / walk / cycle 70% of parents stay to watch game Coaches and spectators are parents of players – no additional parking demands
	Coaches	4	Parents of players			
	Spectators	30				
	Officials	1	1 space/official	1	1	
	Total	65	-	22	31	

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Team			Parking Rate	During Session	Pick-up / Drop-off	Comments / Assumptions
Seniors	Players	30	1 spaces/player		30	<ul style="list-style-type: none"> No players carpool / walk / cycle Coaches are independent of players Spectators typically have a relation with the players resulting in low parking demands
	Coaches	4	1 spaces/coach		4	
	Spectators	100	0.3 spaces/spectator		30	
	Officials	3	1 space/official		3	
	Total	137		-		

Proposed Scheduling & Anticipated Parking Demands:

Based on information provided by Council and the Aspendale Stingrays Soccer Club, we have developed an 'Activity Profile' that replicates the likely activity for the proposed sporting activity at Kearney Drive Reserve.

For each game or training session the 'drop-off' parking demands have been allocated at the start of the activity, the 'during session' demands have been applied for the length of the activity and the 'pick-up' demands have been applied at the end of the session.

This approach allows for consideration of the 'overlap' between matches / session that often leads to the peak parking demands for sporting activity.

Figure 9 below shows the activity profile developed for the proposed sports activity at the Kearney Drive Reserve.

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		Weeknight Training							
		5pm	5:30pm	6pm	6:30pm	7pm	7:30pm	8pm	8:30pm
Field 1	Activity	U10 Training - 4 Teams		<<Pick up					
	Parking Demand	32	24	32					
Field 2	Activity	U10 Training - 4 Teams		<<Pick up					
	Parking Demand	32	24	32					
Field 3	Activity	U11-U18 Training - 2 Teams		<<Pick up					
	Parking Demand	24	4	4	24				
Field 4	Activity	U11-U18 Training - 2 Teams		<<Pick up					
	Parking Demand	24	4	4	24				
Field 4	Activity	U11-U18 Training - 2 Teams		<<Pick up					
	Parking Demand	24	4	4	24				
Total Parking Demand		112	56	72	130	42	42	82	34

		Saturday Competition																		
		8:30am	9am	9:30am	10am	10:30am	11am	11:30am	12noon	12:30pm	1pm	1:30pm	2pm	2:30pm	3pm	3:30pm	4pm	4:30pm	5pm	5:30pm
Field 1	Activity	Drop off>>		U10 Competition - 2 Games		<<Pick up		Drop off>>		U10 Competition - 2 Games		<<Pick up								
	Parking Demand	42	38	38	42	42	38	38	42											
Field 2	Activity	Drop off>>		U10 Competition - 2 Games		<<Pick up		Drop off>>		U10 Competition - 2 Games		<<Pick up								
	Parking Demand	42	38	38	42	42	38	38	42											
Field 3	Activity																			
	Parking Demand																			
Field 4	Activity																			
	Parking Demand																			
Field 4	Activity	Drop off>>																		
	Parking Demand	67																		
Field 4	Activity	Drop off>>																		
	Parking Demand	67																		
Total Parking Demand		84	76	76	84	84	76	76	84	67	67	67	67	134	134	67	67	67	67	0

		Sunday Competition																		
		8:30am	9am	9:30am	10am	10:30am	11am	11:30am	12noon	12:30pm	1pm	1:30pm	2pm	2:30pm	3pm	3:30pm	4pm	4:30pm	5pm	5:30pm
Field 1	Activity	Drop off>>		U10 Competition - 2 Games		<<Pick up														
	Parking Demand	42	38	38	42															
Field 2	Activity	Drop off>>		U10 Competition - 2 Games		<<Pick up														
	Parking Demand	42	38	38	42															
Field 3	Activity																			
	Parking Demand																			
Field 4	Activity																			
	Parking Demand																			
Total Parking Demand		84	76	76	84	62	44	44	44	106	106	44	44	106	106	44	44	44	62	0

Figure 10: Kearney Drive Reserve – ‘Activity Profile’ Parking Demands



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The overall peak parking demand is estimated to occur during the changeover between the 'reserves' and 'seniors' matches on a Saturday afternoon. During this time a peak parking demand of 134 spaces is predicted. There are also a number of other time periods where a marginally lower parking demand is expected including:

- Weekday Training (6:30pm) – 130 spaces
- Sunday Competition (12:30pm, 1:00pm, 2:30pm, 3:00pm) – 106 spaces

Future Soccer Club Growth:

Council have advised that Aspendale Stingrays Soccer Club is likely to grow by 6-8 junior teams into the future. These teams would consist of a mix of age groups and genders.

For the purpose of this assessment, we have adopted the higher growth rate of 8 teams and distributed the growth evenly across the U10 teams and the U11-18 teams (i.e. 4 additional teams each).

We have reviewed the general scheduling previously presented in Figure 10 and allocated the potential 'future' games in the most likely timeslots. The U10 games have been allocated to occur on Saturday morning, with the U11-18 games scheduled to occur on Sundays. On both weekday days, the additional teams result in the operation of the three (3) fields simultaneously.

A summary of the estimated 'Future' parking demands incorporating the additional 8 games (shown in 'red') is shown in Figure 11 below.

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		Weeknight Training							
		5pm	5:30pm	6pm	6:30pm	7pm	7:30pm	8pm	8:30pm
Field 1	Activity	U10 Training - 4 Teams <<Pick up							
	Parking Demand	32	24	32					
	Activity				Seniors Training - 2 Teams				<<Pick up
Field 2	Activity	U10 Training - 4 Teams <<Pick up							
	Parking Demand	32	24	32					
	Activity				U11-U18 Training - 2 Teams				<<Pick up
Field 3	Activity	U11-U18 Training - 2 Teams <<Pick up							
	Parking Demand	24	4	4	24				
	Activity				U11-U18 Training - 2 Teams				<<Pick up
Field 4	Activity	U11-U18 Training - 2 Teams <<Pick up							
	Parking Demand	24	4	4	24				
	Activity				U11-U18 Training - 2 Teams				<<Pick up
Total Parking Demand		112	56	72	154	46	46	106	34

		Saturday Competition																		
		8:30am	9am	9:30am	10am	10:30am	11am	11:30am	12noon	12:30pm	1pm	1:30pm	2pm	2:30pm	3pm	3:30pm	4pm	4:30pm	5pm	5:30pm
Field 1	Activity	Drop off>>		U10 Competition - 2 Games <<Pick up		Drop off>>		U10 Competition - 2 Games <<Pick up												
	Parking Demand	42	38	38	42	42	38	38	42											
	Activity	Drop off>>		U10 Competition - 2 Games <<Pick up		Drop off>>		U10 Competition - 2 Games <<Pick up												
Field 2	Activity	Drop off>>		U10 Competition - 2 Games <<Pick up		Drop off>>		U10 Competition - 2 Games <<Pick up												
	Parking Demand	42	38	38	42	42	38	38	42											
	Activity	Drop off>>		U10 Competition - 2 Games <<Pick up		Drop off>>		U10 Competition - 2 Games <<Pick up												
Field 3	Activity	Drop off>>		U10 Competition - 2 Games <<Pick up		Drop off>>		U10 Competition - 2 Games <<Pick up												
	Parking Demand	42	38	38	42	42	38	38	42											
	Activity	Drop off>>		U10 Competition - 2 Games <<Pick up		Drop off>>		U10 Competition - 2 Games <<Pick up												
Field 4	Activity	Drop off>>		U10 Competition - 2 Games <<Pick up		Drop off>>		U10 Competition - 2 Games <<Pick up		Drop off>>				Seniors Competition - Reserves - 1 Game <<Pick up		Drop off>>		Seniors Competition - Seniors - 1 Game <<Pick up		
	Parking Demand	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
	Activity	Drop off>>		U10 Competition - 2 Games <<Pick up		Drop off>>		U10 Competition - 2 Games <<Pick up		Drop off>>				Seniors Competition - Reserves - 1 Game <<Pick up		Drop off>>		Seniors Competition - Seniors - 1 Game <<Pick up		
Total Parking Demand		126	114	114	126	126	114	114	126	0	0	0	0	0	0	0	0	0	0	0

		Sunday Competition																		
		8:30am	9am	9:30am	10am	10:30am	11am	11:30am	12noon	12:30pm	1pm	1:30pm	2pm	2:30pm	3pm	3:30pm	4pm	4:30pm	5pm	5:30pm
Field 1	Activity	Drop off>>		U10 Competition - 2 Games <<Pick up		Drop off>>		U11 - U18 Competition - 1 Game		Drop off>>		U11 - U18 Competition - 1 Game <<Pick up		Drop off>>		U11 - U18 Competition - 1 Game <<Pick up		Drop off>>		
	Parking Demand	42	38	38	42	31	22	22	22	31	22	22	22	31	22	22	22	22	31	
	Activity	Drop off>>		U10 Competition - 2 Games <<Pick up		Drop off>>		U11 - U18 Competition - 1 Game		Drop off>>		U11 - U18 Competition - 1 Game <<Pick up		Drop off>>		U11 - U18 Competition - 1 Game <<Pick up		Drop off>>		
Field 2	Activity	Drop off>>		U10 Competition - 2 Games <<Pick up		Drop off>>		U11 - U18 Competition - 1 Game		Drop off>>		U11 - U18 Competition - 1 Game <<Pick up		Drop off>>		U11 - U18 Competition - 1 Game <<Pick up		Drop off>>		
	Parking Demand	42	38	38	42	31	22	22	22	31	22	22	22	31	22	22	22	22	31	
	Activity	Drop off>>		U10 Competition - 2 Games <<Pick up		Drop off>>		U11 - U18 Competition - 1 Game		Drop off>>		U11 - U18 Competition - 1 Game <<Pick up		Drop off>>		U11 - U18 Competition - 1 Game <<Pick up		Drop off>>		
Field 3	Activity	Drop off>>		U11 - U18 Competition - 1 Game <<Pick up		Drop off>>		U11 - U18 Competition - 1 Game <<Pick up		Drop off>>		U11 - U18 Competition - 1 Game <<Pick up		Drop off>>		U11 - U18 Competition - 1 Game <<Pick up		Drop off>>		
	Parking Demand	31	22	22	22	31	22	22	22	31	22	22	22	31	22	22	22	22	31	
	Activity	Drop off>>		U11 - U18 Competition - 1 Game <<Pick up		Drop off>>		U11 - U18 Competition - 1 Game <<Pick up		Drop off>>		U11 - U18 Competition - 1 Game <<Pick up		Drop off>>		U11 - U18 Competition - 1 Game <<Pick up		Drop off>>		
Field 4	Activity	Drop off>>		U11 - U18 Competition - 1 Game <<Pick up		Drop off>>		U11 - U18 Competition - 1 Game <<Pick up		Drop off>>		U11 - U18 Competition - 1 Game <<Pick up		Drop off>>		U11 - U18 Competition - 1 Game <<Pick up		Drop off>>		
	Parking Demand	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
	Activity	Drop off>>		U11 - U18 Competition - 1 Game <<Pick up		Drop off>>		U11 - U18 Competition - 1 Game <<Pick up		Drop off>>		U11 - U18 Competition - 1 Game <<Pick up		Drop off>>		U11 - U18 Competition - 1 Game <<Pick up		Drop off>>		
Total Parking Demand		115	98	98	106	115	97	66	66	159	159	66	66	159	159	66	66	66	93	0

Figure 11: Kearney Drive Reserve – ‘Activity Profile’ Parking Demands – ‘Future’ Scenario

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In the 'future' scenario with 8 additional junior teams, the peak parking demand is estimated to be 159 spaces which would occur at 12:30pm, 1:00pm 2:30pm and 3:00pm on a Sunday.

5.1.2 Cricket – Existing Summer Usage

Cricket games typically generate lower parking demands when compared to soccer activity due to the significantly lower number of spectators. Typically, the length of cricket matches at 4-5 hours limits the number of spectators who are willing to commit to this level of time.

Further, the physical space required for a cricket oval, leads to a low level of simultaneous games when compared to the soccer activity (i.e. 4 soccer fields fit into the same footprint as 2 cricket ovals).

Traffix Group has undertaken previous parking demands surveys of cricket ovals. These surveys indicated that each game of senior's cricket generates approximately 36 spaces/game.

Given the current proposal could cater for up to 2 simultaneous cricket games, a peak parking demand in the order of 72 spaces could be expected.

5.1.3 Anticipated Parking Demand Summary

A summary of the anticipated parking demands for the proposed sporting activity at Kearney Drive Reserve are presented in Table 6 below.

Table 6: Anticipated Parking Demands

Sport		Measure	Peak Parking Demand	Day / Time
Soccer (Winter)	Current	Competition - Seniors	134 spaces	3pm Saturday
		Training	130 spaces	Weeknights
	Future	Competition – Juniors	159 spaces	12:30pm, 1:00pm 2:30pm and 3:00pm Sunday
		Training	154 spaces	Weeknights
Cricket (Summer)		Competition	76 spaces	Saturdays

In view of the above, the carparking demands associated with the winter soccer activity clearly govern the carparking requirements for Kearney Drive Reserve. A summary of the critical soccer parking demands are shown in Figure 11 below with the parking demands for the current usage shown in solid lines and the future usage shown in dashed lines.

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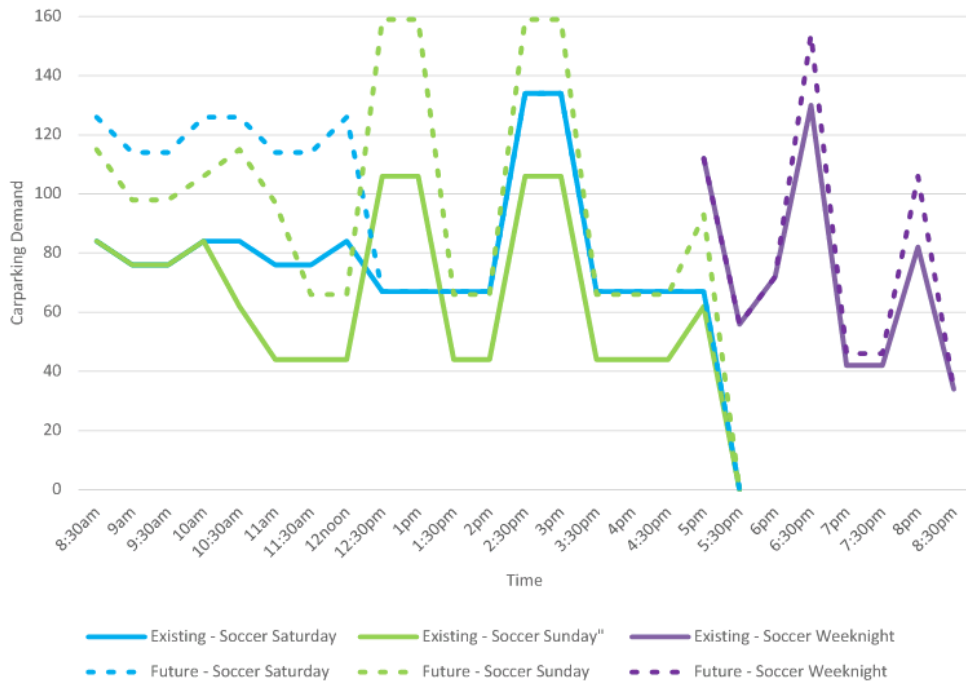


Figure 12: Soccer Parking Demands (Existing and Future)

5.2 Reserve Parking Provision

5.2.1 Initial Council Concept Designs

Council has prepared some preliminary concept plans for possible configuration of Kearney Drive Reserve. These plans indicate the following potential parking options:

- Option 1 – Carpark and pavilion located in the vicinity of Kerr Crescent (81 parking spaces)
- Option 2 – Carpark and pavilion located in the vicinity of Nurten Parade (70 parking spaces).

In our experience, parking facilities for reserves are best provided via a single access rather than multiple fragmented carparks. This is due to the nature of sporting fields whereby on game days approximately half of the parking demands associated with the sporting uses relate to the 'away' team who are unfamiliar with the carparking arrangements. On this basis, we could recommend that parking is provided in one of the potential locations, not both.

A summary of the initial carparking options provided by Council is shown in Figure 7 below.

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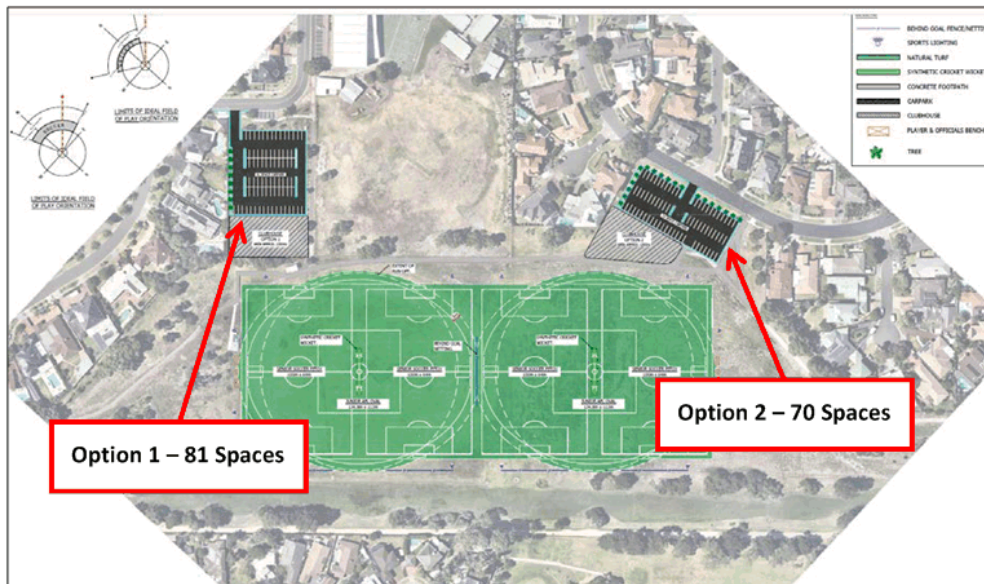


Figure 13: Initial Concept Designs – On-Site Carparking

The table below compares Council’s proposed on-site parking provision with the anticipated peak soccer parking demands.

Table 7: Parking Demand vs Proposed On-Site Parking Supply

Measure	Current Soccer Demands	Future Soccer Demands
Senior Competition		
Peak Parking Demand		134 spaces
Proposed Reserve Parking Provision		70-81 spaces
Shortfall		53-64 spaces
Junior Competition		
Peak Parking Demand	106 spaces	159 spaces
Proposed Reserve Parking Provision	70-81 spaces	70-81 spaces
Shortfall	25-36 spaces	78-89 spaces

The initial Council parking provisions for Option 1 and Option 2 are considerably below the peak parking demands expected for Kearney Drive Reserve. If the proposed supply of 78-89 spaces were adopted, we would expect that parking demands would overflow into the surrounding local streets during both for the senior and junior competition on Saturdays and Sundays.



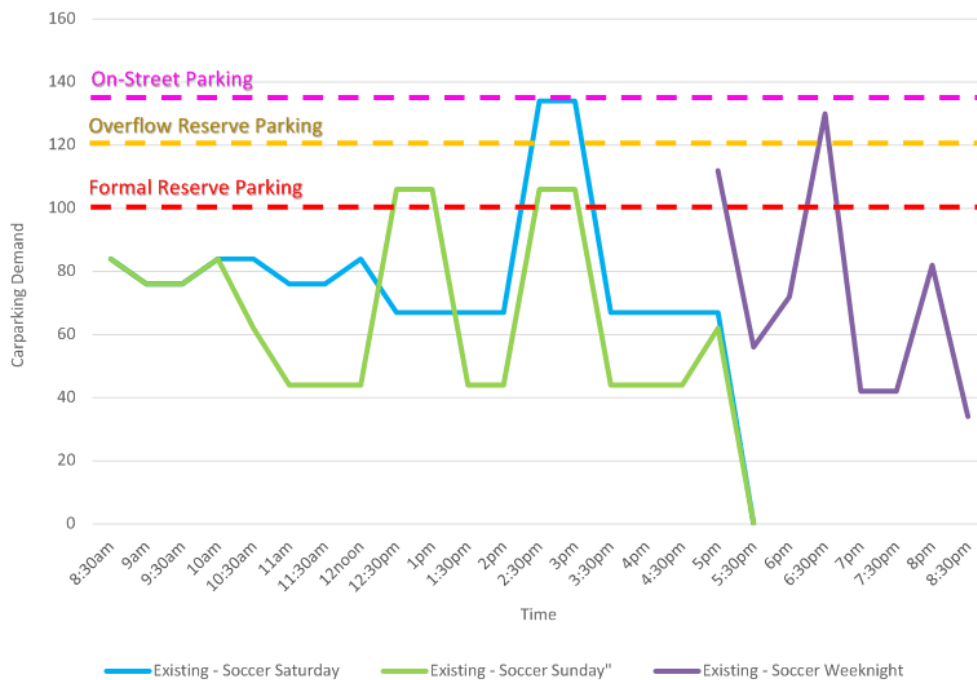
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5.2.2 Recommended Parking Provision – Current Usage

Based on a review of the predicted parking profile for the current soccer usage, the peak parking demand of 134 spaces only occurs during the overlap between the seniors games on a Saturday afternoon. We note that from the development of the parking demands that the seniors game numbers include 30 carparking spaces associated with 100 spectators. In our experience, the level of spectators can be highly variable from week to week depending on the clubs involved in each game. There is potential that this peak parking demands associated with full spectator numbers only occurs on a couple of Saturday afternoons per year. On this basis, it would not be an economical approach to build formal on-site carparking to meet the peak demand (with the spaces remaining unused for the majority of the year).

Whilst, a peak of 130 spaces is also predicted for the weeknight training, we note that a portion of these demands will be associated with parents directly dropping off or picking up children (i.e. not parking and waiting at the reserve). Furthermore, the start and finish times for training are typically less rigid than competition, leading to the potential for less overlap between the various training sessions. On this basis, the actual parking demands for weeknight training are expected to be lower.

The more typical parking demands expected range between 80-100 spaces. Our recommended approach is to provide formal parking for 100 spaces to meet the core parking demands. An informal overflow area could be provided on the reserve to cater for approximately 20 spaces, which would cover parking demands for all but the absolute peak (seniors soccer overlap). During the absolute peak a minor level of parking would occur on-street in the surrounding parking resources (discussed further in the section below).



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Figure 14: Recommended Parking Supply – Current Usage

Overflow Parking Opportunities:

Parking demands in the vicinity of the Kearney Drive Reserve are generally low, with large levels of available parking at all times surveyed.

However, given the residential nature of the surrounding area, there are often sensitivities to overflow parking occurring on-street in front of residential properties. On this basis, we recommend that overflow parking is encouraged to occur within the existing indented parking along Kerr Crescent which is located adjacent to Aspendale Primary School as follows:

- Kerr Crescent indented parking along frontage to Aspendale Primary School (44 spaces)
- Kerr Crescent indented parking along frontage to Kearney Drive Reserve (4 spaces).

A summary of the indented parking on Kerr Crescent is shown in Figure 15 below.



Source: Nearmap

Figure 15: Recommended Overflow Parking Opportunities

A review of the parking surveys undertaken for this study indicate the following peak levels of usage of the indented parking spaces on Kerr Crescent:

Table 8: Parking Demand vs Proposed On-Site Parking Supply

Day / Peak Time	Capacity	No. Cars Parked	Percentage Occupancy	Spaces Available
Tuesday 5pm	44	12	25%	36
Wednesday 6pm		28*	58%*	20*
Thursday 5pm		21	44%	27
Saturday 10am		10	21%	38
Sunday 11am		3	6%	45

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*Event being held at primary school.

The parking survey data indicates that the peak on-street demand of 16 spaces could generally be accommodated in the indented parking on Kerr Crescent (38-45 spaces available). During the weeknights when training occurs, the peak parking demand of up to 10 spaces could be accommodated in the indented parking on Kerr Crescent (20-36 spaces available). As noted previously, we don't expect the full parking demands to occur given a level of direct pick-up / drop-off activity (i.e. not all parents park for training sessions).

Management of Surrounding Residential Streets:

Whilst the above spaces are considered the most desirable to accommodate the overflow parking demands associated with the reserve, they are not necessarily the most proximate parking spaces.

In general, drivers will seek the most proximate spaces. In our experience with sporting grounds, the pavilion is often the centroid of activity and therefore the location of the pavilion can have a bearing on the location of parking. In this instance we are of the view that the parking Option 1 from Kerr Crescent is more desirable as this focuses activity towards the recommended overflow parking location on Kerr Crescent.

Council will also need to manage parking in local streets to focus parking on the desired areas. This would take the form of parking restrictions generally ranging from short term to No Stopping and Permit Zone restrictions.

5.2.3 Recommended Parking Provision – Future Usage

For the future usage, the number of junior soccer games is expected to increase with peak demands expected to increase to 159 spaces. the senior competition remaining the same as the current usage with a peak demand of 126 spaces.

Weekday training is expected to have a peak parking demand of 154 spaces, although similar to the current usage, we don't expect the full parking demands to occur given a level of direct pick-up / drop-off activity (i.e. not all parents park for training sessions).

Clearly these parking demands would extend beyond the recommended reserve parking provision for the existing conditions (100 formal spaces and 20 informal overflow spaces). The recommended approach for the future parking provision is to utilise the indented parking on Kerr Crescent and provide the balance of parking demands on the reserve as shown in Table 9 below

Table 9: Calculation of Reserve Parking Provision – Future Conditions

Day	Peak Demand	Available Spaces on Kerr Crescent	Balance of Parking to be Provided on Reserve
Saturday	126	38	88
Sunday	159	45	114
Training	154	20-36	118-134
Adopted Reserve Provision			120 spaces

The recommended reserve parking provision for the future conditions is 120 spaces. We note that during weeknight training, there may be isolated instances when the school is holding evening events. In these situations, the on-street parking would extend beyond the indented parking on Kerr

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Crescent. Although given the limited number of times per year that this would occur, it is considered to be appropriate.

A summary of the future parking demands and recommend parking provision is shown in Figure 16 below.

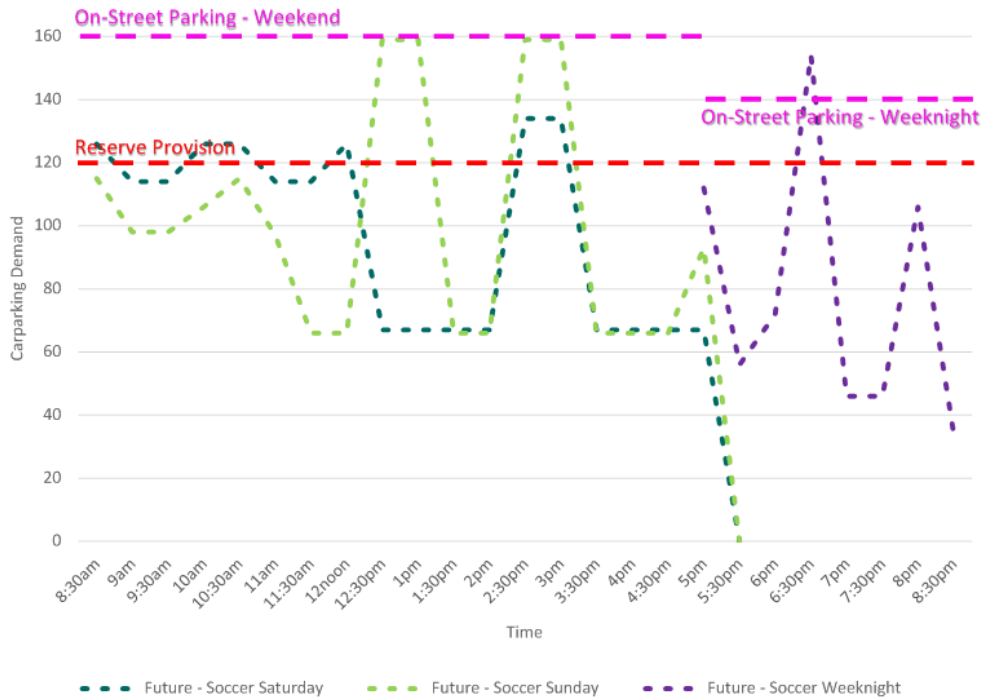


Figure 16: Recommended Parking Supply – Future Usage

5.3 Vehicle Access & Capacity

5.3.1 Vehicle Access & Carpark Layout

Council have developed a range of options for the layout of carparking on the subject site for consultation with the location community.

As described in the parking sections above, we are of the view that it is desirable to provide vehicle access via Kerr Crescent, to encourage any on-street parking to occur in the existing indented parking along Kerr Crescent. Furthermore, the provision of parking on the Kerr Crescent frontage will have a positive impact on existing school pick-up / drop-off by providing additional parking opportunities for parents.

We understand that Council would desirably like to have the pavilion in the centre of the 4 x sporting fields. On this basis, our recommended approach to the on-site parking is as follows:

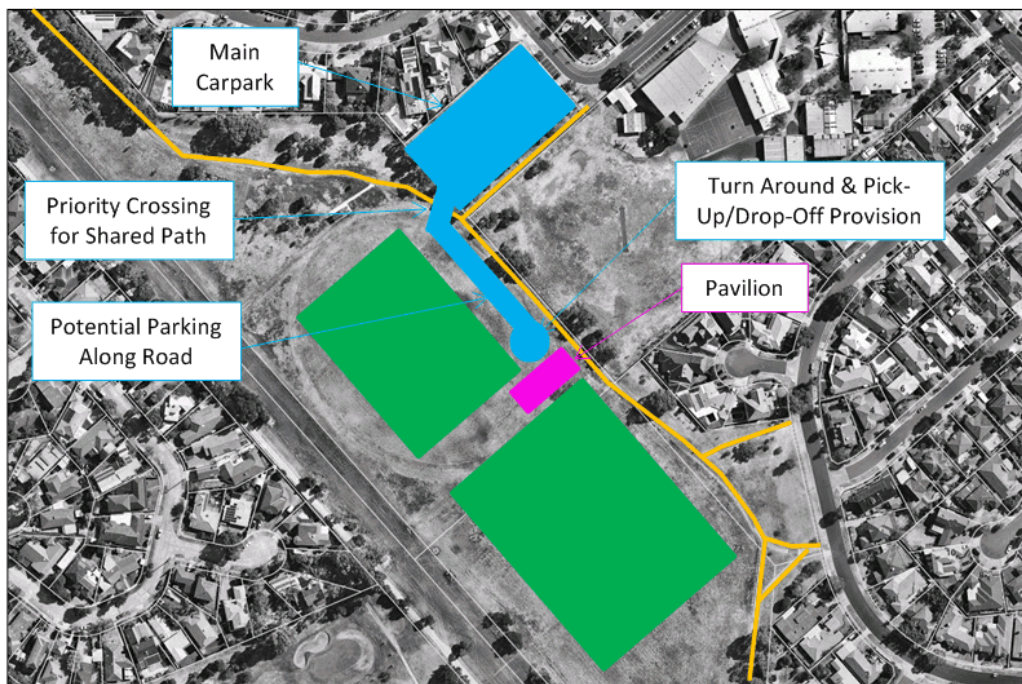
- Provision of the majority of formal carparking within large carpark adjacent to Kerr Crescent and Aspendale Gardens Primary School

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- Continuation of a road to the central pavilion location with a generous turn-around and drop-off area provided in the vicinity of the pavilion.
- Potential to include parking along the road to the pavilion (facing some of the sporting fields).
- Provision of a formal priority crossing point for the existing shared path through the reserve.
- Vehicles access to the on-site carparking would occur via an unsignalised access along the Kearney Drive Reserve frontage.
- Vehicle access could be limited to certain parts of the carpark outside of sporting activity to limit anti-social behaviour.

A summary of the recommended arrangement is shown in Figure 17 below.



Source: Nearmap

Figure 17: Recommend Carparking Layout

5.3.2 Traffic Generation

The volume of vehicles entering and exiting the reserve are based on the activity profiles presented previously, with two key traffic generating behaviours:

- **Drop-off / Pick Up:** Parents who drop off and pickup players at the start and end of their respective training session or game. These parents will generate both a trip into and out of the reserve at the start and end of junior training sessions and games.
- **Longer Term Demands:** Parents, Players, Spectators, Coaches and Officials who stay at the reserve for the entirety of their respective training session or game. These people will generate a trip into the reserve at the start of their respective training session or game and generate a trip out of the reserve at the end of their respective training session or game.

Traffic Engineering Assessment

Kearney Drive Reserve, Aspendale Gardens

Based on the activity profiles shown in Figure 10, a trip generation profile was developed and is attached in Appendix B. Figure 18 below summarises the expected hourly trips generated by a typical soccer training weeknight, Saturday game day and Sunday game day.

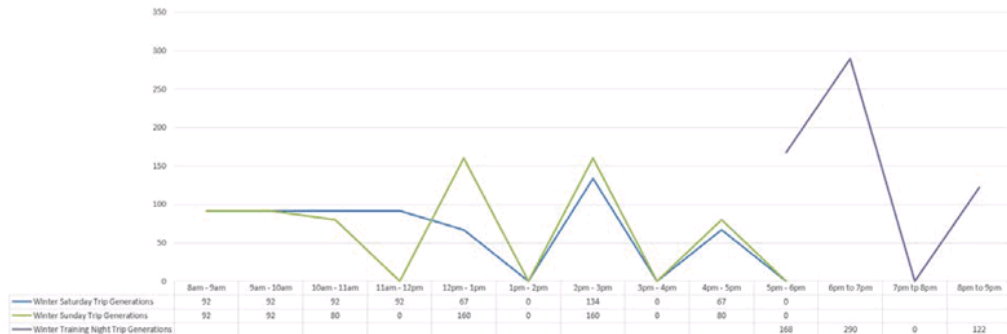


Figure 18: Soccer Trip Generations

The results indicate that the busiest time for vehicles entering and exiting the reserve occurs on a weeknight (training) between 6pm and 7pm. During this period U10s training sessions finish, U11-U18s training session finish and senior training starts. The predicted movements during this time period are as follows:

- 'In' Movements – 138 trips
- 'Out' Movements – 152 trips
- **TOTAL – 290 trips**

In the context of a local street such as Kerr Crescent, the proposed vehicle access point would have sufficient capacity.

5.3.3 Daily Traffic Volumes

The proposed sporting activity at Kearney Drive Reserve is expected to generate an additional 580-664 vehicles per day.

Whilst no traffic data is available for Kerr Crescent, historical traffic volume data supplied by council indicates that Kearney Drive carries up to 2,350 vehicles per day between Nurten Parade and Kerr Crescent. As Kearney Drive is a collector road, it is expected Kerr Crescent would carry less vehicles (estimated in the order of 2,000 vehicles per day).

In view of the above, Kerr Crescent would be expected to carry approximately 2,600vpd follow inclusion of the winter soccer activity. We are of the view that that level of daily traffic volume is appropriate for Kerr Crescent, particularly given that residential properties are only located on one side of the road.

Traffic Engineering Assessment
Kearney Drive Reserve, Aspendale Gardens

6 Conclusion

Having undertaken a detailed traffic engineering assessment of the proposed sporting uses for Kearney Drive Reserve, Aspendale Gardens, we are of the opinion that:

- a) The likely parking demands for the proposed sporting uses have been based on activity profiles for players, coaches, spectators and officials. The peak parking demands expected are:
 - i) Soccer matches (seniors) – 134 spaces
 - ii) Soccer matches (juniors) – 106 spaces
 - iii) Soccer training – 130 spaces
 - iv) Cricket matches – 72 spaces
- b) The parking demands are governed by the soccer activity, which has substantially higher parking demands than the cricket use (due to the ability to hold more simultaneous matches / training).
- c) Council have indicated that the junior soccer club could be expected to grow by 6-8 teams. For the purpose of this assessment we have adopted the upper growth level of 8 teams, spread across both the U10 and U11-18 components. The expected 'future' parking demands expected are as follows:
 - i) Soccer matches (juniors) – 159 spaces
 - ii) Soccer training – 154 spaces
- d) Council have prepared concept plans for two parking options providing between 70-81 parking spaces on-site. This provision is below the peak parking demands expected for the proposed sporting uses Kearney Drive Reserve. We would expect that parking demands would overflow into the surrounding local street during both for the senior and junior competition on Saturday and Sundays.
- e) It is recommended for the existing uses that 100 formal on-site carpark are provided to generally accommodate the core parking demands throughout the day. This would be supplemented by a 20 spaces informal overflow area on the reserve and some usage of the indented parking on Kerr Crescent to cater for peak 'overlap' demands.
- f) In the future with the expansion of the junior teams, parking demands are expected grow. In the 'future' scenario, it is recommended that the formal on-site parking provision is increased to 120 spaces, with the balance of demands to utilise the indented parking along Kerr Crescent.
- g) In order to focus overflow parking towards the desirable indented parking on Kerr Crescent, it is recommended at on-site parking and the sports pavilion is provided in the vicinity of Kerr Crescent. The design should incorporate a road to the pavilion with turn around and pick-up / drop-off facilities. Any crossings of the existing shared path should be provided in a formal priority crossing format. Council will also need to consider parking management (i.e. restrictions) to avoid parking in alternative more proximate locations that may be along residential frontages.
- h) A review of traffic generation indicates that:
 - i) Peak hour volumes are expected to occur on a training night between 6pm-7pm, when 138 'in' trips and 152 'out' trips are expected. In the context of a local street such as Kerr Crescent, the proposed vehicle access point would have sufficient capacity.

**Traffic Engineering Assessment**

Kearney Drive Reserve, Aspendale Gardens

- ii) The sporting activity is expected to generate an additional 580-664vpd. Whilst no existing traffic data is available for Kerr Crescent, we would expect it to carry approximately 2,600 vpd following inclusion of the winter soccer activity. We are of the view that that level of daily traffic volume is appropriate for Kerr Crescent, particularly given that residential properties are only located on one side of the road.

Traffic Engineering Assessment
Kearney Drive Reserve, Aspendale Gardens



Appendix A

Kearney Drive Reserve Parking Surveys

G25017R-01B



Keamey Drive Reserve
 Parking Surveys

Requested By: Scott Holden
 Surveyed By: Andrea Vignone

Survey Dates: 8 Three Surveys

Location	Restriction	Capacity Min - Max	Thursday 21st June 2018							Saturday 23rd June 2018							Sunday 24th June 2018							Tuesday 26th June 2018				Wednesday 27th June 2018							
			5 PM	6 PM	7 PM	8 PM	9 PM	9 AM	10 AM	11 AM	12 PM	1 PM	2 PM	3 PM	4 PM	5 PM	9 AM	10 AM	11 AM	12 PM	1 PM	2 PM	3 PM	4 PM	5 PM	5 PM	6 PM	7 PM	8 PM	9 PM	5 PM	6 PM	7 PM	8 PM	9 PM
ON-STREET CARPARKING (NORTH OF EDITHVALE WELANDS)																																			
Keamey Drive																																			
North Side																																			
Nurten Parade (east) to Nurten Parade (west)	Unrestricted	22	4	4	5	4	5	5	5	4	5	4	2	3	3	5	6	3	3	6	5	7	7	6	4	4	6	4	5	5	9	5	6	6	7
Nurten Parade (east) to Karielle Drive	Unrestricted - Marked Bays No stopping (08:30-11:30am & 3-4pm on School Days)	9	2	2	0	1	1	0	1	1	1	1	0	1	0	2	0	1	0	1	0	0	1	0	1	2	1	0	0	0	5	3	7	0	0
Karielle Drive to Jacqueline Drive	Unrestricted	15	4	5	5	5	5	4	4	2	2	3	3	4	4	4	4	4	6	5	5	4	4	3	4	4	4	4	5	5	4	4	5	4	4
Jacqueline Drive to Opposite Armer Court	Unrestricted	12	3	3	5	4	3	4	5	4	4	5	5	6	5	5	3	2	2	3	3	4	4	5	4	3	3	3	3	2	3	3	4	4	
Opposite Armer Court to Blanca Drive	Unrestricted	9	2	3	3	1	1	1	1	2	2	2	4	4	4	3	2	4	3	4	5	5	4	4	5	4	5	4	4	3	3	2	2	4	2
South Side																																			
Nurten Parade (east) to Nurten Parade (west)	Unrestricted	17	2	3	5	2	2	6	5	5	4	5	4	4	4	7	3	4	4	4	3	4	4	3	2	3	5	7	7	8	4	4	4	5	
Nurten Parade (east) to Kerr Crescent	Unrestricted - Marked Bays No stopping (08:30-11:30am & 3-4pm on School Days)	10	4	3	3	1	1	2	2	2	2	1	1	2	1	1	0	0	0	0	1	0	0	0	0	1	0	0	0	0	7	10	6	1	0
Kerr Crescent (east) to Kerr Crescent (west)	Unrestricted	14	1	1	3	1	1	1	1	1	1	1	1	0	0	1	0	0	1	0	0	1	0	0	1	2	2	2	2	2	3	2	2	2	
Kerr Crescent (west) to Armer Court	Unrestricted	12	0	1	2	1	1	1	1	1	1	1	1	2	1	1	1	1	1	1	1	1	1	1	1	1	1	1	0	2	2	2	3	3	
Armer Court to Blanca Drive	Unrestricted	16	1	3	3	2	3	5	4	5	3	2	1	3	3	4	3	3	3	4	4	5	5	3	4	1	3	3	3	4	0	2	2	3	3
Keamey Drive	Capacity	143 - 143	143	143	143	143	143	143	143	143	143	143	143	143	143	143	143	143	143	143	143	143	143	143	143	143	143	143	143	143	143	143	143	143	143
	Total Number of Cars Parked		23	24	28	22	23	31	30	27	25	27	22	23	24	32	24	21	24	23	27	29	30	26	23	27	29	28	29	30	30	40	31	30	
	Total Number of Vacant Spaces		120	119	117	121	120	112	113	116	114	116	121	116	119	111	119	122	119	114	116	114	113	117	118	114	114	115	114	113	105	104	103	112	113
	Percentage Occupancy		16%	17%	18%	16%	16%	22%	21%	19%	17%	19%	16%	20%	17%	22%	17%	15%	17%	20%	19%	20%	21%	18%	17%	19%	20%	20%	20%	20%	27%	27%	28%	22%	21%
Nurten Parade																																			
North-East Side																																			
Keamey Drive to Lorna Court	Unrestricted	7	2	4	3	4	3	5	6	6	4	4	3	3	3	4	4	4	3	3	4	4	4	4	5	6	4	5	5	4	3	5	4	4	
Lorna Court to Opposite June Place	Unrestricted	27	2	3	2	3	3	5	4	3	3	1	1	3	4	4	3	3	2	3	5	3	3	4	3	2	2	3	3	3	2	3	3	2	4
Opposite June Place to Nurten Drive	Unrestricted	16	2	2	4	4	3	2	2	1	1	1	0	0	1	1	2	2	4	3	2	2	3	1	1	2	2	4	2	3	3	3	4	2	2
South-West Side																																			
Keamey Drive to Opposite Lorna Court	Unrestricted	8	2	4	5	3	4	4	3	3	4	4	2	2	2	2	2	2	2	2	3	3	2	1	1	2	3	5	5	5	0	2	4	4	
Opposite Lorna Court to June Place	Unrestricted	20	1	1	0	0	3	3	1	0	1	1	1	1	0	0	0	0	0	0	0	0	0	0	0	1	0	0	0	1	2	1	0	0	
June Place to Keamey Drive	Unrestricted	17	2	3	4	4	4	4	4	4	3	3	3	3	3	2	3	1	2	2	3	3	3	4	3	4	2	3	4	4	3	4	5	3	
Nurten Parade	Capacity	104 - 104	104	104	104	104	104	104	104	104	104	104	104	104	104	104	104	104	104	104	104	104	104	104	104	104	104	104	104	104	104	104	104	104	104
	Total Number of Vacant Spaces		93	87	81	86	84	83	84	87	88	90	94	94	91	92	93	92	90	91	88	89	89	90	91	89	87	86	86	84	83	81	81	80	
	Percentage Occupancy		31%	38%	30%	37%	35%	30%	39%	36%	35%	33%	33%	33%	33%	33%	33%	33%	33%	33%	33%	33%	33%	33%	33%	33%	33%	33%	33%	33%	33%	33%	33%	33%	33%

Fields: 100-metre radius property boundary
 200m: 100-metre radius property boundary
 Major: Major roads
 Other: Other roads

Keamey Drive Reserve
Parking Surveys



Prepared By: Jason Hedges
Reviewed By: Andrew Vongpanich
Survey Dates: 8 Weeks Surveys

Location	Restriction	Capacity Min - Max	Thursday 21st June 2018					Saturday 23rd June 2018					Sunday 24th June 2018					Tuesday 26th June 2018					Wednesday 27th June 2018															
			5 PM	6 PM	7 PM	8 PM	9 PM	9 AM	10 AM	11 AM	12 PM	1 PM	2 PM	3 PM	4 PM	5 PM	9 AM	10 AM	11 AM	12 PM	1 PM	2 PM	3 PM	4 PM	5 PM	5 PM	6 PM	7 PM	8 PM	9 PM	5 PM	6 PM	7 PM	8 PM	9 PM			
Derrick Close																																						
North East Side																																						
Nurse Parade to NB #12	Unrestricted	9	1	1	2	3	3	3	3	4	2	3	3	3	3	3	3	4	3	4	4	3	4	4	3	3	1	2	4	3	3	2	2	3	5	3		
South West Side																																						
Nurse Parade to NB #12	Unrestricted	9	5	4	4	4	3	2	1	4	3	2	2	2	1	0	0	0	1	1	2	2	2	2	2	4	2	2	1	1	4	4	3	3	2			
	Capacity	18-18	18	18	18	18	18	18	18	18	18	18	18	18	18	18	18	18	18	18	18	18	18	18	18	18	18	18	18	18	18	18	18	18	18	18		
	Total Number of Cars Parked		6	5	6	7	6	4	5	5	5	6	5	5	5	4	4	5	4	5	4	6	6	5	5	5	4	6	4	4	6	6	6	6	7			
	Total Number of Vacant Spaces		12	13	12	11	12	14	13	13	13	13	13	13	13	14	14	13	14	13	14	12	12	13	13	13	14	12	14	14	12	12	12	12	11			
	Percentage Occupancy		33%	28%	33%	39%	33%	22%	28%	28%	28%	28%	31%	28%	28%	22%	22%	17%	22%	28%	22%	33%	33%	28%	28%	22%	33%	22%	22%	33%	33%	33%	33%	33%	33%			
Lorna Court																																						
North East Side																																						
Nurse Parade to NB #12	Unrestricted	15	7	6	6	6	6	5	5	6	6	6	6	5	6	5	6	6	6	4	4	4	4	3	4	3	6	5	5	6	4	5	5	6	4	5	7	8
South West Side																																						
Nurse Parade to NB #12	Unrestricted	24	1	3	1	1	4	3	3	3	4	3	2	2	2	5	2	1	1	2	2	1	2	3	2	3	2	2	2	2	2	2	2	2	2	2	2	3
	Capacity	23-23	23	23	23	23	23	23	23	23	23	23	23	23	23	23	23	23	23	23	23	23	23	23	23	23	23	23	23	23	23	23	23	23	23	23	23	23
	Total Number of Cars Parked		8	7	7	7	9	8	8	9	8	8	7	8	10	5	5	5	6	5	6	5	7	7	6	5	6	7	7	8	8	8	7	9	11			
	Total Number of Vacant Spaces		21	22	22	22	20	21	21	20	20	20	21	22	21	19	24	24	24	24	23	24	23	24	22	22	23	24	21	20	22	21	21	21	22	20	18	
	Percentage Occupancy		28%	24%	24%	24%	31%	28%	28%	31%	31%	31%	28%	24%	28%	24%	24%	37%	17%	17%	21%	17%	21%	17%	21%	17%	24%	24%	21%	17%	21%	24%	24%	28%	28%	24%	31%	
Atherton Close																																						
North West Side																																						
Nurse Parade to NB #22	Unrestricted	14	2	2	2	2	2	3	3	3	4	3	3	3	3	3	3	3	3	3	3	2	2	2	2	2	2	2	2	2	2	2	2	2	2	2	3	
South East Side																																						
	Unrestricted	14	0	0	1	1	1	2	2	2	2	1	2	1	1	2	2	3	3	4	4	3	3	2	1	0	1	1	1	1	2	0	1	3	1	2		
	Capacity	23-23	23	23	23	23	23	23	23	23	23	23	23	23	23	23	23	23	23	23	23	23	23	23	23	23	23	23	23	23	23	23	23	23	23	23	23	
	Total Number of Cars Parked		2	2	3	3	3	5	5	5	5	4	5	4	4	5	5	6	6	7	6	5	5	4	3	1	3	3	3	4	2	3	5	4	5			
	Total Number of Vacant Spaces		26	26	25	25	25	23	23	23	23	24	23	24	24	24	23	22	22	21	22	23	25	24	25	27	25	25	25	24	26	25	25	24	23	23		
	Percentage Occupancy		7%	7%	11%	11%	11%	18%	18%	18%	18%	14%	18%	14%	14%	18%	18%	21%	21%	22%	21%	18%	18%	14%	11%	4%	13%	11%	11%	14%	7%	11%	14%	14%	18%			
Carole Court																																						
North West Side																																						
Nurse Parade to NB #12	Unrestricted	8	2	2	1	1	1	3	1	1	2	2	2	2	2	2	2	2	2	2	2	2	2	2	2	2	2	2	2	2	2	2	2	2	2	2		
South East Side																																						
Nurse Parade to NB #12	Unrestricted	7	1	1	1	1	2	1	1	2	2	1	1	2	2	2	2	2	2	3	2	3	0	1	1	2	2	2	2	2	2	2	2	2	2	2		
	Capacity	13-13	13	13	13	13	13	13	13	13	13	13	13	13	13	13	13	13	13	13	13	13	13	13	13	13	13	13	13	13	13	13	13	13	13	13		
	Total Number of Cars Parked		3	3	2	2	3	4	4	3	3	3	4	4	3	4	4	4	4	5	4	4	4	5	4	5	2	5	4	4	5	5	5	4	3	5		
	Total Number of Vacant Spaces		12	12	13	13	12	11	13	12	12	12	11	11	12	11	11	11	10	11	12	13	12	11	11	12	12	12	12	12	11	12	12	12	12	11		
	Percentage Occupancy		26%	26%	19%	19%	28%	37%	27%	27%	28%	26%	28%	27%	27%	27%	27%	27%	27%	27%	27%	27%	27%	27%	27%	27%	27%	27%	27%	27%	27%	27%	27%	27%	27%	27%		

All Site Information is Property Boundary
 DWG: - Equipment to Property Boundary
 N/A: - Not Applicable
 * - Not to Scale

Keamey Drive Reserve
Parking Surveys



Requested By: Neil Holton
Surveyed By: Andrea Vignone

Survey Dates: 8 Times Between

Location	Restriction	Capacity Min - Max	Thursday 21st June 2018							Saturday 23rd June 2018							Sunday 24th June 2018							Tuesday 26th June 2018					Wednesday 27th June 2018															
			5 PM	6 PM	7 PM	8 PM	9 PM	9 AM	10 AM	11 AM	12 PM	1 PM	2 PM	3 PM	4 PM	5 PM	9 AM	10 AM	11 AM	12 PM	1 PM	2 PM	3 PM	4 PM	5 PM	9 PM	6 PM	7 PM	8 PM	9 PM	5 PM	6 PM	7 PM	8 PM	9 PM									
Cunningham Close																																												
North West Side																																												
Nurton Parade to NB #21	Unrestricted	10	4	4	5	6	6	5	5	6	6	5	7	6	5	6	7	6	6	6	7	6	6	6	7	6	6	6	8	7	7	7	8	8	4	4	4	6	6	5	6	5	7	7
South East Side																																												
Nurton Parade to NB #21	Unrestricted	33	5	5	4	4	4	2	3	2	1	6	4	3	3	3	6	6	4	4	4	4	3	5	5	5	5	2	4	5	5	3	3	5	5	3	3	5	5	5				
	Capacity	52 - 52	52	52	52	52	52	52	52	52	52	52	52	52	52	52	52	52	52	52	52	52	52	52	52	52	52	52	52	52	52	52	52	52	52	52	52	52	52	52	52	52	52	52
	Total Number of Cars Parked		9	9	10	10	10	8	9	8	8	13	10	9	9	10	12	12	10	12	11	11	10	15	13	5	6	8	11	11	8	9	10	12	12									
	Total Number of Vacant Spaces		23	23	23	22	22	24	23	24	24	19	22	23	23	20	20	22	20	21	21	22	19	19	27	36	24	21	21	24	23	22	20	20										
	Percentage Occupancy		28%	28%	31%	31%	31%	25%	28%	25%	25%	41%	31%	20%	28%	31%	30%	38%	31%	30%	34%	34%	31%	41%	41%	16%	16%	25%	34%	34%	25%	28%	31%	30%	38%									
June Place																																												
North Side																																												
Nurton Parade to NB #4	Unrestricted	3	0	0	3	1	1	1	1	1	1	1	1	1	2	3	1	1	1	3	1	1	2	2	3	1	1	2	3	3	3	3	3	3	3	3	3	3	3	3	3			
South Side																																												
Nurton Parade to NB #4	Unrestricted	4	1	1	0	0	0	2	2	2	1	1	1	1	2	2	2	2	2	2	2	2	2	1	3	1	1	1	1	1	1	1	1	1	1	1	1	1	1	1	1	1		
	Capacity	7 - 7	7	7	7	7	7	7	7	7	7	7	7	7	7	7	7	7	7	7	7	7	7	7	7	7	7	7	7	7	7	7	7	7	7	7	7	7	7	7	7	7		
	Total Number of Cars Parked		1	1	1	1	1	3	3	3	2	2	2	2	4	5	3	3	3	3	3	3	4	3	4	3	4	3	4	3	3	1	1	1	1	1	1	1	1	1	1	1		
	Total Number of Vacant Spaces		6	6	6	6	6	4	4	4	5	5	5	5	3	2	4	4	4	4	4	4	4	4	4	4	4	4	4	4	4	4	4	4	4	4	4	4	4	4	4	4	4	
	Percentage Occupancy		14%	14%	14%	14%	14%	43%	43%	43%	29%	29%	29%	29%	57%	71%	43%	43%	43%	43%	43%	43%	43%	43%	43%	43%	43%	43%	43%	43%	43%	43%	43%	43%	43%	43%	43%	43%	43%	43%	43%	43%	43%	
Kerr Crescent																																												
North Side																																												
Keamey Drive to Keamey Drive	Unrestricted - Marked Bays	9	2	3	3	3	3	3	3	2	2	1	1	0	0	0	2	1	1	2	2	2	1	1	0	0	2	4	2	2	2	4	6	6	6	6	6	6	6	6	6	6		
	Unrestricted	6	0	0	1	1	1	2	2	2	1	1	1	1	1	1	1	1	1	1	1	1	1	1	1	1	1	1	1	1	1	1	1	1	1	1	1	1	1	1	1	1		
South Side																																												
Keamey Drive to Keamey Drive	Unrestricted - Marked Bays	22	10	8	1	0	0	2	6	3	1	2	2	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
	No Parking (8-10:30am & 2:30-4pm on School Days)	4	4	4	3	0	0	3	2	3	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
	Unrestricted - Marked Bays	18	6	3	2	1	1	4	2	1	1	1	2	1	1	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
	Unrestricted - Marked Bays	4	1	0	0	0	0	0	0	1	1	0	0	0	0	0	0	2	3	2	2	1	1	2	1	2	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
	Unrestricted	7	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
	Capacity	70 - 70	70	70	70	70	70	70	70	70	70	70	70	70	70	70	70	70	70	70	70	70	70	70	70	70	70	70	70	70	70	70	70	70	70	70	70	70	70	70	70	70	70	70
	Total Number of Cars Parked		25	18	8	5	5	14	15	12	6	5	5	3	2	2	7	7	8	9	11	10	10	7	4	14	15	17	9	9	18	35	34	19	19									
	Total Number of Vacant Spaces		47	54	62	65	65	56	55	58	64	65	65	67	68	68	63	63	62	61	59	60	60	63	66	56	55	53	67	67	52	37	36	51	51									
	Percentage Occupancy		36%	26%	11%	7%	7%	20%	21%	17%	9%	7%	7%	4%	3%	3%	10%	10%	11%	13%	16%	14%	14%	10%	8%	20%	21%	24%	4%	4%	26%	47%	48%	27%	27%									

North - Northmeads/Whangarei boundary
South - Eastmeads/Whangarei boundary
Major - Major
Other - Other

Prepared by T raffix Group Pty Ltd

Page 3 of 8



Keamey Drive Reserve
Parking Surveys

Requested By: Scott Holden
Surveyed By: Andrea Vignone
Survey Dates: 8 Three Surveys

Location	Restriction	Capacity Min - Max	Thursday 21st June 2018							Saturday 23rd June 2018							Sunday 24th June 2018							Tuesday 26th June 2018					Wednesday 27th June 2018							
			5 PM	6 PM	7 PM	8 PM	9 PM	9 AM	10 AM	11 AM	12 PM	1 PM	2 PM	3 PM	4 PM	5 PM	9 AM	10 AM	11 AM	12 PM	1 PM	2 PM	3 PM	4 PM	5 PM	5 PM	6 PM	7 PM	8 PM	9 PM	5 PM	6 PM	7 PM	8 PM	9 PM	
West Side																																				
Keamey Drive to Bee Ting Court	Unrestricted	10	2	2	3	1	1	2	2	3	3	4	4	4	4	2	1	2	2	2	2	0	1	1	2	2	2	3	3	2	3	4	2	3	2	2
East Side																																				
Keamey Drive to Opposite Bee Ting Court	Unrestricted	16	1	3	3	3	3	0	0	0	0	0	0	0	1	1	1	2	2	1	1	0	1	1	0	1	1	1	1	1	1	0	0	0	0	
	Capacity	52 - 52	52	52	52	52	52	52	52	52	52	52	52	52	52	52	52	52	52	52	52	52	52	52	52	52	52	52	52	52	52	52	52	52	52	
	Total Number of Cars Parked	5	5	6	4	4	2	2	3	3	4	4	4	4	5	2	3	4	4	4	1	2	1	3	3	2	4	4	3	4	5	2	3	2	2	
	Total Number of Vacant Spaces	27	27	26	26	28	30	30	29	29	28	28	28	28	27	29	29	28	28	28	31	30	31	29	29	30	28	29	29	28	27	30	29	30	30	
	Percentage Occupancy	16%	16%	19%	13%	13%	6%	6%	6%	6%	9%	13%	13%	13%	8%	6%	6%	13%	13%	13%	6%	9%	6%	9%	6%	13%	13%	9%	13%	16%	6%	6%	6%	6%		
Site Ting Court																																				
North West Side																																				
Keamey Drive to SB 810	Unrestricted	14	2	3	3	1	1	1	2	1	0	0	1	2	1	2	2	1	2	3	2	2	1	3	2	2	3	1	2	2	3	3	3	3	3	
South East Side																																				
Keamey Drive to SB 810	Unrestricted	25	3	6	6	7	7	5	5	5	4	4	4	4	4	3	3	3	6	4	5	3	4	4	4	4	4	4	5	4	4	4	4	5		
	Capacity	29 - 29	29	29	29	29	29	29	29	29	29	29	29	29	29	29	29	29	29	29	29	29	29	29	29	29	29	29	29	29	29	29	29	29	29	
	Total Number of Cars Parked	5	7	9	8	8	6	7	6	4	4	5	6	5	6	5	4	5	9	6	7	6	7	6	6	7	5	6	7	7	7	7	7	8		
	Total Number of Vacant Spaces	24	22	20	21	21	23	22	23	25	25	24	23	24	23	24	25	24	20	23	22	23	22	23	23	23	24	23	22	22	22	22	22	21		
	Percentage Occupancy	17%	24%	31%	28%	28%	21%	24%	21%	14%	14%	17%	21%	17%	21%	14%	17%	35%	21%	24%	23%	24%	21%	21%	21%	24%	17%	21%	24%	24%	24%	24%	24%	28%		
SUMMARY -- ON STREET CARPARKING (NORTH OF EDITHVALE WETLANDS)																																				
Car Parking Supply		502 - 502	502	502	502	502	502	502	502	502	502	502	502	502	502	502	502	502	502	502	502	502	502	502	502	502	502	502	502	502	502	502	502	502	502	
Total Number of Cars Parked		109	108	110	109	107	121	126	113	102	105	95	97	95	111	103	97	103	117	110	111	108	115	116	97	109	118	108	114	110	110	102	116	137		
Total Number of Vacant Spaces		473	474	472	462	475	461	458	469	489	477	487	493	497	471	479	485	479	465	472	471	474	467	486	485	473	464	474	468	452	431	420	440	445		
Percentage Occupancy		19%	19%	19%	17%	18%	21%	21%	15%	18%	18%	16%	17%	16%	19%	18%	17%	18%	20%	19%	19%	19%	20%	17%	19%	20%	19%	20%	19%	20%	22%	26%	28%	23%	24%	
ON-STREET CARPARKING SOUTH OF EDITHVALE WETLANDS																																				

Red: Metropolitan Property Boundary
Green: Local Government Property Boundary
Blue: Major Road
Grey: Right of Way

Keamey Drive Reserve
Parking Surveys



Prepared By: Scott Hedges
Reviewed By: Andrew Vorpersen
Survey Dates: 8 Weeks Surveys

Location	Restriction	Capacity Min - Max	Thursday 21st June 2018							Saturday 23rd June 2018							Sunday 24th June 2018							Tuesday 26th June 2018					Wednesday 27th June 2018							
			5 PM	6 PM	7 PM	8 PM	9 PM	9 AM	10 AM	11 AM	12 PM	1 PM	2 PM	3 PM	4 PM	5 PM	9 AM	10 AM	11 AM	12 PM	1 PM	2 PM	3 PM	4 PM	5 PM	5 PM	6 PM	7 PM	8 PM	9 PM	5 PM	6 PM	7 PM	8 PM	9 PM	
Tarango Drive																																				
North Side																																				
Niranga Avenue to Beqa Court	Unrestricted	3	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Beqa Court to Opposite Tyabb Grove	Unrestricted	25	4	2	5	2	2	2	3	3	4	4	5	4	4	3	4	4	2	2	2	5	4	7	5	3	3	4	4	2	4	5	6	6	4	
Tyabb Grove to Birch Court	Unrestricted	23	4	6	6	8	7	6	6	3	3	6	9	10	9	8	9	9	10	11	9	8	9	8	2	3	4	3	5	5	6	6	7	6		
South Side																																				
Niranga Avenue to Opposite Beqa Court	Unrestricted	7	1	1	1	1	1	1	1	2	0	0	0	0	0	0	0	0	0	0	0	0	0	1	1	1	1	1	1	1	1	1	1	1	1	
Opposite Beqa Court to Tyabb Grove	Unrestricted	19	3	2	3	4	4	7	5	5	4	4	5	6	6	3	3	2	2	3	2	7	10	5	6	6	6	7	5	4	3	3	3	3		
Tyabb Grove to Opposite Birch Court	Unrestricted	22	1	2	2	2	2	3	3	2	1	1	1	1	3	2	2	2	2	2	3	3	3	2	2	1	3	1	1	2	2	2	2	2		
Tarango Drive	Capacity	103 - 103	103	103	103	103	103	103	103	103	103	103	103	103	103	103	103	103	103	103	103	103	103	103	103	103	103	103	103	103	103	103	103	103		
	Total Number of Cars Parked		13	13	17	17	16	18	18	15	12	18	19	20	19	18	18	15	16	18	19	20	22	24	15	15	16	18	16	15	16	18	19	16		
	Total Number of Vacant Spaces		90	90	86	86	87	85	85	88	91	85	84	83	84	85	85	88	87	85	84	77	71	79	88	88	87	85	87	88	87	85	84	87		
	Percentage Occupancy		13%	13%	17%	17%	16%	17%	17%	15%	12%	17%	18%	19%	18%	17%	17%	16%	16%	17%	18%	19%	20%	22%	15%	15%	16%	17%	16%	15%	16%	17%	18%	16%		
Tyabb Grove																																				
West Side																																				
Tarango Drive to Haka Avenue	Unrestricted	34	5	6	5	5	5	3	2	2	4	5	5	5	5	8	5	5	4	6	6	6	1	1	2	1	0	1	0	0	1	1	6	6	8	
East Side																																				
Tarango Drive to Haka Avenue	Unrestricted	10	1	1	1	1	1	4	3	1	1	0	0	1	1	1	0	0	0	0	1	1	3	5	5	6	5	6	6	6	6	6	1	1	1	
Tyabb Grove	Capacity	24 - 24	24	24	24	24	24	24	24	24	24	24	24	24	24	24	24	24	24	24	24	24	24	24	24	24	24	24	24	24	24	24	24	24	24	
	Total Number of Cars Parked		4	7	6	6	6	5	5	3	5	5	5	6	6	9	5	5	4	6	7	7	6	6	6	6	6	6	6	7	7	7	7	8		
	Total Number of Vacant Spaces		18	17	18	18	18	19	19	21	19	19	18	18	18	15	19	19	20	18	17	17	18	18	18	18	18	18	18	17	17	17	17	15		
	Percentage Occupancy		25%	29%	25%	25%	25%	21%	21%	21%	19%	21%	21%	21%	25%	25%	28%	21%	21%	17%	25%	29%	29%	25%	25%	25%	25%	25%	25%	25%	25%	25%	29%	29%	28%	
Haka Avenue																																				
North Side																																				
Tarango Drive to Tyabb Grove	Unrestricted	23	6	5	5	5	5	2	3	3	3	3	4	4	6	7	6	5	5	5	10	10	9	8	5	4	3	6	6	6	6	6	7	7		
Tyabb Grove to Opposite Birch Court	Unrestricted	8	2	4	4	6	7	7	9	10	7	8	9	9	5	5	3	3	3	3	4	4	3	6	5	2	4	5	6	5	5	5	7	7	5	
South Side																																				
Tarango Drive to Opposite Tyabb Grove	Unrestricted	27	1	3	3	3	3	1	1	3	4	4	2	3	3	3	1	0	0	4	5	8	3	3	3	4	3	3	1	4	3	3	1	1		
Tyabb Grove to Birch Court	Unrestricted	18	8	10	13	10	11	16	13	15	16	17	14	11	9	10	9	9	9	10	8	11	10	9	7	6	7	8	8	11	11	10	10	12		
Haka Avenue	Capacity	76 - 76	76	76	76	76	76	76	76	76	76	76	76	76	76	76	76	76	76	76	76	76	76	76	76	76	76	76	76	76	76	76	76	76		
	Total Number of Cars Parked		18	22	23	24	26	28	28	31	30	32	28	24	23	25	19	18	19	17	28	27	33	27	22	17	17	21	23	20	28	25	28	25	25	
	Total Number of Vacant Spaces		58	54	53	52	50	50	50	45	46	44	52	52	53	57	57	57	59	48	48	43	49	54	59	59	55	53	56	50	51	48	51	51		
	Percentage Occupancy		24%	29%	30%	32%	34%	38%	38%	41%	41%	42%	38%	32%	30%	31%	25%	24%	25%	22%	37%	36%	43%	36%	29%	22%	22%	28%	30%	26%	38%	33%	37%	33%	33%	

All data is representative of typical weekday
traffic. Equipment may vary depending on
traffic volume. Report prepared by
Traffic Group Pty Ltd.



Keamey Drive Reserve
Parking Surveys

Requested By: Neil Holden
Surveyed By: Andrea Vignone

Survey Dates: 8 Times Surveys

Location	Restriction	Capacity Min - Max	Thursday 21st June 2018					Saturday 23rd June 2018					Sunday 24th June 2018					Tuesday 26th June 2018					Wednesday 27th June 2018												
			5 PM	6 PM	7 PM	8 PM	9 PM	9 AM	10 AM	11 AM	12 PM	1 PM	2 PM	3 PM	4 PM	5 PM	9 AM	10 AM	11 AM	12 PM	1 PM	2 PM	3 PM	4 PM	5 PM	5 PM	6 PM	7 PM	8 PM	9 PM	5 PM	6 PM	7 PM	8 PM	9 PM
Wanda Court																																			
West Side																																			
Joka Avenue to WD #4	Unrestricted	4	1	1	1	1	1	0	1	1	1	1	1	1	1	0	0	0	0	0	0	0	2	2	3	2	1	2	2	0	2	2	2		
East Side																																			
Joka Avenue to WD #4	Unrestricted	7	0	0	0	0	0	1	1	1	1	2	0	0	1	1	1	1	0	0	2	3	1	1	1	1	1	1	1	0	0	0	1		
	Capacity	11-11	11	11	11	11	11	11	11	11	11	11	11	11	11	11	11	11	11	11	11	11	11	11	11	11	11	11	11	11	11	11			
	Total Number of Cars Parked		1	1	1	1	1	1	2	2	2	3	1	1	2	2	1	1	0	0	2	1	1	1	3	3	4	5	2	3	2	2	3		
	Total Number of Vacant Spaces		10	10	10	10	10	10	9	9	9	8	10	10	9	9	10	10	11	11	9	10	10	10	8	8	7	8	9	11	9	9	8		
	Percentage Occupancy		9%	9%	9%	9%	9%	9%	18%	18%	18%	27%	9%	9%	18%	18%	9%	9%	0%	0%	18%	9%	9%	9%	27%	27%	36%	27%	18%	27%	18%	0%	18%	18%	27%
Kwena Court																																			
West Side																																			
Joka Avenue to WD #3	Unrestricted	3	0	0	0	1	1	0	0	0	0	0	0	0	0	0	0	0	0	1	0	0	0	0	0	0	0	1	1	1	1	1			
East Side																																			
Joka Avenue to WD #3	Unrestricted	3	0	1	1	1	1	0	0	0	1	1	0	0	2	0	0	0	0	0	0	0	0	0	0	0	1	0	0	0	0	0	0		
	Capacity	0-9	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0		
	Total Number of Cars Parked		0	1	1	2	2	0	0	0	1	1	0	0	2	0	0	0	0	1	0	0	0	0	0	0	2	0	0	0	1	1	1	1	
	Total Number of Vacant Spaces		3	3	3	3	3	3	3	3	3	3	3	3	3	3	3	3	3	3	3	3	3	3	3	3	3	3	3	3	3	3	3	3	
	Percentage Occupancy		0%	33%	33%	66%	66%	0%	0%	0%	33%	33%	0%	0%	66%	66%	0%	0%	0%	33%	0%	0%	0%	0%	0%	0%	66%	0%	0%	0%	33%	33%	33%	33%	
Ulrah Court																																			
North Side																																			
Joka Avenue to ND #5	Unrestricted	5	2	1	1	1	1	2	1	1	1	1	1	1	1	1	1	1	1	1	1	1	1	1	1	1	1	1	1	1	1	1	1		
South Side																																			
Joka Avenue to ND #5	Unrestricted	4	0	0	0	1	1	0	0	0	0	0	0	0	0	1	1	1	1	1	1	1	1	1	1	1	1	1	1	1	1	1	1	1	
	Capacity	0-9	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
	Total Number of Cars Parked		2	1	1	2	2	1	1	1	1	1	1	1	1	2	2	2	2	2	2	2	2	2	2	2	2	2	2	2	2	2	2	2	
	Total Number of Vacant Spaces		3	3	3	3	3	3	3	3	3	3	3	3	3	3	3	3	3	3	3	3	3	3	3	3	3	3	3	3	3	3	3	3	
	Percentage Occupancy		22%	11%	11%	22%	22%	11%	11%	11%	11%	11%	11%	11%	11%	22%	22%	22%	22%	22%	22%	22%	22%	22%	22%	22%	22%	22%	22%	22%	22%	22%	22%	22%	

NA: Non-mechanised Property Boundary
 GB: Green Boundary
 M: Major Road
 CR: Cycle Route

Traffic Engineering Assessment
Kearney Drive Reserve, Aspendale Gardens



Appendix B Activity Profile Trip Generations

G25017R-01B



G25017: Kearney Drive Reserve - Traffic Generation

		Weeknight Training								Saturday Competition													
		9:00am	1:00pm	5:00pm	6:00pm	9:00am	1:00pm	5:00pm	6:00pm	9:00am	1:00pm	5:00pm	6:00pm	9:00am	1:00pm	5:00pm	6:00pm	9:00am	1:00pm	5:00pm	6:00pm	Total	
Field 1	Activity	0.01 Training - 4 Goals								0.01 Training - 4 Goals													
	Pages In	14	6							14	6												30
	Pages Out	6	14							6	14												30
	Activity	0.01 Training - 4 Goals								0.01 Training - 4 Goals													
	Pages In	14	6							14	6												30
Field 2	Activity	0.01 Training - 4 Goals								0.01 Training - 4 Goals													
	Pages In	14	6							14	6												30
	Pages Out	6	14							6	14												30
	Activity	0.01 Training - 4 Goals								0.01 Training - 4 Goals													
	Pages In	14	6							14	6												30
Field 3	Activity	0.01 Training - 4 Goals								0.01 Training - 4 Goals													
	Pages In	14	6							14	6												30
	Pages Out	6	14							6	14												30
	Activity	0.01 Training - 4 Goals								0.01 Training - 4 Goals													
	Pages In	14	6							14	6												30
Field 4	Activity	0.01 Training - 4 Goals								0.01 Training - 4 Goals													
	Pages In	14	6							14	6												30
	Pages Out	6	14							6	14												30
	Activity	0.01 Training - 4 Goals								0.01 Training - 4 Goals													
	Pages In	14	6							14	6												30
Total Pages In		112								112													450
Total Pages Out		48								48													198
Total Pages		64								64													252

		Saturday Competition								Saturday Competition																
		9:00am	10:00am	11:00am	12:00pm	1:00pm	2:00pm	3:00pm	4:00pm	9:00am	10:00am	11:00am	12:00pm	1:00pm	2:00pm	3:00pm	4:00pm	9:00am	10:00am	11:00am	12:00pm	1:00pm	2:00pm	3:00pm	4:00pm	Total
Field 1	Activity	0.01 Competition - 1 Goals								0.01 Competition - 1 Goals																
	Pages In	4								4																8
	Pages Out	4								4																8
	Activity	0.01 Competition - 1 Goals								0.01 Competition - 1 Goals																
	Pages In	4								4																8
Field 2	Activity	0.01 Competition - 1 Goals								0.01 Competition - 1 Goals																
	Pages In	4								4																8
	Pages Out	4								4																8
	Activity	0.01 Competition - 1 Goals								0.01 Competition - 1 Goals																
	Pages In	4								4																8
Field 3	Activity	0.01 Competition - 1 Goals								0.01 Competition - 1 Goals																
	Pages In	4								4																8
	Pages Out	4								4																8
	Activity	0.01 Competition - 1 Goals								0.01 Competition - 1 Goals																
	Pages In	4								4																8
Field 4	Activity	0.01 Competition - 1 Goals								0.01 Competition - 1 Goals																
	Pages In	4								4																8
	Pages Out	4								4																8
	Activity	0.01 Competition - 1 Goals								0.01 Competition - 1 Goals																
	Pages In	4								4																8
Total Pages In		16								16																64
Total Pages Out		4								4																16
Total Pages		12								12																48

		Saturday Competition								Saturday Competition																
		9:00am	10:00am	11:00am	12:00pm	1:00pm	2:00pm	3:00pm	4:00pm	9:00am	10:00am	11:00am	12:00pm	1:00pm	2:00pm	3:00pm	4:00pm	9:00am	10:00am	11:00am	12:00pm	1:00pm	2:00pm	3:00pm	4:00pm	Total
Field 1	Activity	0.01 Competition - 1 Goals								0.01 Competition - 1 Goals																
	Pages In	4								4																8
	Pages Out	4								4																8
	Activity	0.01 Competition - 1 Goals								0.01 Competition - 1 Goals																
	Pages In	4								4																8
Field 2	Activity	0.01 Competition - 1 Goals								0.01 Competition - 1 Goals																
	Pages In	4								4																8
	Pages Out	4								4																8
	Activity	0.01 Competition - 1 Goals								0.01 Competition - 1 Goals																
	Pages In	4								4																8
Field 3	Activity	0.01 Competition - 1 Goals								0.01 Competition - 1 Goals																
	Pages In	4								4																8
	Pages Out	4								4																8
	Activity	0.01 Competition - 1 Goals								0.01 Competition - 1 Goals																
	Pages In	4								4																8
Field 4	Activity	0.01 Competition - 1 Goals								0.01 Competition - 1 Goals																
	Pages In	4								4																8
	Pages Out	4								4																8
	Activity	0.01 Competition - 1 Goals								0.01 Competition - 1 Goals																
	Pages In	4								4																8
Total Pages In		16								16																64
Total Pages Out		4								4																16
Total Pages		12								12																48



Draft 28 May 2018

Mark Stockton
Team Leader Sport and Recreation | Parks and Recreation
City of Kingston
Email: mark.stockton@kingston.vic.gov.au

Dear Mark

Aspendale Gardens Sports Ground

Our ref: 25719

In response to Council's request for vegetation assessment of the proposed sports facility in Aspendale Gardens I inspected the site on 27 April and can provide the following.

Summary

The conservation significance of the site is low. Depending on the location of the development footprint, there may be a requirement for a planning permit to remove up to four plant species native to Victoria, and a protected flora permit to remove up to two plant species protected under the Flora and Fauna Guarantee Act. These are not seen as significant constraints in the design and construction of the facility.

Description

Most of the vegetation consists of introduced grassland comprising introduced grasses and herbs, sometimes in mixture with a generally low level of cover by certain native grasses and herbs (Attachment 1).

Small patches of planted Coast Manna Gum *Eucalyptus viminalis*, Black Wattle *Acacia mearnsii* and Coast Wattle *Acacia longifolia* subsp. *sophorae* occur on the lower southwest edge of the site. Black Wattle has naturalised with 15–20 seedlings.

Scattered planted trees (Kingston-indigenous and Australian native) are also present.

Biosis Pty Ltd
Melbourne Resource Group

38 Bertie Street
Port Melbourne VIC 3207

Phone: 03 9646 9499
Fax: 03 9646 9242

ACN 006 175 097
ABN 65 006 175 097

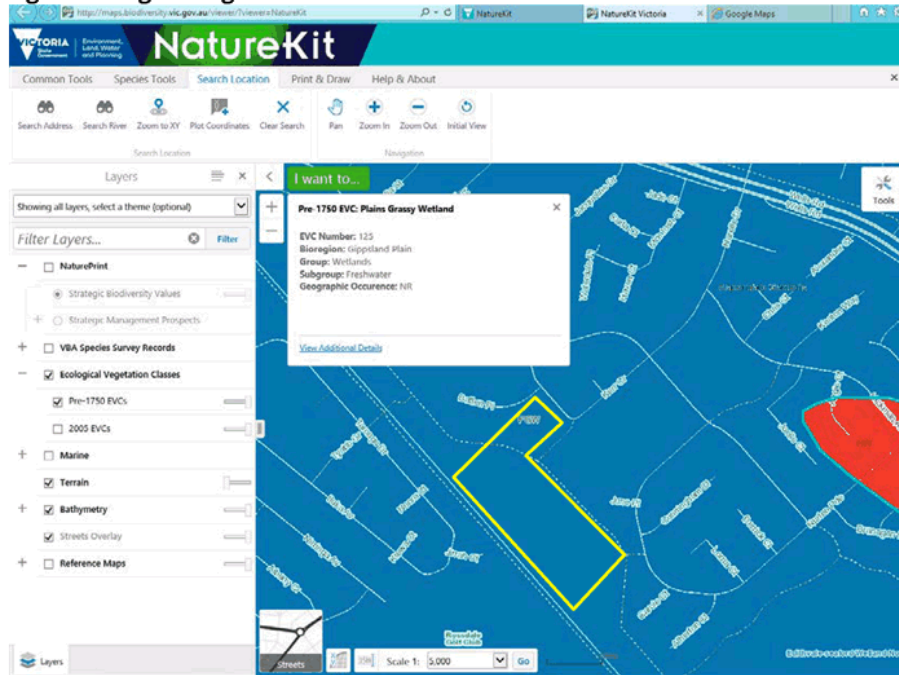
Email: melbourne@biosis.com.au

biosis.com.au



NatureKit vegetation mapping indicates the original vegetation was Plains Grassy Wetland (DELWP 2018). The site is within the former Carrum Swamp:

Figure 1. Original vegetation



Non-planted native vegetation is subject to permit for removal, as below.

Figure 2. Vegetation



Photos of the vegetation are in Attachment 2.



Conservation significance

None of the plant species are listed as threatened in the City of Kingston (Kingston Flora Database 2017).

The grassland and plantations provide habitat and food resources for some local birds and invertebrates. Five common bird species of open habitats were noted foraging on the grassland: Australian Magpie, Australian Magpie-lark, Noisy Minor, Crested Pigeon and Eastern Rosella.

The overall conservation significance of the site is low.

Permit requirements

A planning permit under the Planning and Environment Act is required to 'remove, destroy or lop' plants 'native to Victoria' – Clause 52.17 (Native Vegetation), Kingston planning scheme.

Under Clause 52.17 there is an exemption for native vegetation planted by Councils, as follows:

Planted vegetation	Native vegetation that is to be removed, destroyed or lopped that was either planted or grown as a result of direct seeding. This exemption does not apply to native vegetation planted or managed with public funding for the purpose of land protection or enhancing biodiversity unless the removal, destruction or lopping of the native vegetation is in accordance with written permission of the agency (or its successor) that provided the funding.
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PARTICULAR PROVISIONS - CLAUSE 52.17

PAGE 6 OF 9

Accordingly the following native vegetation requires a planning permit for removal:

- Plants that are native to Victoria and not planted, including Black Wattle seedling progeny (Attachment 1)

A permit under the Flora and Fauna Guarantee Act is required from DELWP to 'take' protected flora. All of the planted and non-planted Coast and Black Wattles are protected flora (Attachment 1).



Biodiversity Assessment Guidelines

Non-planted native vegetation is subject to permit for removal, however it is not sufficient on the site to invoke the *Guidelines for the removal, destruction or lopping of native vegetation* of the Department of Environment, Land, Water and Planning (DELWP 2017) which consider the following:

(a) 'patch' vegetation which is 'an area of vegetation where at least 25 per cent of the total perennial understorey plant cover is native'

Site: small patches of planted Coast Manna Gum *Eucalyptus viminalis* and Black Wattle *Acacia mearnsii* occur on lower southwest edge of study area (exempt from permit as planted).

(b) 'scattered trees' which are canopy tree species greater than 3 metres in height that occur outside patches.

Site: none present.

Offset requirement

Since the *Guidelines for the removal, destruction or lopping of native vegetation* (DELWP 2017) are not invoked, there is no offset requirement.

Lighting

Melbourne Water has expressed concerns about lighting (letter to Kingston City Council, 19 March 2018):

The proposed lighting used around the fields should be as low impact as possible given the proximity of the proposed fields to Edithvale Wetlands. Excess lighting, especially at night can cause disturbance to frogs and birds including impacts on migratory patterns. Edithvale wetlands is a Ramsar listed wetland and it supports key threatened species such as the Australasian Bittern and the Sharp tailed Sandpiper.

Council should consider planting vegetation between the fields and the drain to continue the corridor along the drain. Also consider planting vegetation at the Edithvale wetlands end of the fields to block out light to the wetlands.

The site is 250 metres from Edithvale Wetland to the southeast, with houses and gardens occupying most of that distance. It is difficult to see how lighting would significantly affect wildlife.

The floodway or drain immediately west of the site is usually dry due to large drains and is vegetated largely by introduced plants. It is low quality habitat for waterbirds and frogs.

The Melbourne Water recommendations are appropriate in order to minimise potential impacts.



Planting

Planting will be appropriate between the fields and the drain. The precise location of the plantation will depend on the facility layout, but is likely to be a series of linear screening belts.

Suitable hardy species indigenous to Kingston include the following:

<i>Acacia melanoxylon</i>	Blackwood
<i>Allocasuarina littoralis</i>	Black Sheoak
<i>Allocasuarina verticillata</i>	Drooping Sheoak
<i>Banksia integrifolia</i>	Coast Banksia
<i>Banksia marginata</i>	Silver Banksia
<i>Eucalyptus viminalis</i> subsp. <i>pryoriana</i>	Coast Manna-gum
<i>Goodenia ovata</i>	Hop Goodenia
<i>Leptospermum continentale</i>	Prickly Tea-tree
<i>Lomandra longifolia</i>	Spiny-headed Mat-rush
<i>Melaleuca ericifolia</i>	Swamp Paperbark

References

DELWP 2017. Guidelines for the removal, destruction or lopping of native vegetation.

Department of Environment, Land, Water and Planning, Victoria (December 2017).

www.environment.vic.gov.au/_data/assets/pdf_file/0021/91146/Guidelines-for-the-removal,-destruction-or-lopping-of-native-vegetation,-2017.pdf

DELWP 2018. NatureKit. Department of Environment, Land, Water and Planning, Victoria.

maps.biodiversity.vic.gov.au/viewer/?viewer=NatureKit

Kingston Flora Database 2017. Kingston Flora Database, Kingston Conservation and Environment Coalition.

Please contact me if you have any enquiries.

Regards

Dr Jeff Yugovic


Attachment 1. Flora native to Victoria

Scientific name	Common name	Planted (exempt)	Non-planted	FFG Act protected flora
Site-indigenous species:				
<i>Phragmites australis</i> (?)	Common Reed		✓	
Kingston-indigenous species:				
<i>Acacia longifolia</i> subsp. <i>sophorae</i>	Coast Wattle	✓		1 plant
<i>Acacia mearnsii</i>	Black Wattle	✓	✓	20-30 plants
<i>Dysphania pumilio</i>	Clammy Goosefoot		✓	
<i>Eucalyptus melliodora</i>	Yellow Box	✓		
<i>Portulaca oleracea</i>	Common Purslane		✓	
Victorian non Kingston-indigenous species:				
<i>Eucalyptus polyanthemus</i>	Red Box	✓		



Attachment 2. Photos



Photo 1. Introduced grassland



Photo 2. Plantation of Manna Gum and Black Wattle, southwest edge of site



Photo 3. Seedling progeny of planted Black Wattle



Photo 4. Plantation with Coast Wattle, southwest edge of site



Photo 5. Plantation of Australian native trees, near primary school



Photo 6. Floodway immediately west of site

Ordinary Meeting of Council

10 December 2018

Agenda Item No: 10.6

BENTLEIGH GREENS SOCCER CLUB'S PROPOSED SCOREBOARD UPGRADE

Contact Officer: Mark Stockton, Team Leader Sport and Recreation

Purpose of Report

To present and discuss a request from the Bentleigh Greens Soccer Club for the development and co-funding to upgrade the scoreboard facility at Pitch No. 1 at the Kingston Heath Regional Soccer Complex.

Disclosure of Officer / Contractor Direct or Indirect Interest

No Council officer/s and/or Contractor/s who have provided advice in relation to this report have declared a Conflict of Interest regarding the matter under consideration.

OFFICER RECOMMENDATION

That Council;

1. Approve funding of \$15,000, to match the contribution of the Bentleigh Green Soccer Club, for the purpose of purchasing an upgraded scoreboard for Pitch 1 at the Kingston Heath Regional Soccer Complex;
2. Approve the allocation of resources to meet the project delivery costs, including project management, regulatory approvals and environmental management i.e. soil management to upgrade of the scoreboard for Pitch 1 at the Kingston Heath Regional Soccer Complex;
3. Confirm the funding contribution from the Bentleigh Green Soccer Club in the form of a Funding Agreement to be authorised by the General Manager City Assets and Environment; and
4. Refers funding allocation to upgrade the Pitch 1 scoreboard at the Kingston Heath Regional Soccer Complex to Council's 2018/19 mid-year budget review process.

1. Executive Summary

The Bentleigh Greens Soccer Club (the Club) have made a request of Council for approval and co-funding to upgrade the scoreboard on Pitch No. 1 at the Kingston Heath Regional Soccer Complex. Pitch No. 1 currently has an operational electronic scoreboard that provides very simple functionality, i.e. showing purely the score (e.g. 1-0), compared to modern scoreboards.

The Club has requested the installation of a new 3.8m x 2.9m LED screen to replace the current scoreboard. In addition to showing the score, the proposed scoreboard would enable the live video feed of the game or games at other venues and provide opportunities to attract increased sponsorship opportunities and advertising.

The Club has confirmed funding of \$45,000, comprising a grant of \$15,000 from Ms Clare O'Neil MP, a club commitment of \$15,000 and has also had a "similar amount" provided by one of its sponsors. Given an expected cost of \$91,200 to purchase and install the proposed scoreboard, a funding contribution from Council would be approximately \$46,200 including a 20% contingency of \$15,200 for unknown latent ground conditions.

Council's Sports Infrastructure Policy, adopted August 2013 states that "all installation costs, replacement costs and ongoing costs including maintenance and repairs associated with scoreboards is the responsibility of the tenant club(s)." Whilst Council's co-funding towards the proposed scoreboard would be inconsistent with the Policy, Pitch 1 is used shared with the Bayside United Soccer Club. As it is not solely used by the Club, there is merit for Council to consider making a funding contribution to the upgrade of the scoreboard. The upgraded scoreboard would also assist with the management of patron safety, particularly the display of exit/entry points.

A number of options have been identified in response to the Club's request and they are discussed within this report.

2. Background

The Kingston Heath Regional Soccer Complex (KHRSC) is the premier soccer facility within the City of Kingston and is home to Bentleigh Greens Soccer Club (the Club). Pitch No. 1 includes a large pavilion, grandstand and existing electronic scoreboard in the North-West corner. The ground serves as the main pitch used by the Bentleigh Greens Senior team, as well as Bayside United's Senior Women's team (secondary tenant). Spectators at these games may range from 300 people up to 4000 people for high level matches.

The Club has a membership of just over 300 (membership details of Kingston based soccer clubs is set out in Appendix 1).

The Club has made a request of Council for approval and a funding contribution to upgrade the scoreboard on Pitch No 1 at the KHRSC.

The Club has received funding via the Commonwealth Government's Community Infrastructure Fund from Clare O'Neil MP for a total of \$15,000, as well as a "similar amount" from one its sponsors. The Club has committed to matching the \$15,000 achieving a committed funding contribution of \$45,000.

The Club has received a quote from Electronic Signage Australia to retrofit a large LED Screen (3.8m x 2.9m) to the existing scoreboard structure. The cost quote for this project is approximately \$55,000 GST exclusive. This cost does not include investigations and approvals, which officers' estimate would increase the total cost to \$91,200.

The Club has nominated the following benefits associated with this proposal:

1. This new scoreboard would allow an increase in sponsorship and advertising revenue for the Club and other users of the facility as it could provide advertising opportunities during home matches.
2. KHRSC being Council's premier football facility, should be kept to a high standard as feasible; and
3. The upgraded scoreboard would also benefit the Club in terms of spectator engagement.

These benefits are discussed further within Section 3.3.

The Club's request, including initial request and supply of supporting information is set out as Appendix 2.

The existing scoreboard is serviceable and in working order. It is an older scoreboard with its functionality limited to just showing the match score. The Club has indicated that it is supportive of the existing scoreboard being relocated to another venue. Given the specialist engineering required to relocate the scoreboard, its reuse would most likely incur similar costs to that of providing a new scoreboard.

3. Discussion

3.1 Council Plan Alignment

Goal 2 - Our sustainable green environment with accessible open spaces
Direction 2.5 - Provide for a variety of sport and recreation opportunities across Kingston through the Sport and Leisure Strategy.

3.2 Consultation/Internal Review

As the proposal to upgrade the scoreboard has limited impact upon surrounding properties and other users of the reserve, the level of required consultation is considered minor.

Given the size of the scoreboard, further town planning consideration i.e. possible permit application may be required.

3.3 Operation and Strategic Issues

3.3.1 Club Benefits

3.3.1.1 Sponsorship and Advertising

The Club indicate that the estimated revenue from sponsorship and advertising is "...unclear at this stage. The Club has not gone to market, but existing sponsors are interested in being able to run ads during matches. The Club intends to use the scoreboard initially to provide additional value to existing sponsors and generate publicity that will hopefully draw other potential sponsors."

As the proposed scoreboard can broadcast a live video feed of the game or games at other venues, it presents significant opportunities to generate revenue.

The upgraded scoreboard would give the Club additional funding to enhance its sustainability. It is unlikely that the scoreboard would financially benefit other users of Pitch 1; namely Bayside United Soccer Club.

Potentially Council could seek a financial return through the possible co-funding of the scoreboard.

3.3.1.2 Maintaining the status of the KHRSC

The Club is recognised as one of the strongest domestic clubs within the State. The Club indicate that it is considered a strong candidate for nomination to a proposed second tier national competition.

The upgraded scoreboard assists to maintain the status of the KHRSC and position it as a suitable venue to host matches within the proposed second tier national competition. As a Council asset, an upgraded scoreboard also increases the investment in its sport and recreation facilities.

The scoreboard would also be available for use by other uses at the KHRSC, including the Bayside United Soccer Team (female equivalent to the Club with the NPL).

3.3.1.3 Spectator Engagement

As Council's regional soccer facility, the KHRSC attracts significant spectator numbers. The upgraded scoreboard would enhance the spectator engagement through the replay of game highlights, similar to the replays shown at other large sporting arenas.

Whilst not highlighted by the Club, the upgraded scoreboard could also be used to display safety messages i.e. emergency evacuation for spectators.

3.3.1.4 Policy

Council's Sports Infrastructure Policy, adopted August 2013 (refer Appendix 3), states that "all installation costs, replacement costs and ongoing costs including maintenance and repairs associated with scoreboards is the responsibility of the tenant club(s)."

There have been a number of scoreboard development projects in recent years with clubs' funding the installation costs, including the scoreboard installation works at the following sites; Hihett Recreation Reserve, Mentone Reserve, Roy Dore Reserve and Walter Galt Reserve (planning underway). The clubs funding of such works have generally included grants from the Commonwealth and/or State.

Council has assisted many scoreboard projects with in-kind staffing and in some cases resourcing specialist investigations i.e. soil analysis.

3.3.2 Options

Option 1: As per Council's Sportsground Infrastructure Policy, the Club would need to meet all costs associated with the upgrade of the scoreboard.

Option 2: Guide the Club to apply for our Kingston Community Grants program. These grants will allow a small contribution up to \$5,000 from Council to support the project.

This will be dependent on the assessment process and does not guarantee funding to this project. The club will also have to wait as the grants applications do not open until February 2019.

Option 3: Council may provide a funding contribution of \$15,000 to match the Club's contribution. Given the complex nature of the works, it is further proposed that Council would also meet the associated project management, regulatory approvals and environmental management i.e soil management.

Whilst this approach is somewhat inconsistent with Council Policy, it does recognise the shared use of the Pitch and assists with the management of patron safety, particularly the display of exit/entry points at Council's regional soccer facility.

4. Conclusion

4.1 Environmental Implications

The proposed scoreboard uses LED (low power) technology and therefore uses low levels of power to operate.

4.2 Social Implications

The upgraded scoreboard provides entertainment opportunities for spectators at soccer events.

4.3 Resource Implications

The associated cost estimate for the development of the scoreboard are listed below:

- Environmental Management Assessment and Planning - \$10,000 #
- Geotechnical surveying and reporting - \$5,000
- Design development - \$4,000
- Permits - \$2,000
- Procurement and construction - scoreboard \$55,000

Sub-total Cost	\$76,000
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• Contingency @ 20%	\$15,200
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Estimated Probable Total Cost	\$91,200
-------------------------------	----------

Less

Club contributions	\$45,000
--------------------	----------

Funding Gap	\$46,200
--------------------	-----------------

Costs are subject to further site assessment and development of an environmental management plan

With full funding, the delivery of the scoreboard would be best timed, for mid-2019.

4.4 Legal / Risk Implications

To mitigate the construction and procurement management risks, it is proposed that Council project manage the upgrade of the scoreboard facility.

Appendices

Appendix 1 - Kingston Soccer Club Memberships (Ref 18/2173) [↓](#)

Appendix 2 - Bentleigh Greens Soccer Club request for scoreboard (Ref 18/604799) [↓](#)

Appendix 3 - Sportsground Infrastructure Policy - Appendix 2 Policy Paper (Ref 13/84584) [↓](#)

Author/s: Bridget Draper, Manager Kingston Active
Mark Stockton, Team Leader Sport and Recreation
Reviewed and Approved By: Daniel Freer, General Manager City Assets and Environment

10.6

BENTLEIGH GREENS SOCCER CLUB'S PROPOSED SCOREBOARD UPGRADE

- 1 Kingston Soccer Club Memberships 489
- 2 Bentleigh Greens Soccer Club request for scoreboard 491
- 3 Sportsground Infrastructure Policy - Appendix 2 Policy Paper503

SOCCKER CLUBS	Total players	Total Kingston residents	Total non-Kingston residents	Total Junior Players	Total Senior Players	% Kingston players
Aspendale Stingrays Soccer Club	445	402	43	425	20	90
Chelsea Soccer Club	399	303	96	320	79	76
Bentleigh Greens Soccer Club	301	99	202	282	19	33
Kingston City Soccer Club	190	36	154	150	40	19
Dingley Stars Football Club	88	14	74	42	46	16
Totals	1423	854	569	1219	204	<i>na</i>

Source: 2017 FFV participation figures

Mark Stockton

From: John Nevins
Sent: Thursday, 25 October 2018 4:07 PM
To: Lisa Stewart
Cc: Bridget Draper; Mark Stockton; Steve Staikos
Subject: FW: Bentleigh Greens - request for support
Attachments: Electronic Signage Australia - Bentleigh Greens Video Board Quotation (4....pdf)

Lisa,

Please record in HPE for Daniel to prepare advice for discussion with Councillors, initially at a CIS and please print out a copy of these emails for my Daniel F folder.

Regards,

John



John Nevins
 Chief Executive Officer
kingston.vic.gov.au
 d 9581 4706 e john.nevins@kingston.vic.gov.au

For enquiries, contact the City of Kingston



From: Abraam Gregoriou [REDACTED]
Sent: Thursday, 25 October 2018 3:13 PM
To: John Nevins <John.Nevins@kingston.vic.gov.au>
Cc: Steve Staikos <Steve.Staikos@kingston.vic.gov.au>; Daniel Freer <Daniel.Freer@kingston.vic.gov.au>; Mauro Bolin <Mauro.Bolin@kingston.vic.gov.au>
Subject: Re: Bentleigh Greens - request for support

John

Thank you for your email. Below is my response to your questions.

As I have mentioned, the club is not wedded to a particular model or structure. We are happy to work with Council on what is the most appropriate and cost effective product. I received another quote this week and it appears that \$50k will get you a modern scoreboard that has the capability to play video. Daniel also introduced me to a contact at St Kilda FC that might be able to assist with other potential options.

As always happy to discuss.

Abraam

1. *The financial contribution the Club will make. Your attached email of 10 October advises the Club intends to financially contribute.*

The club intends to match the funding provided by the Federal Government, \$15,000.

2. *What is the estimated revenue the Club will earn from advertising revenue on the new score board?*

This remains unclear at this stage. The club has not gone to market but existing sponsors are interested in being able to run ads during matches. The club intends to use the scoreboard initially to provide additional value to existing sponsors and generate publicity that will hopefully draw other potential sponsors.

3. *Does the Club have a proposed alternate use for the existing score board and is this alternate installation/use funded?*

The club does not have an intended use of the existing score board. We are happy for it to be used at another facility within the City of Kingston.

Bentleigh Greens notes that there is an option for a new scoreboard to be retrofitted to the existing site which could reduce the cost.

4. *Is it accepted that the current and proposed new score boards are Council assets?*

Yes. The club would however like to be able to brand the scoreboard with a potential sponsor subject to Council approval.

5. *Why does the existing score board need to be replaced?*

Funding committed by the Federal Government and matched by Bentleigh Greens provides a unique opportunity for Council to update key infrastructure at Kingston Heath at relative limited cost.

Since the previous score board was installed the club and the facility has grown dramatically. Kingston Heath plays host to national competitions that have been televised to a national audience of tens of thousands and witnessed by thousands of local spectators.

Given the recent investment by Bentleigh Greens and Council, Kingston Heath is the only football facility in the south east that can host a broadcasted match. This gives Kingston Heath the ability to host other clubs in the region thereby providing our community the opportunity to see quality football locally and showcase our municipality nationally.

Arguably Bentleigh Greens and Kingston Heath are the most well know football entities outside of the A-League. The club itself has strong ambitions, looking forward towards a possible national second division that will complement the A-League. The competition is also likely to expand with NPL teams from 2020 able to field multiple junior teams. Infrastructure such as a scoreboard as not seen as luxuries but necessities for hosting these games.

The current scoreboard is based on outdated technology and does not fit into the club's vision of modern facility. A new scoreboard would allow the club and other potential users the opportunity to advertise their sponsors, create highlight packages and showcase their club. This again will put Kingston Heath at the forefront of being one of the best football facilities in Victoria.

6. *Confirm the dimensions of the new score board are 2880mm(H) x 3840mm(W), as per attached quote*

That is our preferred option however the club is open to discussing with Council other options.

On Oct 22, 2018, at 04:47 PM, John Nevins <John.Nevins@kingston.vic.gov.au> wrote:

Abraam,

Thank you for your attached email which I have briefly discussed with the Mayor.

The allocation of Council funding for a new score board for Pitch No; 1 would require a Council budget allocation supported by a report to Council. To inform such a report could you please provide responses to the following questions and any other information you consider relevant.

1. The financial contribution the Club will make. Your attached email of 10 October advises the Club intends to financially contribute.
2. What is the estimated revenue the Club will earn from advertising revenue on the new score board?
3. Does the Club have a proposed alternate use for the existing score board and is this alternate installation/use funded?
4. Is it accepted that the current and proposed new score boards are Council assets?
5. Why does the existing score board need to be replaced?
6. Confirm the dimensions of the new score board are 2880mm(H) x 3840mm(W), as per attached quote

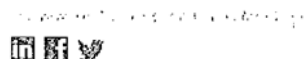
Regards,

John



John Nevins
Chief Executive Officer

kingston.vic.gov.au
d 9581 4706 e john.nevins@kingston.vic.gov.au



From: Abraam Gregoriou [REDACTED]
Sent: Monday, 15 October 2018 4:12 PM
To: John Nevins <John.Nevins@kingston.vic.gov.au>
Cc: Steve Staikos <Steve.Staikos@kingston.vic.gov.au>; Daniel Freer <Daniel.Freer@kingston.vic.gov.au>; Mauro Bolin <Mauro.Bolin@kingston.vic.gov.au>
Subject: Re: Bentleigh Greens - request for support

John

Great to hear from you and thank you for chasing this up.

In planning the works we obtained a quote from a provider who has done similar work for other clubs. Attached is a quote for the works. This provider installed the same scoreboard at the Veneto Club in Bullen which has been well received.

The plan would be to completely replace the current scoreboard on pitch 1 with a new scoreboard that has the ability to show video images including replays and advertisements of major sponsors.

We have secured \$15K from the Commonwealth for the project and one of our major sponsors has committed a similar amount. As you can see from the quote a large proportion of the cost is the electrical engineering. This is what we would seek Council support for which could be in kind and any additional funding that Council is willing to commit.

For the club we are wanting to secure the best possible scoreboard for the facility and are therefore open to the possibility of a different design in partnership with Kingston Council.

Please let me know if you require additional information and how we can progress.

Abraam

On Oct 12, 2018, at 03:48 PM, John Nevins <John.Nevins@kingston.vic.gov.au> wrote:

Abraam,

While the Mayor and I have briefly discussed your attached email request it would inform our discussion if Council knew how much the project will cost and how much you are requesting from Council.

Could you also confirm the new board would replace the existing board and the dimensions of the new board.

Regards,

John



John Nevins
Chief Executive Officer

kingston.vic.gov.au
d 9581 4706 e john.nevins@kingston.vic.gov.au

www.kingston.vic.gov.au



From: Steve Staikos
Sent: Friday, 12 October 2018 3:37 PM
To: John Nevins <John.Nevins@kingston.vic.gov.au>
Subject: RE: Bentleigh Greens - request for support

Hi John,

Can we assist?

Cr Steve Staikos
MAYOR

d 9581 4706

From: Abraam Gregoriou [REDACTED]
Sent: Wednesday, 10 October 2018 11:51 AM
To: Steve Staikos <Steve.Staikos@kingston.vic.gov.au>
Cc: Arthur Athanasopoulos [REDACTED] Meno Menelaou
[REDACTED]
Subject: Bentleigh Greens - request for support

Steve

Hope you are well and enjoyed your recent holiday.

Through the Community Infrastructure Fund, Clare O'Neil MP has offered the club \$15,000 to install a new scoreboard at the club. While the current score board is satisfactory given the prominence of the facility we believe there is an opportunity to upgrade it to one that has more modern features.

The club intends to contribute to the project financially and provide in kind support where possible.

I am also seeking your support for Kingston Council to financially contribute to the project. We have an initial quote but are seeking other options.

Are you able to advise if this is a possibility? I understand that current grant rounds are closed but we hope that some support for this project could be provided.

Abraam

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Electronic Signage Australia
 PO Box 1473
 Camberwell East VIC 3126

www.esignsaus.com
 info@esignsaus.com

0423 009 665
 0408 948 797

QUOTATION
 30/08/2018

QUOTATION NUMBER: BM002534

To: Bentleigh Greens Soccer Club
 301-307 Centre Dandenong Rd, Cheltenham VIC 3192



	DESCRIPTION	TOTALS
LED Video Board (3840mm w X 2880mm h) with a 10mm pitch	Full Colour LED Video Board 3840mm wide X 2880mm high with a 10mm pitch	\$29,890.00
Video Processor	The video processor allows the video board to be used to show TV/Foxtel, live camera feed	\$1,740.00
Steel sub-frame & supports	Fabricated steel sub-frame and supports (additional steel members) to engineers' specifications	\$5,590.00
Laptop & associated software	Laptop for scoreboard control & associated software for gameplay and advertising	\$775.00
Installation	Please see page 4 for installation requirements and inclusions	\$3,485.00
*Electrical and data work	*Please see page 4 for electrical and data works needed <i>(please note this is pricing is an estimate until confirmed by an electrician)</i>	*\$12,785.00
Delivery	Delivery of scoreboard to Bentleigh Greens	Included
	Subtotal	\$54,265.00
	GST 10%	\$5,426.50
	TOTAL	\$59,691.50

Our quote is provided on a best endeavors basis and the customer agrees to meet the cost of any additional work, services or materials that are required to address any event or situation which arises during the works that was unexpected or was not known or disclosed by the customer at the time of the quote being prepared.

Payment Terms: 50% deposit required upon order / 40% required upon delivery / 10% required upon commissioning
Lead Time: 10-12 weeks from deposit date

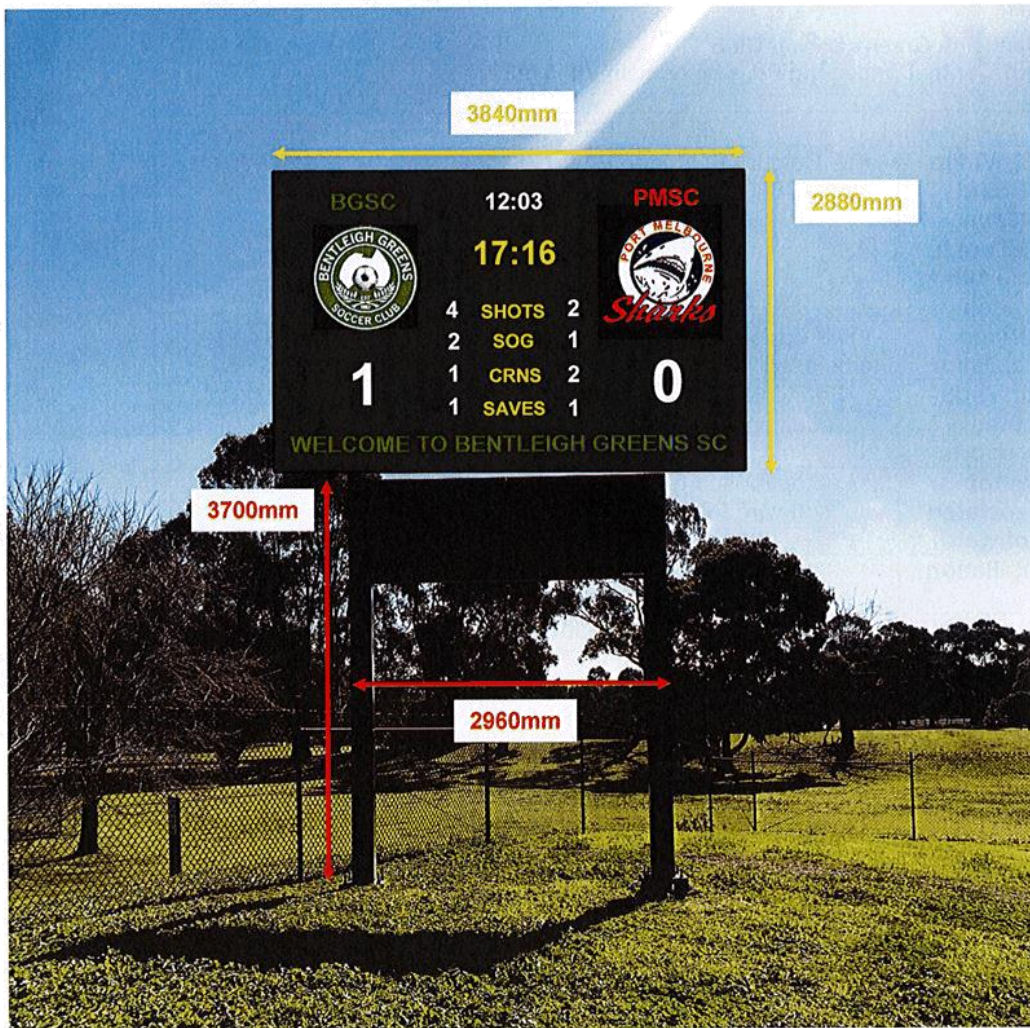
• BizMac DeSIGNS Pty Ltd trading as Electronic Signage Australia • ABN 60 163 569 453
 • PO Box 1473, Camberwell East VIC 3126 • www.esignsaus.com • info@esignsaus.com



Electronic Signage Australia
PO Box 1473
Camberwell East VIC 3126

www.esignsaus.com
info@esignsaus.com

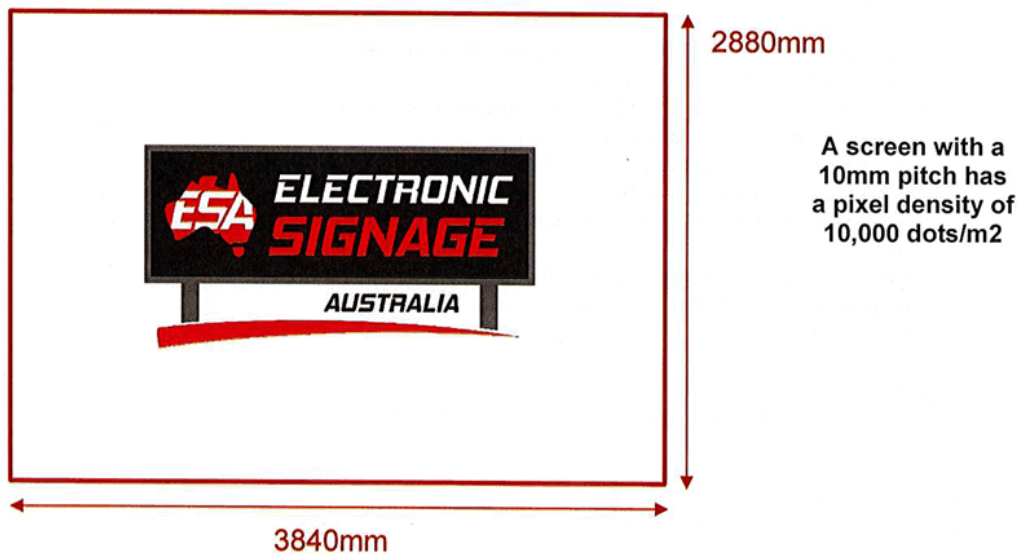
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Video Board size 3840mm X 2880mm (Pitch 10mm)



Video Board Inclusions:

- Delivery of video board to Bentleigh Greens
- Laptop controller
- Electronic Signage Australia scoring software for Soccer including:
 - Screen 1 – Gameplay mode*
 - Screen 2 – Sponsor display*
 - Screen 3 – Player display*
 - Screen 4 – Full screen mode*
- Ability for users to input any message or content required (club/sponsor videos etc)
- Electronic Signage Australia 3d Animations (automatically played following a goal in soccer)
- Scoreboard setup (includes calibration with control system)
- On-site training and instruction manuals

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0423 009 665
 0408 948 797

Installation Requirements:

The following is included in installation of the video board and costs are based on using the existing structure. Additional costs may be incurred if the existing structure is not suitable for use.

- Engineering documentation and certification for steel sub-frame (drawings of existing structure needed)
- Steel sub-frame and supports installation onto existing structure
- Video board installation into steel sub-frame (includes necessary boom-lift and crane hire)
- Electrical and data work – trenching from clubrooms (approx 140m) to allow electrical and communications (fibre-optic) run. Fibre-optic is needed for the screen to be used to show TV/live camera feed etc.

Note: This scoreboard size requires 39 amps upon in-rush (start-up) and D-Curve circuit breakers to allow for start-up current

- Temporary fencing

Not included:

- Building permit (if required and fees subject to council permit fees)

Servicing:

- On-call support (phone and email)
- 12 month warranty (parts and labour)
- Inspections for repairs to be attended to within 1-3 business days of first call-out
- Repairs to be performed within 10 business days of first inspection
- Cost of replacement panels depends on size of area needing replacement

Kind Regards,

Nick Birrell
 Product Development Executive
nick@esignsaus.com
 0423 009 665



Signed in acceptance by authorised representative

Signed..... Name..... Date.....

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 • PO Box 1473, Camberwell East VIC 3126 • www.esignsaus.com • info@esignsaus.com



Sportsground Infrastructure Policy

August 2013

Trim No: 13/84584

1. Introduction

The Sportsground Infrastructure Policy defines the shared responsibilities for investing in and maintaining infrastructure that supports organised sporting activities on Council's sportsgrounds.

The level of use of sportsground infrastructure available to the general public at times when not in use by the tenant club influences the level of responsibility expected of clubs in contributing to the initial capital and ongoing maintenance costs of sporting infrastructure.

The terms and conditions of this policy apply as described below; where there are no leases or other agreements in place between Council and the tenant club(s).

Where tenant clubs are identified as having responsibility for installation or repairs under this policy they must seek advice from the Sport and Recreation team prior to commencement. Depending on the scope, range and type of works proposed, tenants may need permits or formal approval from Council to undertake works on Council land or property.

2. Asset Management

It is acknowledged that all sportsground infrastructure on Council land is a Council asset. Council assumes an asset management role for sportsground infrastructure to ensure it remains in safe working order, minimising the risk to users and the general public.

As part of this role Council will undertake regular condition inspections of sportsground infrastructure. These inspections will include an assessment of the structural integrity and functional condition. Any issues identified through this inspection process will be addressed by Council's contractors.

In the execution of its role as asset manager, Council may identify that certain sportsground infrastructure is no longer able to be maintained in a safe manner and poses a risk to public safety. In this situation, Council will act in accordance with its legal obligations and will take action to ensure that the risk is removed. Removal of sportsground infrastructure in this manner does not change each parties responsibility under this Policy and does not require Council to replace the removed sportsground infrastructure.

3. Goal Posts

Council will provide the appropriate goal posts required for the individual sports at the commencement of each season.

These posts will remain in place for the duration of the season to service the training and match day needs of the tenant clubs, local schools and casual use by residents outside of these times.

Council will maintain goal posts in regards to painting, alignment and safety.

The priority for replacement of goal posts will be determined by Council.

The purchasing, provision, repair and replacement of any and all netting used in conjunction with goal posts is the responsibility of the tenant club(s).

Council does not provide any padding for goal posts.

4. Coaches Boxes and Interchange/Substitutes Benches

Coaches boxes and interchange/substitutes benches are in place at Council sportsgrounds for match day use by the tenant club.

Council supports the installation of coaches boxes and interchange/substitutes benches that occupy the minimum amount of space within the reserve. These structures are to be single level and be integrated with the sportsground fence where such a fence exists. Where a sportsground fence is not in place the coaches boxes and interchange/substitutes benches are to be located as close to the boundary line of the field of play as is safely permitted.

The provision, replacement and ongoing costs including maintenance and repairs of coaches boxes and interchange/substitutes benches is a joint responsibility between the clubs that use them and Council. However all installations are to be consistent with Council's standard design.

5. Scoreboards

Permanent scoreboards are in place at Council sportsgrounds for match day use by the tenant club.

Council supports the installation of scoreboards that cater for the needs of summer and winter season tenants and occupy the minimum amount of space within the reserve.

Council does not support the construction of new scoreboards incorporated in or attached to non-habitable buildings within reserves.

All installation costs, replacement costs and ongoing costs including maintenance and repairs associated with scoreboards is the responsibility of the tenant club(s).

6. Cricket Pitches

6.1 Turf Pitches

Turf cricket pitches are used by the tenant club(s) for their match day requirements.

At the conclusion of each winter season Council will undertake a process of scarifying, top-dressing, levelling and seeding turf pitch tables in preparation for the summer season.

All maintenance and pitch preparation activities and the costs associated with these activities throughout the remainder of the year (winter and summer season) will be the responsibility of the tenant club(s).

The refurbishment, replacement, expansion or upgrade of turf cricket pitches is to be funded by the club(s) that utilises the facility.

6.2 Synthetic Pitches

Synthetic cricket pitches are used for junior cricket, senior cricket (men's and women's), local schools and by the community on a casual basis.

Council is responsible for the installation and full replacement of synthetic cricket pitches. The full replacement of the pitch includes relaying the concrete slab and synthetic grass surface.

Tenant club's are responsible for repairs to and replacement of the synthetic grass covering on the concrete slab when required throughout the life of the concrete slab due to fair wear and tear.

Council will meet the cost of repairs and replacement to the synthetic grass when vandalised.

Council will determine the priority and timeframe for all repairs and replacement it undertakes.

Council will ensure that all synthetic pitches on grounds used for winter sport are covered to enable the safe use of the ground.

7. Cricket Practice Net Facilities

Council's standard provision of a three net facility provides durable off field practice facilities for clubs and publicly accessible facilities for the community at times when not being used by clubs. Each club has access to at least one training net facility.

The preferred positioning of the practice nets is on the edge of the sportsground to minimise their intrusion into the reserve and to facilitate any activities not contained within the net area (bowlers run ups and cricket balls hit out of the net) spreading out only onto the active sportsground space.

7.1 Replacement

The full replacement cost of a standard practice net facility is to be funded by Council on a like for like basis to a maximum of four nets per facility.

Clubs that would like any additional nets as part of a facility replacement, or specifications above the standard provision will be responsible for 100% of the additional costs.

A club relocated by Council to a facility for a season or part of a season will not be required to make any contribution to the replacement cost of their temporary facility nor will the club that regularly uses the facility receive any discount on their pro-rata contribution for that season at the time of replacement.

Council will fund new infrastructure at sportsgrounds where a facility does not currently exist if it is to be used by a club(s) that does not currently have a training facility.

Club's expanding their activities and seeking a second practice net facility at a sportsground that does not currently have such a facility will be responsible for the entire required club contribution for that facility.

7.2 Maintenance

The tenant club(s) is responsible for general maintenance and serviceability of the facilities throughout the year. The tenant club(s) is responsible for repairs to the practice facility netting and the replacement of the synthetic grass surface on all practice pitches when worn out through fair wear and tear.

Council will inspect the condition of cricket practice net facilities annually and prioritise any repairs required to run-ups, concrete bases and/or structural poles.

The prioritisation of the replacement of cricket practice net facilities will be determined by Council.

8. Sportsground Fences

The use of fencing around the active playing areas within Council reserves provides a barrier between the participants, spectators and other reserve users and clearly delineates the active playing area from the passive open space within the reserve.

The construction and maintenance of a standard sportsground fence is the responsibility of Council.

Any damage to the fence caused by the activities of the tenant club including the hanging of sponsor's signs will incur a repair cost to borne by the tenant club(s).

9. Baseball Fencing

High fencing around baseball diamonds protects participants, spectators and other reserve users from errant baseballs and clearly delineates the active sport area from the surrounding open space.

Initial installation and the full replacement of these fences is the responsibility of Council. This includes the player dugouts incorporated into the design of fences. The prioritisation of the replacement of these fences will be determined by Council.

The maintenance and all associated costs of these fences is a joint responsibility of the club and Council and is to be undertaken by Council Contractors as identified and agreed by both the club and Council.

Failure to address identified problems in a timely manner may result in:

- Restrictions on the use of the infrastructure; and/or
- Council undertaking the rectification and passing all costs on to the club(s);

All installations, upkeep, maintenance and replacement of practice and warm-up areas is the responsibility of the tenant club(s).

10. Athletics Field Event Infrastructure

Jumps pits and run ups, throwing circles and surrounding fencing, high jump aprons, and javelin runways are the fixed elements of athletics field event infrastructure in place within and alongside Council's athletics tracks.

Initial installation and the full replacement cost of athletics field event infrastructure is to be jointly funded by Council and the tenant club(s) on the basis that each contribute 50% of the cost.

Council will manage the installation of all athletics field event infrastructure.

The maintenance and all associated costs is a responsibility of the club and is to be undertaken as directed by Council and at other times as identified by the club.

11. Sportsground Lighting

Sportsground lighting is an essential training requirement for the safe and effective running of organised sporting activities, particularly during winter season.

Sportsground lighting on Kingston sportsgrounds will be designed and constructed to Australian Standards. The standard lux levels provided by sportsground lighting will be 100 lux, unless a special sporting need i.e. regional sportsground such as Kingston Heath Regional Soccer Centre, can be demonstrated.

Council will be responsible for the supply of electricity to the switchboard from which the sportsground lights are controlled.

Sportsground tenant clubs will be responsible for all utility and service charges connected to the operation of the sportsground lights.

Globe replacement will be undertaken by Council as part of the inspection and maintenance program, however the costs will be attributed to the current sportsground tenant club.

11.1 Investment in Upgrades and New Projects

11.1.1 Unused and superseded sportsground lights

Where sportsground lighting infrastructure within a reserve has been superseded, has been identified as being in poor condition, or is no longer in use, Council reserves the right to remove this infrastructure.

While every endeavour will be made to minimise any impact on tenant clubs, there will be no obligation on Council to replace such infrastructure or provide compensation.

11.1.2 Existing sportsgrounds

11.1.2(a) No sportsground lights

Where an existing sportsground does not have sportsground lights the installation of sportsground lights will be considered on a case by case basis with an emphasis on installations funded through external funding programs or user contributions.

Council will be the project manager on any new installation project.

11.1.2(b) Existing sportsground lights

Council will contribute the full cost of upgrading existing sportsground lights to the relevant Australian Standard.

Council will not contribute to any partial upgrade of this infrastructure unless it forms part of a lighting scheme that results in the eventual lighting level meeting the appropriate Australian Standard for the entire sportsground.

11.1.3 New sportsgrounds

Inclusion of sportsground lights in the design of any new sportsground will be considered on a case by case basis with an emphasis on installations funded through external funding programs or user contributions.

Council will be the project manager on any new installation project.

11.1.4 Replacement

Sportsground lighting infrastructure has a long life span which provides opportunity for funds to be set aside for replacement by Council.

Council's sportsground lighting inspections will identify infrastructure due for replacement.

Replacement is considered to be installing new for old and includes as a minimum the complete replacement of a light pole, its footings and all lamps attached to it.

Full replacement costs are to be funded by Council on the basis that:

- Council will be the project manager on any replacement project;
- Council will install sportsground lighting infrastructure to the relevant Australian Standard at the time.

Ordinary Meeting of Council

10 December 2018

Agenda Item No: 10.7

FORESHORE SHARED PATH CONSTRUCTION PROJECT 1 - NAPLES ROAD, MENTONE TO RENNISON STREET, PARKDALE

Contact Officer: **Rachelle Quattrocchi, Manager Infrastructure**

Purpose of Report

This report seeks Council's approval to award contract 18/63 – Foreshore Shared Path Construction Project 1 – Naples Road, Mentone to Rennison, Street Parkdale. It is proposed that CDN Constructors Pty Ltd be accepted as the preferred tenderer for this contract based on their submitted tender of \$4,884,740.77 ex GST and that the CEO or their delegate be authorised to negotiate and award Contract 18/63.

Disclosure of Officer / Contractor Direct or Indirect Interest

No Council officer/s and/or Contractor/s who have provided advice in relation to this report have declared a Conflict of Interest regarding the matter under consideration.

OFFICER RECOMMENDATION

That Council:

1. Authorise the Chief Executive Officer or their delegate to negotiate terms and award Contract No. 18/63 Foreshore Shared Path Construction Project 1 – Naples Road, Mentone to Rennison Street, Parkdale on a Lump Sum basis to CDN Constructors Pty Ltd for the tendered price of \$4,884,740.77 ex GST (\$3,369,000 Shared Path & \$1,515,740.77 Road Works).
2. Approve a Contingency Allowance of \$488,474.08 ex GST (10% of contract sum) from civil infrastructure capital allocation to cover unexpected financial variations relating to works or contract negotiations.
3. Direct the Chief Executive Officer or their delegate to advocate to the State Government for the removal of the pavement re-sheeting for this section of Beach Road, and if successful remove the relevant scope of works from this contract.

1. Executive Summary

Officers have evaluated tender submissions received for Contract No. 18/63 and have ranked CDN Constructors Pty Ltd as the highest scoring tenderer. This ranking has been arrived at after scoring of financial factors and company capabilities.

CDN Construction Pty Ltd has passed financial viability checks and has a long company history with an extensive list of successfully completed road and shared path reconstruction projects with the City of Kingston and other municipalities.

The discrete shared path works associated with this project are relative to the 2015 cost estimate of \$3.3M. This original estimate included the shared path construction and associated landscaping. The presented tender includes new and upgraded bus stops, the relocation of power poles and the installation of signalised pedestrian crossings which have been requested following the approval process with VicRoads and Public Transport Victoria – these elements were not considered in the 2015 cost estimates.

A significant contributing factor to the increase in the tender price has been the inclusion of the resurfacing of the entire width of Beach Road. This has been a requirement of VicRoads that was not included within the original cost estimate and contributes to approximately 30% of the tendered price. Officers have continued to hold discussions with VicRoads to seek contribution funding towards this element or consider an alternate treatment to line marking changes along the road length. Beach Road was resurfaced in 2011 and is currently in very good condition with an estimated 8 years remaining road surface asset life. The alternate treatment proposed to VicRoads is to black out the existing with black thermoplastic paint with a fine aggregate and subsequently re-line mark, or alternatively grind out existing line markings and re-line mark – these alternatives have an estimated cost of approximately \$80,000.

2. Background

The 700m-long shared path between Charman Road and Mentone Life Saving Club was completed by Council in 2015 following a period of consultation, planning, design and construction. This section of the trail has been welcomed by the community, with high use by recreational cyclists and pedestrians. With the completion of this section of works, Council has planned for the next section of shared path between the Mentone Life Saving Club and Peter Scullin Reserve, Mordialloc.

At the Ordinary Council Meeting on 26 June 2017, Council adopted the following resolution with regards to this project:

1. *Following community consultation receive and thank the community for its feedback on Council's March 2017 Bay Trail alignment option;*
2. *In response to the community's feedback make the following changes to Council's March 2017 Bay Trail alignment:*
 - a. *Widen the road widths proposed on the Council adopted March 2017 Bay Trail alignment from a point 60 metres north of Mentone Parade to the southern end of the indented parking opposite Naples Road by 0.35 metres, to provide parking in the kerbside lane along this section of the foreshore;*
 - b. *Maintain a 3.0m wide shared path through this section, consistent with the rest of the Bay Trail through Kingston, except for approximately 50 metres adjacent to and encroaching into the sand heathland, where the path is to be narrowed to 2.9 metres with the buffer to the road reduced to 0.75 metres to retain and protect environmentally significant vegetation;*
 - c. *Retain existing road widths and car-parking arrangements abutting the existing foreshore side kerb and channel section for Beach Road between the Mordialloc Creek and Rennison Street.*
3. *To give effect to the above changes, adopts the Bay Trail alignment as shown in attachment 8 as its revised June 2017 Bay Trail alignment;*

4. *Authorise the CEO or his delegate to do all things necessary to implement Council's revised Bay Trail alignment to the satisfaction of all relevant statutory authorities.*

Following this resolution, the project has proceeded through Statutory Planning Approvals by Council and following objection to the VCAT. Officers have developed a detailed design based on the adopted functional layout plan and sought prices from suitably qualified tenderers to construct the path for the section between the Mentone Lifesaving Club and Rennison St. VicRoads and PTV have issued written approval for the section of shared path subject to the approval of the final plans (inclusive of detail for bus stops and traffic signals).

3. Discussion

3.1 Council Plan Alignment

Goal 1 - Our well-planned, liveable city supported by infrastructure to meet future needs
Direction 1.1 - Intergenerational land use planning for a sustainable community

Works covered by Contract No. 18/63 have been identified by Council officers as necessary to sustain Council road and shared path infrastructure in the vicinity of Beach Road, Mentone to Rennison St.

3.2 Consultation/Internal Review

Feedback and input into the award of this contract has been sought from relevant internal departments. This includes:

- Advice from Traffic and Transport on traffic management approvals;
- Foreshore Management Team on vegetation removal and maintenance requirements;
- Infrastructure on design, construction management planning and tender documentation;
- Procurement and Risk on the procurement model use for the project, which included an Expression of Interest (E.O.I. No. 18/29) to establish a shortlist of pre-qualified contractors from whom quotes for the project were sought; and
- Communications and Community Relations on the communication plan for the project works.

Given the value and risk associated with the award of the contract, external legal advice was also sought on the wording of the contract to ensure Council's interests are protected. There is potential for latent conditions which may delay the project due to existing ground conditions and the sensitive nature of the area where works are to occur.

3.3 Operation and Strategic Issues

3.3.1 Victorian Civil and Administrative Appeals Tribunal (VCAT) Hearing

The path works were subject to a planning permit for the removal of native vegetation and alteration to the vehicle crossovers onto Beach Road. Following the Notice of Decision to grant a planning permit issued by Council at the Planning Committee on 24 January 2018 a request for review of the decision was lodged at VCAT.

Following consideration of the matter at the VCAT, a determination has been provided resulting in a favorable decision to Council to grant a planning permit. Coastal Management Act and Cultural Heritage approvals have been obtained.

3.3.2 Timeline for construction

Given the impending summer season, it is proposed that through the negotiation of terms for the contract, works will not commence until approximately April 2019.

3.4 Tender Evaluation

Prices were sought from the approved contractor panel appointed under Expression of Interest process, with tenders closing on Thursday, 21 June 2018, at 2:00PM.

Two tenders were received and evaluated by a Tender Evaluation Panel, consisting of Tony Pell, Senior Construction Engineer, Darren Stephens, Senior Design Engineer, Fleur Gascoyne, Strategic Projects Co-ordinator and Simon Nicolosi, Public Place Project Officer. Procurement procedures were followed in line with advice received from Council's procurement department.

Tender evaluation for contract 18/63 was undertaken by a scored tender evaluation method. Appendix 2 includes a breakdown of the scoring for the detailed evaluations. The detailed evaluation involved scoring tenderers against the following criterion:

- Tender price
- Ability to meet set constraints
- Methodology
- Access to suitable plant and equipment
- Access to suitable sub-contractors/ in-house trades

Scoring for the CDN Constructors Pty Ltd and Evergreen Civil Pty Ltd were derived after conducting a face to face interview and detailed analysis of the tender submissions.

CDN Constructors Pty Ltd obtained the highest overall score in the detailed evaluation. CDN Constructors Pty Ltd is recommended as the preferred tenderer for this project.

CDN Constructors Pty Ltd has a long company history with an extensive list of successfully completed road and shared path reconstruction projects with the City of Kingston and other municipalities.

3.5 Project Costs

The discrete shared path works associated with this project are relative to the 2015 cost estimate of \$3.3M. This original estimate included the shared path construction and associated landscaping. The presented tender includes new and upgraded bus stops, the relocation of power poles and the installation of signalised pedestrian crossings which have been requested following the approval process with VicRoads and Public Transport Victoria – these items were not considered in the 2015 cost estimate.

In addition to the inclusion of other items to the scope of works, there have been a number of factors that have contributed to the tender price and cost of the works.

**City of Kingston
Ordinary Meeting of Council**

Agenda

10 December 2018

- The cost of the project works was initially estimated in 2015, the cost of civil construction generally increases by around 3-3.5% per annum, meaning there has been a 10% increase since this time.
- There has also been a price increase of materials including concrete, asphalt and steel, that has resulted in substantial cost increases for these materials. The 2015 cost estimate included the price of a shared path as \$80 per sq. m (based on tender/contract pricing) whereas the current tender price is based on \$125 per sq. approximately 35% price increase
- As part of the Vic Roads approval, Vic Roads has required that the entire width of Beach Road be resurfaced with asphalt, this was not included within the original cost estimate for the works. These requirements have resulted in a significant cost to the works, approximately 30% of the tendered amount.

Project Cost Breakdown:

Element	Description	Cost (approx.)	Budget Source
Civil works	set out, excavation and preparation, footpath construction and associated works	\$2.85M	Bay Trail Implementation
Car park works	set out, excavation, resurfacing, pavement marking and sections of asphalt path	\$215K	Bay Trail Implementation
Traffic Signalised Works	pedestrian crossings and associated works	\$147K	Road & Car Park works
Bus Improvement Works	Supply install, services	\$157K	Walking and Cycling Paths
Asphalt works	including preparation, base preparation, traffic management, asphalt resurfacing and pavement marking	\$1.5M	Road & Car Park works
Contingencies	Allowance of up to 10% contingencies if needed	\$488K	Road & Car Park works

Conclusion

Following the tender evaluation of the Foreshore Shared Path Construction Project 1 – Naples Road, Mentone to Rennison, Street Parkdale, CDN Constructors Pty Ltd, is recommended as the preferred tender for award of contract No. 18/63. The works include the stage of the shared path between the Mentone Life Saving Club and Peter Scullin Reserve, Mordialloc.

3.6 Environmental Implications

The construction of the foreshore shared path will have a positive environmental impact by providing a safe path to cycle on which may in turn partially reduce reliance on motor vehicles in the area.

3.7 Social Implications

The 700m-long Bay Trail between Charman Road and Mentone Life Saving Club was completed by Council in 2015 following a period of consultation, planning, design and construction. This section of the trail has been welcomed by the community, with high use by recreational cyclists and pedestrians.

The construction of the section of shared path between Mentone Life Saving Club to Mordialloc considers better viability of the path from the street, retaining gravel walking path to provide separation between pedestrians and cyclists and offset of any vegetation loss.

Given the nature of the construction there will be some inconvenience, where possible this will be minimised. As part of the planning of the construction works, hours of work and the staging of the works have been carefully considered to minimise community disruption as much as possible.

3.8 Resource Implications

The funding for this contract is planned to be delivered through Council's adopted capital works program. Based on the project cost of \$3,369,740 funding will be drawn from the following project areas:

• Bay Trail Implementation	\$3,065,000
• Walking and Cycling Paths	\$157,000
• Mordialloc Creek Asset Renewals	\$100,000
• Car Park Works	\$47,000

In addition to the contract sum to be awarded, it is recommended that council approve a contingency allowance of 10% of the contract sum to cover unexpected project variations through delivery and contract negotiation. It is prudent to allow for variations of this magnitude as it is not possible to fully predict subsurface ground conditions and service authority asset locations prior to commencement of excavation works. This allowance is standard for projects of this nature. The contingency amount of \$488,474.08 will be drawn from the following project areas as required:

• Car Park Works	\$85,000
• Road works	\$403,474.08

The asphalt works of \$1.5M (which includes traffic management) are currently being negotiated with VicRoads.

To support the additional cost of \$1.5M for asphalt works and the \$488,474.08 from contingencies, budgeted projects from the road reconstruction and car park works budget will be used to offset the cost.

This will mean that some road and car parking projects will be delayed until the 2019/2020 financial year. It is proposed that Tootal Road (Dingley By-pass to McLure Road – budget \$1.5M) and Bourke Road, Clarinda (Old Dandenong to Clarinda Rd - budgeted \$1.1M) road projects to cover the cost of Beach Road asphalt works and contingency amount of \$403,474.08. With the remaining contingency amount of \$85,000 and the \$47,000 project cost to be budgeted from the Governor Road Car Park project (\$250K). Given the recent ceding of committee of management role over the Mordialloc Creek, Council's previous allocation of \$100,000 is no longer required.

A further bid to the 19/20 Capital Works Program will be required to complete the final section of the Bay Trail between Mordialloc Creek and Rennison Street.

3.9 Legal / Risk Implications

Through the tender negotiations the preferred tenderer has been made aware of the potential for a change in scope or a delay to the commencement date of the works. To further mitigate any risk and in protecting the interests of Council, the contract has been drafted to consider latent conditions.

Appendices

Appendix 1 - Foreshore Shared Path - Naples to Rennison - Face Sheet of drawing CON-1863 (Ref 18/94459) - Confidential

Appendix 2 - Contract 18-63 Foreshore Shared Path Naples to Rennison - Signed Tender Evaluation Summary (Ref 18/272777) - Confidential

Appendix 3 - Communication from VicRoads of in principle approval for final detailed design plans for Shared Path Foreshore Project (Ref 18/311674)

Author/s: Rachelle Quattrocchi, Manager Infrastructure

Reviewed and Approved By: Daniel Freer, General Manager City Assets and Environment

[REDACTED]

From: Rachelle Quattrocchi
Sent: Monday, 9 July 2018 2:06 PM
To: Katerina Kaldellis
Subject: FW: Final Detailed Design Plans for Shared Path Foreshore Project
Attachments: Standard Requirements.pdf

Follow Up Flag: Follow up
Flag Status: Flagged

[Here is the email and attachment...](#)

From: Alex.Brigo [REDACTED]
Sent: Friday, 6 July 2018 5:37 PM
To: Rachelle Quattrocchi [REDACTED]
[REDACTED]
Subject: Final Detailed Design Plans for Shared Path Foreshore Project

Hi Rachelle

As discussed in our meeting yesterday, VicRoads provides in-principle approval for the detailed design drawings received on 2 July 2018 for the proposed Bay Trail from Mentone to Mordialloc, it was agreed at the meeting that the changes required are minor and could be accommodated on a set of revised plans.

Therefore the detailed plans are approved in-principle subject to the following matters being addressed to the satisfaction of VicRoads:

- Alignment plans to show limits of batter
- Mentone Life Saving Club- car parks to adopt wheel stops adjacent to the share user path
- Final Street Lighting Plans to be supplied for review
- Details on electricity pole relocation to be supplied for review. It will have to be relocated clear of SUP to the foreshore side
- Bus Stops kerb & channel types to be shown
- Parkdale Yacht Club - more detail to be provided on the path through carpark and treatment proposed to separate SUP with cars. We understand that this is interim arrangement pending the club redevelopment
- Opposite Surf Street- access driveway to foreshore may require the shared user path to be strengthened for vehicle crossing
- Near Herbert Street- Shared user path tie-in to the existing foreshore tracks needs additional work (just 5 to 1 batter from the SUP path, connects need grading)
- Cross sections- show bus stops hard stand depth
- Cross sections CH 2280 to 2294.14- new re-sheet pavement surface shown undercutting the existing pavement levels
- Drainage - For where pipe diameters change size, it is preferred to match the pipe obverts and not the pipe inverts.

- Signing & linemarking- show RRPM's for re-sheet pavement works
- Signing and linemarking- ensure sign face legends match the signs required on each sheet
- Signing and linemarking - show SUP warning signage for people exiting the foreshore tracks
- Signing and linemarking- ensure the signage agrees within the Traffic Signal Remodel Plans & the Signing and Linemarking Plans
- Turning templates to be supplied for left turning movements out of side roads, due to Beach Road centreline being moved.
- A Road Safety Audit on the final drawings including traffic signal, street lighting and traffic management plans
- Incorporating any recommendations by RSA and VicRoads
- Resubmit final drawings including traffic signal plans, street lighting plans, traffic management plans and MOA for review/approval
- VicRoads will undertake surveillance only of the construction works within VicRoads demarcation for our maintenance (kerb to kerb excluding designated parking bays).

Further to this, VicRoads still has concerns with the 1.6m wide bike path, west of Naples Road. This width is significantly lower than the minimum requirement for a bike path. Council should develop measures to ensure safe operation at this point, whether it be through dismounting riders and/or signage such as 'end path' etc.

The VicRoads Improvement Projects team will now be handed the detailed designs, I have copied in Nathan who will be the responsible officer for the next stage of the project. In order to meet VicRoads' requirements for these tasks, the applicant will be required to comply with the requirements documented in 'Standard Requirements – Externally Funded Projects' and any other requirements considered necessary depending on the nature of the work.

A copy of the "Standard Requirements -Externally Funded Projects' has again been attached to this email for your reference.

If you have any further questions please contact Steven Yang on 9881 8973 or myself after 16 June.

Alex Brigo

Manager Project Interface
Metro South East

VicRoads

12 Lakeside Drive BURWOOD EAST

T 03 9881 8718

M [REDACTED]

E [REDACTED]

W vicroads.vic.gov.au

I acknowledge the Traditional Aboriginal Owners of Country throughout Victoria and pay my respect to Elders past and present and to the ongoing living culture of Aboriginal people.

STANDARD REQUIREMENTS EXTERNALLY FUNDED PROJECTS

GENERAL

- ❖ Your project requires you to do work within or near the road reserve that creates or modifies road infrastructure of which VicRoads will ultimately take ownership. While projects of this type may be referred to as "Externally" or "Developer" Funded Projects, the processes and requirements for undertaking this work will apply to your project.
- ❖ As instigator and owner of the project you are fully responsible for all aspects of work, repair or restitution associated with undertaking the work, to the satisfaction of VicRoads.

PLAN REVIEW

- ❖ Plan review to date has been in respect of a 'functional' arrangement. Proceeding with the development now requires review of the detailed design plans and specifications before work can commence, regardless of the standard of documentation provided to date.
- ❖ VicRoads can review the detailed design plans and provide comment for incorporation into the plans to be used for construction purposes. A fee is charged to do this review.
- ❖ The review does not guarantee completeness or accuracy. VicRoads accepts no responsibility for the plans or their use. All errors, omissions and required modifications/departures remain the responsibility of the project owner, for incorporation into the work, as a consequence of undertaking the project.
- ❖ A Road Safety Audit must be done on the detailed design plans. The audit must be undertaken by an independent, VicRoads' prequalified road safety auditor and all findings of the audit incorporated into the design or otherwise addressed to the satisfaction of VicRoads. All auditors must also be accredited by VicRoads/ARRB. A copy of the Road Safety Audit, the audit response and subsequently updated plans must be forwarded to VicRoads prior to the plans being accepted for construction purposes.
- ❖ Where structural design and drawings are required the design must be documented and submitted by a VicRoads' prequalified company. The design must also be independently proof engineered by a VicRoads' prequalified company.

CONSENT

- ❖ The project owner is required to apply for and receive Consent under the provisions of the Road Management Act to be able to work within the road reserve by making an application for Consent and providing the necessary Consent fee.
- ❖ The application for Consent should be made when your detailed design plans have been finalised. The application form to apply for consent will be forwarded at this stage.

RELATED CHARGES, COSTS & FEES

- ❖ The project owner is required to provide payment for a number of aspects of the project, including:
 - Charges for design and specification review, if applicable.
 - The Consent Fee.
 - Charges for certification audits (surveillance) of the project, if applicable.
 - Payment of direct costs incurred by VicRoads, if applicable.
 - Payment of ongoing operating costs, if applicable.
 - Lodgement of a performance retention that will be returned partly at completion of the work (50%) and the remainder at the expiry of a 12 month performance period.
- ❖ The project owner will be issued with invoices, as required, for all applicable charges, costs and fees.
- ❖ The project owner is required to lodge the performance retention to ensure the work is undertaken, completed and performs to VicRoads' satisfaction. In the event that the work is not to our satisfaction then the funds will be called on and used to achieve this.

- ❖ The project owner is required to prepare and submit an estimate of the value of the work within the road reserve for the purposes of determining the performance retention. VicRoads will determine this amount should the estimate appear understated.
- ❖ The value of the performance retention is normally a set percentage of the value of the work but may be increased depending on the nature of the work.
- ❖ The performance retention should be provided in the form of two equal bank guarantees so one can be returned at completion of the work as described above. The guarantees shall be provided by the legal owner of the project, to the benefit of VicRoads, and with no expiry date. Guarantees will not be accepted from representatives or contracted workers of the project owner.

BEFORE COMMENCING WORK

- ❖ Before commencing work the project owner has the responsibility to ensure:
 - Final design plans and specifications have been reviewed and accepted for use for construction purposes.
 - Consent has been provided.
 - Payment has been made for all relevant charges, costs and fees.
 - Construction contractors have complied with specified requirements.
 - Traffic Management Plans and associated memoranda of authorisation have been accepted for use.
 - Independent Road Safety Audits have been undertaken and incorporated, to VicRoads' satisfaction, on both the detailed design and any Traffic Management Plans.
 - Any other authority requirements have been complied with.
- ❖ VicRoads will provide written advice of permission to commence work when all requirements have been met. No singular approval or provision of Consent will constitute overall approval to commence until this written advice has been issued.

CONSULTANTS AND CONTRACTORS

- ❖ Consultants and contractors engaged by the project owner must be able to comply with the following requirements:
 - All consultants and contractors must be VicRoads' Prequalified, unless otherwise approved by VicRoads.
 - Consultants must be able to provide evidence of a professional indemnity insurance policy for an amount of at least \$5 million covering the work undertaken.
 - The contractor must be able to provide evidence of a public liability insurance policy for an amount of at least \$10 million effective for the duration of the work, indemnifying VicRoads against claims for damages or injury arising from the work, or to a value as otherwise determined.
 - The contractor must provide evidence of a current Workcover insurance policy effective for the duration of the work.
 - Consultants and contractors must operate under a Quality System.
 - The contractor must provide evidence that it has developed and maintained a Quality Management System, an Occupational Health and Safety System, an Environmental Management System and a Traffic Management System, to the satisfaction of VicRoads.
 - All consultants and contractors (including subcontractors) must be prepared to participate in certification activities (including surveillance and audits) to confirm compliance with VicRoads' standards or requirements.
 - Contractors must provide full contact details including an emergency contact that is able to respond to urgent requirements outside normal work times.
 - Consultants preparing Traffic Signal Plans must prepare these plans in accordance with the current VicRoads' drafting standards and must provide the design on a Microstation DGN file. Prequalified Consultants are generally aware of these requirements. VicRoads will not assist in preparing the design to the correct standard or format.

THE WORK & WORKMANSHIP

- ❖ All work shall be in accordance with VicRoads' Standard Specification for Roadworks and Bridgeworks and other published VicRoads' Specifications, standards, guidelines and practices.
- ❖ Work shall be in accordance with relevant Australian Standards and Statutes.
- ❖ Work times will be restricted at VicRoads' discretion to suit the local traffic conditions and the nature of the work. Preliminary advice in respect of this matter can be provided to assist with organising the work.
- ❖ The project owner, or its contractor, will be responsible for all maintenance of the work site, including pre-existing assets, during the course of the project.
- ❖ VicRoads reserves the right to undertake certification activities on all aspects of the project to assess compliance with its requirements.
- ❖ Certification of the work is to confirm compliance with VicRoads' requirements. Project owners are responsible for their own supervision and approval of work to ensure standards are met. Certification of work by VicRoads will not release the project owner of its obligations of meeting standards if deficiencies are subsequently identified.
- ❖ The project owner warrants performance of all work for a period of 12 months and is responsible for rectifying any defects arising during this period.
- ❖ A photographic record of the work shall be maintained, at intervals not exceeding one week, or at major changes in work or traffic layout, and shall be forwarded to VicRoads on a regular basis.

SPECIAL REQUIREMENTS

- ❖ The project owner is responsible for compliance with all project requirements and for ensuring that all work is undertaken to VicRoads' satisfaction. This will include, if applicable, ensuring compliance with planning permit obligations.
- ❖ Where permit requirements or the nature of the work requires approvals from the Responsible Authority or other organisations VicRoads will not grant approval to commence work, or allow work to continue, until the Developer provides evidence to VicRoads of all necessary approvals.
- ❖ Special requirements may be necessary depending on the nature of the development. These requirements will be brought to the project owner's attention, as they arise, during the course of the project.
- ❖ By commencing the work the project owner accepts that should it or its representatives fail to meet their obligations in accordance with VicRoads' specified requirements, VicRoads reserves the right to complete any outstanding work or to rectify any non-compliant work. All costs (direct and indirect) associated with undertaking such work shall be borne by the project owner and may be deducted from the performance retention being held by VicRoads.
- ❖ By commencing work the project owner accepts that VicRoads reserves the right to seek payment of any additional costs incurred on the project should the pre-payment estimate be insufficient to cover these costs. This may occur due to any actions, delays or increased involvement associated with the project.
- ❖ In exceptional circumstances the project owner may be required to modify the work previously undertaken albeit that the work may have been satisfactory at the time.

COMPLETING THE WORK

- ❖ The project owner is responsible for ensuring the project is completed to VicRoads' satisfaction, without defects or omissions.
- ❖ The project owner will be responsible for providing as-built drawings to VicRoads, indicating any departures or changes to the design that have eventuated during the construction.
- ❖ On completion of the project a joint inspection will be held between the project owner, VicRoads and any other related parties to assess the status of the work. VicRoads will either determine that the work has been completed or will otherwise advise the project owner of the outstanding work that must be undertaken. Defects and omissions will not be permitted to extend into the 12 month performance period.
- ❖ Successful completion of all work will instigate commencement of the 12 month performance period.

11. Corporate Services Reports

Ordinary Meeting of Council

10 December 2018

Agenda Item No: 11.1

ASSEMBLY OF COUNCILLORS RECORD REPORT

Contact Officer: Stephanie O'Gorman, Governance Officer

Purpose of Report

To provide copies of the Assembly of Councillors records in line with Section 80A of the Local Government Act 1989 to support openness and transparency of Governance processes.

Disclosure of Officer / Contractor Direct or Indirect Interest

No Council officer/s and/or Contractor/s who have provided advice in relation to this report have declared a Conflict of Interest regarding the matter under consideration.

OFFICER RECOMMENDATION

That Council note the contents of this report for the public record.

1. Executive Summary

This report contains records for all meetings defined as an Assembly of Councillors under Section 80A of the Local Government Act 1989, (the Act).

2. Background

The Act requires that Assembly of Councillors records are reported to the next possible meeting of Council. This seeks to promote openness and transparency of Council decision making and to place on public record any declarations of direct or indirect interests by Councillors.

3. Discussion

3.1 Council Plan Alignment

Goal 5 - Our well-governed and responsive organisation

Direction 5.1 - Support decision making to provide an efficient and effective council which embodies the principles of democracy

The reporting of Assembly of Councillors meets the requirements of the Act and is critical to Direction 5.1.

3.2 Consultation/Internal Review

Not applicable to this report.

3.3 Operation and Strategic Issues

3.3.1 Legislative Requirements

As prescribed by section 80A of the Act, the written record only needs to be a simple document that records:

- The names of all Councillors and staff at the meeting;
- A list of the matters considered;
- Any conflict of interest disclosed by a Councillor; and
- Whether a Councillor who disclosed a conflict leaves the assembly.

A standard Assembly of Councillors form will be used as the record for the purposes of the Act. These form the appendices to the report. At times, however to avoid duplication, minutes of some meetings may be attached as the record of the Assembly if they include the required information, including disclosures.

Section 80A of the Act requires a Councillor attending an assembly to disclose a conflict of interest and leave the room whilst the matter is being considered.

This requirement is explained in further detail in Practice Note No. 6 Assemblies of Councillors which was authored by Local Government Victoria. This Practice Note advises that unlike Council meetings, it is not necessary for a Councillor to disclose any details of the conflict of interest. It is sufficient to just disclose that the conflict of interest exists and this is all that should be recorded.

The rationale behind this limited requirement is to protect Councillors' privacy. In Council or Special Committee meetings, Councillors have an option under the Act to disclose a conflict of interest in writing to the CEO, which allows for the nature and type of the conflict of interest to remain private. The Act does not provide this option in relation to Assemblies of Councillors and thus Councillors are only required to disclose the existence of a conflict of interest and not the nature and type of interest at an assembly.

4. Conclusion

The report is provided in line with Section 80A of the Act which requires that the record of an assembly must be reported to the next practical Ordinary Meeting of Council and recorded in the minutes of that meeting.

4.1 Environmental Implications

Nil

4.2 Social Implications

Tabling Assembly of Council records supports disclosure and transparency of Council operations.

4.3 Resource Implications

Nil

4.4 Legal / Risk Implications

Reporting Assemblies of Councillors to Council meets the legislative requirement contained in section 80A of the Act.

Appendices

Appendix 1 - Assembly of Councillors Record - Strategic Councillor Information
Session - 3 December 2018 (Ref 18/623906) [↓](#)

Author/s: Stephanie O'Gorman, Governance Officer
Reviewed and Approved By: Phil DeLosa, Manager Governance
Paul Franklin, General Manager Corporate Services

11.1

ASSEMBLY OF COUNCILLORS RECORD REPORT

1	Assembly of Councillors Record - Strategic Councillor Information Session - 3 December 2018.....	517
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Assembly of Councillors Record

This Form MUST be completed by:

- (i) The appropriate attending Council Officer or;
- (ii) Chairperson of any Council Advisory Committee where there is no Council Officer present and returned IMMEDIATELY to the Manager Governance for filing.

Assembly details:

Date: 3 December 2018

Time: 5.45pm

Assembly Location: Oakleigh Room, 1230 Nepean Highway Cheltenham

Assembly Reason: Strategic Councillor Information Session

Attendees:

Councillor/s:

Cr Georgina Oxley (Mayor)
 Cr Tamara Barth (6.08pm)
 Cr Tamsin Bearsley (arrived 6.03pm)
 Cr Ron Brownlees OAM
 Cr David Eden (arrived at 6.03pm)
 Cr Geoff Gledhill
 Cr George Hua
 Cr Steve Staikos
 Cr Rosemary West OAM

Officer/s:

John Nevins, Chief Executive Officer
 Mauro Bolin, General Manager Community Sustainability
 Paul Franklin, General Manager Corporate Services
 Daniel Freer, General Manager City Assets and Environment
 Jonathan Guttman, General Manager Planning and Development
 Megan O'Halloran, Manager Communications & Community Relations
 Phil De Losa, Manager Governance
 Joanne Creedon, Governance Officer
 Ian Nice, Manager City Development
 Paul Marsden, Manager City Strategy
 Tony Collins, Team Leader Parks and Recreation
 Steve Lewis, Manager Community Buildings
 Julian Harvey, Manager Property and Arts
 Ross Gregory, Manager Traffic and Transport
 Madeline Di Pietrantonio, Social and Affordable Housing Implementation Coordinator

Apologies:

Nil

Matter/s Discussed:

1. Apologies
2. Disclosures by Councillors, Officers and Contractors of any Conflict of Interest

Assembly of Councillors Record

This Form MUST be completed by;

- (i) The appropriate attending Council Officer or;
- (ii) Chairperson of any Council Advisory Committee where there is no Council Officer present and returned IMMEDIATELY to the Manager Governance for filing.

3. Notes of the Strategic CIS Meeting of 19 November 2018
4. Planning Delegations Email Report - November 2018
6. Draft Agenda - Ordinary Meeting of Council - See Separate Agenda
5. CON 18/50 - Stormwater Drainage Maintenance
7. Aspendale Senior Citizen Works - Financial Impact
8. Kingston's Social and Affordable Housing Policy - Presentation from SGS Economics and Planning
9. Biodiversity Strategy Update
11. Response to Notice of Motion 3/2018 - Capital Works Parkdale Shopping Centre
12. Parking Management Policy - Online Permit Process
10. CEO Performance Review with Andrew Marty, SACS
16. Legacy Landfill Liability
17. Open Space Opportunity - 2 Horscroft Place Moorabbin
18. Mordialloc Freeway Environmental Effects Statement - DRAFT Council Submission
19. Mordialloc Freeway Visual and Landscape Assessment - Update on Community Consultation
21. Endeavour Cove Comprehensive Development Plan
13. Bentleigh Greens Soccer Club's Proposed Scoreboard Upgrade
14. Sports Fields Feasibility Consultation Findings and Next Steps - Aspendale Gardens
15. Kingston Disc Golf Course Stakeholder Consultation Findings and Next Steps
20. Bay Trail Update Presentation
22. Confidential
- Australia Day Award Nominations 2019
23. Councillor Appointments to Committees and Organisations 2019
24. Report on Legal Advice
25. Invitations
26. Councillor/CEO Only Discussion

Conflict of Interest Disclosures:

Did senior officer present ask for disclosure of Conflicts of Interest? Yes

Councillor Disclosures: (refer 2 over page).

Cr Bearsley disclosed a Conflict of Interest on Item Sports Fields Feasibility Consultation Findings and Next Steps - Aspendale Gardens.

Record if a Councillor left the meeting during the discussion.

Cr Bearsley disclosed a conflict of interest in Item 14 - Sports Fields Feasibility Consultation Findings and Next Steps - Aspendale Gardens and left the meeting prior to any discussion on this item.

Assembly of Councillors Record

This Form MUST be completed by;

- (i) The appropriate attending Council Officer or;
- (ii) Chairperson of any Council Advisory Committee where there is no Council Officer present and returned IMMEDIATELY to the Manager Governance for filing.

Officer Disclosures: (refer 4 over page)

Nil

Completed by: Stephanie O’Gorman, Governance Officer

Date: 5 December 2018

Assembly of Councillors Record - Any record of an Assembly of Councillors is reported at next practicable Council meeting and recorded in the Minutes.

Requirements and explanation:

1. Section 80A(1) and (2) Officer Requirements (re Written Record to be made of disclosure of Conflicts of Interest):

Section 80A(1) and (2) of the Local Government Act 1989, stipulates:

- "(1) At an assembly of Councillors, the Chief Executive Officer must ensure that a written record is kept of:
- (a) the names of all Councillors and members of Council staff attending,
 - (b) the matters considered,
 - (c) any conflict of interest disclosures made by a Councillor attending under subsection (3),
 - (d) whether a Councillor who has disclosed a conflict of interest as required by subsection (3) leaves the assembly."
- (2) The Chief Executive Officer must ensure that the written record of an assembly of Councillors is, as soon as practicable-
- (a) reported at an ordinary meeting of the Council; and
 - (b) incorporated in the minutes of that Council meeting."

2. Section 80A(3) and (4) Councillor Requirements (re Conflict of Interest):

Section 80A(3) and (4) of the Local Government Act 1989, stipulates:

- "(3) If a Councillor attending an assembly of Councillors knows, or would reasonably be expected to know, that a matter being considered by the assembly is a matter that, were the matter to be considered and decided by Council, the Councillor would have to disclose a conflict of interest under section 79, the Councillor must, at the time set out in subsection (4), disclose to the assembly that he or she has a conflict of interest and leave the assembly whilst the matter is being considered by the assembly. Penalty: 120 penalty units.
- (4) A Councillor must disclose the conflict of interest either-
- (a) immediately before the matter in relation to which the Councillor has a conflict of interest is considered; or
 - (b) if the Councillor realises that he or she has a conflict of interest after consideration of the matter has begun, as soon as the Councillor becomes aware that he or she has a conflict of interest."

3. Section 3(1) definition:

"**Assembly of Councillors**" (however titled) means a planned or scheduled meeting of at least five Councillors and one member of Council staff, or an advisory committee of the Council where one or more Councillors are present which considers matters that are intended or likely to be:

- the subject of a decision of the Council; or
- subject to the exercise of a function, duty or power of the Council that has been delegated to a person or Committee; or

but does not include a meeting of the Council, a Special Committee of the Council, a club, association, peak body, political party or other organisation."

Brief Explanation:

Some examples of an *Assembly of Councillors* will include:-

- Meeting / briefing of five Ward Councillors;
- Advisory committee or Village Committee Meeting where 1 or more Councillor is present
- Other Councillor briefing sessions;
- Budget discussions;
- Workshops re key Council priorities;
- Site inspections / preliminary planning conferences;

providing **at least five Councillors and one Council Staff member is present** and the matter/s considered are intended **or likely to be** subject of a future decision by the Council **OR** an officer decision under delegated authority.

As a matter of good practice, it would be considered exceptional not to deem any scheduled / planned meeting of five or more Councillors and an officer/s as an Assembly of Councillors. If you require further clarification, please call the Governance team.

4. Section 80B Officer Requirements (re Disclosure of Conflicts of Interest):

A member of Council staff who has a conflict of interest in a matter in which they also have delegated power, duty or function must:

- not exercise the power or discharge the duty or function; and
- disclose the type of interest and the nature of the interest to the Chief Executive Officer, in writing, as soon as he or she becomes aware of the conflict of interest in the matter, including those situations when the Officer is exercising a statutory power or duty of the Chief Executive Officer.

Ordinary Meeting of Council

10 December 2018

Agenda Item No: 11.2

QUICK RESPONSE GRANTS

Contact Officer: Gabrielle Pattenden, Governance Officer

Purpose of Report

To seek Council's consideration of Quick Response Grant applications received.

Disclosure of Officer / Contractor Direct or Indirect Interest

No Council officer/s and/or Contractor/s who have provided advice in relation to this report have declared a Conflict of Interest regarding the matter under consideration.

OFFICER RECOMMENDATION

That Council approve the following grant applications:

- Chelsea Kindergarten - \$1000.00
- Bayside Cricket Club - \$1000.00
- Highmoor Uniting Tennis Club - \$1000.00

That Council not approve the following grant application:

- The Pyjama Foundation

1. Executive Summary

The Quick Response Grants Program gives individuals and community groups the opportunity to apply for small grants to help them achieve their goals and ambitions.

This Program responds to the community's need for a form of grant that is flexible and efficient in terms of the time between application and approval and applies to smaller amounts of funding to a maximum of \$2,000.00.

Quick Response Grants are a category under Council's Community Grants Program.

2. Background

In November 2015 Council adopted the Quick Response Grants Program and Guidelines. Grant applications are checked for eligibility in line with a set of criteria outlined in the Guidelines. An application must be submitted to Council and considered for approval at an Ordinary Meeting of Council.

Any not-for-profit group, school or community organisation providing services within the City of Kingston may apply.

Individuals must be a resident of the City of Kingston and participating in an activity in an unpaid capacity and not as a requirement of any formal course of study or of their employment. Individuals can apply for a grant to assist them to participate in a sporting, educational, recreational or cultural activity; other pursuit of a personal development nature; or who is in needing circumstances.

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Community groups can apply for a grant to assist with the provision of a service, program or activity used by or of benefit to Kingston residents.

3. Discussion

3.1. Council Plan Alignment

Goal 3: Our connected, inclusive, healthy and learning community
Direction 3.4 Promote an active, healthy and involved community life

3.2. Operation and Strategic Issues

3.2.1. Assessment of Application Criteria

Applications for Quick Response Grants are assessed against the criteria outlined in the guidelines as follows:

- Does the proposed activity/event/project support the delivery of one or more of Council Plan strategic goals?
- Does the proposed activity/event/project benefit the City of Kingston residents?
- Has the applicant demonstrated a clear need for funds?
- If the applicant is an organisation, that it is a not-for-profit and has a bank account in the name of organisation.
- If the applicant is an organisation, are funds needed at short notice or can they wait for the Annual Grants program?
- Can the project be funded under any other Council grant program?

4. Applications

Name:	The Pyjama Foundation	
Amount requested:	\$2000.00	
Description of Project/Event:	The Pyjama Foundation is holding our annual Christmas Party for foster children, carers and volunteers at Gumbaya World. We provide mentoring services for foster children across Melbourne, with a large proportion coming from Mornington Peninsula. Our Love of Learning program pairs local adult mentors, 'Pyjama Angels', with local foster children, to boost the children's numeracy and literacy skills. This Party allows volunteers that work individually to meet each other and for the children to have a fun day out. We also celebrate the work of foster carers and volunteers and create a space for the community to bond and thrive.	
How the funds will be used:	\$3750 will be spent on entry tickets for 150 people (at \$25pp), and \$1800 on catered lunch.	
Assessment Criteria:		
• The activity/event/project supports one or more of the Council Plan strategic goals		✓
• The activity/event/project benefits the City of Kingston residents		✓
• The applicant has demonstrated a clear need for funds		✓
• The applicant is an individual or not for profit organisation		✓
• Funds are needed at short notice		✓
• The project cannot be funded under any other Council Grant program		✓
Grants received in current or last financial year		
Nil		
Officer Comment:		
This application is not recommended for approval as the event is to be held outside the City of Kingston. The Foundation has received a grant of \$1000.00 from the City of Casey.		

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Name:	Chelsea Kindergarten	
Amount requested:	\$2000.00	
Description of Project/Event:	Installation of a large shade sail in kindergarten playground	
How the funds will be used:	To erect a large sun shade with above ground concrete footings across the turf area of the playground at Chelsea Kindergarten.	
Assessment Criteria:		
<ul style="list-style-type: none"> • The activity/event/project supports one or more of the Council Plan strategic goals • The activity/event/project benefits the City of Kingston residents • The applicant has demonstrated a clear need for funds • The applicant is an individual or not for profit organisation • Funds are needed at short notice • The project cannot be funded under any other Council Grant program 		<ul style="list-style-type: none"> ✓ ✓ ✓ ✓ ✓ ✓
Grants received in current or last financial year		
2017/18 - \$2,000 – Calm Kids		
Officer Comment:		
This application meets the assessment criteria and is recommended for approval for an amount of \$1000.00.		

Name:	Bayside Cricket Club	
Amount requested:	\$2000.00	
Description of Project/Event:	Our curation equipment (lawn mower, line marking and roller etc) at our 2 Turf Grounds (Dane Road Reserve and Highett Reserve) are now dated and require replacement so that we can continue to provide quality cricket pitches and outfields for our local community of junior and senior cricketers.	
How the funds will be used:	Purchase of new equipment to assist with curation of turf grounds at Dane Road and Highett Reserves.	
Assessment Criteria:		
<ul style="list-style-type: none"> • The activity/event/project supports one or more of the Council Plan strategic goals • The activity/event/project benefits the City of Kingston residents • The applicant has demonstrated a clear need for funds • The applicant is an individual or not for profit organisation • Funds are needed at short notice • The project cannot be funded under any other Council Grant program 		<ul style="list-style-type: none"> ✓ ✓ ✓ ✓ ✓ ✓
Grants received in current or last financial year		
Nil		
Officer Comment:		
This application meets the assessment criteria and is recommended for approval for an amount of \$1000.00.		

**City of Kingston
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Name:	Highmoor Uniting Tennis Club	
Amount requested:	\$2000.00	
Description of Project/Event:	Installation of Tennis Victoria's Book-a-Court system [BAC] incl necessary fencing modification and electrical preworks	
How the funds will be used:	To defray the costs of modifying the front gate to make it compatible to the new online booking and keypad access system	
Assessment Criteria:		
• The activity/event/project supports one or more of the Council Plan strategic goals		✓
• The activity/event/project benefits the City of Kingston residents		✓
• The applicant has demonstrated a clear need for funds		✓
• The applicant is an individual or not for profit organisation		✓
• Funds are needed at short notice		✓
• The project cannot be funded under any other Council Grant program		✓
Grants received in current or last financial year		
Nil		
Officer Comment:		
This application meets the assessment criteria and is recommended for approval for an amount of \$1000.00.		

5. Conclusion

The grant applications in this report have been assessed according to the assessment criteria approved by Council in the Quick Response Guidelines.

5.1. Environmental Implications

Not applicable to this report.

5.2. Social Implications

The allocation of Quick Response Grants allows for Council to provide funds on a small scale to groups and individuals or towards projects or events that are consistent with Council's strategic directions and of benefit to Kingston's residents and community.

5.3. Resource Implications

Funds for Quick Response Grants are allocated by Council through its annual budget process.

5.4. Legal / Risk Implications

Not applicable to this report.

Author/s: Gabrielle Pattenden, Governance Officer
 Reviewed and Approved By: Phil DeLosa, Manager Governance
 Paul Franklin, General Manager Corporate Services

Ordinary Meeting of Council

10 December 2018

Agenda Item No: 11.3

APPOINTMENT OF MEMBERS TO STRATEGIC ADVISORY COMMITTEES

Contact Officer: Stephanie O'Gorman, Governance Officer

Purpose of Report

The purpose of this report is to recommend the appointment of new members to the Sport and Recreation Advisory Committee.

Disclosure of Officer / Contractor Direct or Indirect Interest

No Council officer/s and/or Contractor/s who have provided advice in relation to this report have declared a Conflict of Interest regarding the matter under consideration.

OFFICER RECOMMENDATION

That Council appoint the following applicants to the Sport and Recreation Advisory Committee until 30 June 2020:

- Nicole Adamson; and
- Darren McLeod.

1. Executive Summary

Following a resolution of Council in March 2018, membership vacancies were advertised for Council's Advisory Committees.

The Sport and Recreation Advisory Committee (the Committee) was identified as having two membership vacancies and two applications have been received. In accordance with the Committee's Terms of Reference (Appendix 1), all applicants must undertake an interview as part of the appointment process.

Following the review of the applications and the completion of the interview process it is recommended that Council appoint two new members to the Sport and Recreation Advisory Committee.

2. Background

At its Ordinary Meeting on 26 March 2018 Council resolved:

1. *That Council endorse the continuation of the following Advisory Committees and adopt the terms of reference contained in Appendix 1 of this report:*
 - *Arts and Cultural Advisory Committee*
 - *Business and Economic Development Advisory Committee*
 - *Community Safety Advisory Committee*
 - *Public Spaces and Environment Advisory Committee*
 - *Sport and Recreation Advisory Committee*

2. *That Council endorse the continuation of the Access and Equity Advisory Committee and adopt the updated committee terms of reference contained in Appendix 2 of this report.*
3. *That Council endorse the continuation of the Youth Advisory Committee and undertake a review of the committee's terms of reference.*
4. *That the Festivals and Events Advisory Committee be reclassified as a Network Committee.*
5. ***That the current membership of the Advisory Committees be extended until 30 June 2019 and a recruitment process undertaken to fill current vacancies that exist on committees (with an appointment until 30 June 2020).***

At the Ordinary Meeting of Council on 24 September 2018, it was further resolved to reconsider the numbers of members of the Strategic Advisory Committees and Ward Committees with a view to all committees having a maximum 21 members. In light of this subsequent resolution, appointment of Advisory Committee members will be an ongoing process to allow for increased membership numbers.

3. Discussion

3.1 Council Plan Alignment

Goal 5 - Our well-governed and responsive organisation

Direction 5.1 - Support decision making to provide an efficient and effective council which embodies the principles of democracy

Council is responsive to the needs of the community and recognises the important feedback role that Advisory Committees play.

3.2 Consultation/Internal Review

Officers advertised for vacancies on the Committee via the following outlets:

- Council website;
- Council social media platforms;
- Local newspapers; and
- Relevant sport and recreation networks.

3.3 Operation and Strategic Issues

3.3.1 Appointment Process

A total of two (2) applications were received and two (2) vacancies are available on the Committee in line with the current Terms of Reference. However, Council has recently resolved to consider increasing membership numbers to a maximum of 21.

In accordance with the Terms of Reference, both applicants have been interviewed and found to meet the membership criteria for the Sport and Recreation Advisory Committee. Due to availability, one applicant was interviewed by a panel comprised of officers and one Councillor and the other applicant was interviewed by only officers but is willing to attend a further interview with Councillor representatives if required.

Their professional experience coupled with their community involvement will hold these applicants in good stead to provide valuable contributions to the Sport and Recreation Advisory Committee and the City of Kingston.

A breakdown of each applicant and their alignment with the membership criteria is contained in Appendix 2.

3.3.2 Continuation of Existing Members

The Council resolution requires that membership of the existing committee members be extended until 30 June 2019 and that a recruitment process be undertaken to fill current vacancies that exist on committees (with an appointment until 30 June 2020).

All six (6) existing members of the committee have confirmed that they will retain their membership until June 2019.

The appointment of two new member will bring the committee membership to a total of eight (8) members.

4. Conclusion

The appointment of new members to the Committee will further enhance advice and feedback to Council on matters concerning sport and recreation within the municipality.

The applicant recommended for appointment meets the criteria for membership as set out in the Terms of Reference and brings specific knowledge and expertise in the area of sport and recreation aligns strongly with the membership criteria in the

4.1 Environmental Implications

Not applicable.

4.2 Social Implications

Appointing members of the community to the Sport and Recreation Advisory Committee enables community participation in specific areas of Council business, which ensures that Council considers the views of the community in its decision making processes.

4.3 Resource Implications

Not applicable.

4.4 Legal / Risk Implications

Not applicable.

Appendices

Appendix 1 - FINAL Sport and Recreation Advisory Committee Terms of Reference (Adopted 26 March 2018) (Ref 15/68824) [↓](#)

Appendix 2 - Summary of Applications - Sport and Recreation Advisory Committee - November 2018 (Ref 18/617923) [↓](#)

Author/s: Stephanie O'Gorman, Governance Officer

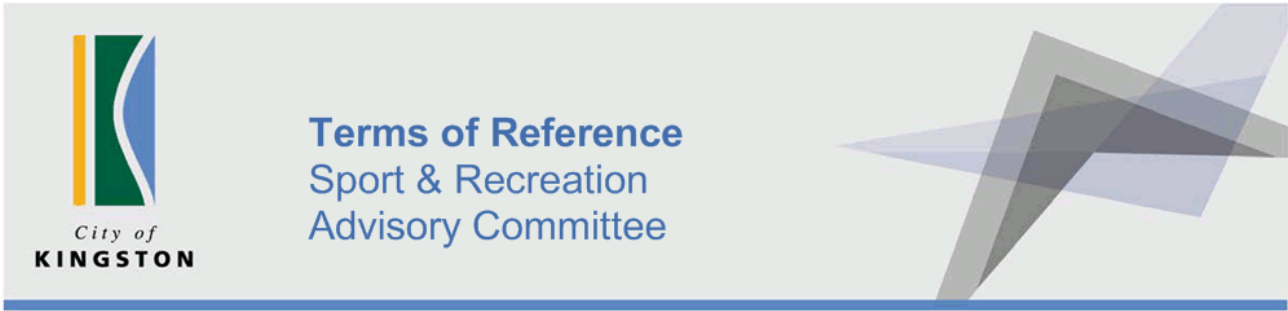
Reviewed and Approved By: Phil DeLosa, Manager Governance

Paul Franklin, General Manager Corporate Services

11.3

APPOINTMENT OF MEMBERS TO STRATEGIC ADVISORY COMMITTEES

1	FINAL Sport and Recreation Advisory Committee Terms of Reference (Adopted 26 March 2018).....	531
2	Summary of Applications - Sport and Recreation Advisory Committee - November 2018	537



<p>Name of Committee:</p>	<p>Sports and Recreation Advisory Committee</p>
<p>Purpose of Committee:</p>	<p>The Sports and Recreation Advisory Committee has been established to provide advice to the City of Kingston in relation to broad-based sports and recreation issues within the community.</p> <p>The Sports and Recreation Committee is an advisory committee of Council with a Councillor or Councillors nominated on an annual basis.</p> <p>The establishment of a Sports and Recreation Advisory Committee provides an important forum for identifying municipal-wide issues and opportunities and advising Council about effective policy and service provision regarding sports and recreation within the City of Kingston.</p> <p>As with all City of Kingston Advisory Committees, the Sports and Recreation Advisory Committee has a purely strategic focus. The committee should focus on issues and opportunities that are relevant across the entire municipality.</p> <p>The committee should provide specialist advice to Council as an input into decisions that impact municipal-wide Council policy.</p> <p>The Sports and Recreation Advisory Committee has no delegated decision making power from Council.</p>
<p>Council Plan Objectives</p>	<p>To provide advice to Council on the provision of :</p> <ul style="list-style-type: none"> • Assets that are managed and used to meet the needs of the community and provide for current and future generations • Quality community sports and recreational facilities that are well used and accessible • Parks, playgrounds and reserves that meet community needs, e.g. recreational competition sports, public health. • A community linked through trails, tracks and pathways • Green wedge planned and improved for current and future generations • Connected and effective community groups. • Increased participation in physical activity through Sport and Recreation services

ADVISORY COMMITTEE TERMS OF REFERENCE

<p>Criteria for membership:</p>	<ul style="list-style-type: none"> • Current involvement in service provision, policy or program development in the area of sports and recreation; • Endorsement by their organisation if applicable; • Strong community networks and linkages; • An ability to constructively participate in an advisory capacity; • An ability to represent a broad range of views that reflect the diversity of the community; • A strong understanding of the local community and its social, environmental and economic influences; • Good knowledge and understanding of the local issues that are relevant to sports and recreation; • A willingness to contribute positively to meetings in a fair and unbiased manner; • An ability to look beyond personal interests for the benefit of the community and residents of the City of Kingston; • An ability and willingness to encourage participation from and provide feedback to the community regarding sports and recreation; • A capacity to commit to the Advisory Committee for the required duration; • A willingness to celebrate the success and achievements of sports and recreation in the City of Kingston.
<p>Selection of Members:</p>	<p>The selection of the membership will involve calling for nominations. An advertisement may be placed in the local media and on Council's website. Specific representatives may be invited to nominate. A nomination form must be completed by interested representatives and all nominations will be assessed against the Criteria for Membership.</p> <p>Appointment will be for a period of two years. Should there be a need for the Sport and Recreation Advisory Committee to continue beyond this time, a re-appointment process will be undertaken for all members in accordance with the selection process outlined in these Terms of Reference.</p> <p>The Advisory Committee membership should be reflective of a broad range of sport and recreation interests.</p> <p>All nominees will be interviewed before appointment to the Committee. The interview panel will consist of a least two Councillors on the Committee and at least one Council Officer representative.</p> <p>Appointment will be for a period of two years, with a maximum of two terms i.e. four years. Should there be a need for the Sport and Recreation Advisory Committee to continue beyond this time, a re-appointment process will be undertaken for all members in accordance with the selection process outlined in these Terms of Reference.</p> <p>If inadequate nominations are received during the re-appointment process, members whose term has expired (i.e. served for the max four years) are eligible to re-nominate.</p>

ADVISORY COMMITTEE TERMS OF REFERENCE

<p>Chairperson:</p>	<p>The role of Chairperson shall be undertaken by a Councillor.</p> <p>A Councillor may also choose not to undertake the role of Chair. In this case an independent Advisory Committee member may be elected to the role through a vote or consensus among the independent members.</p> <p>The appointed Chairperson is responsible for the conduct of meetings, ensuring fair and equitable opportunities for views and opinions to be voiced and discussed by the Advisory Committee.</p> <p>A Deputy Chairperson should be elected to chair meetings in the absence of the Chairperson.</p>
<p>Membership:</p>	<p>The Sports and Recreation Advisory Committee will consist of the following membership structure:</p> <ul style="list-style-type: none"> • A maximum of three (3) Councillor representatives (appointed by Council resolution) • A maximum of eight (8) representatives from service providers/ agencies or organisations who have expertise and experience in the provision of sports and recreation services in the City of Kingston;
<p>Executive Support:</p>	<p>The Governance Department will:</p> <ul style="list-style-type: none"> • Provide terms of reference to new committee members. • Facilitate a review process for the committee and its terms of reference every two years. • Provide information and education on good governance issues such as; conflict of interest, confidentiality and register of interests. • Offer training for the chairperson if required. • Oversee the recruitment and selection process for committee membership. <p>The Sport and Recreation Department will:</p> <ul style="list-style-type: none"> • Compile and circulate agendas, attend meetings, compile and distribute minutes. • Compile the annual report of the committee on behalf of the Chairperson <p>The General Manager of City Assets and Environment and selected Council Officers will provide professional advice to the committee as required.</p>
<p>Quorum and Voting:</p>	<p>The Committee will operate with 'one third plus one' of the total membership.</p> <p>It is preferable that decisions of the Sports and Recreation Advisory Committee are made by consensus however; there may be circumstances where a matter is decided by a vote.</p> <p>Each member is entitled to one vote, except that the Chairperson who may exercise a casting vote should this be necessary.</p>

ADVISORY COMMITTEE TERMS OF REFERENCE

<p>Conflict of Interest and Confidentiality:</p>	<p>The Local Government Act identifies direct and indirect conflicts of interest which require disclosure as and when they arise. Members of the Sports and Recreation Advisory Committee must be fully aware of their responsibilities with regard to the management of interests in relation to the discharge of their duties as members of the Sports and Recreation Advisory Committee.</p> <p>Any matter deemed by a member to represent a Conflict of Interest shall be reported to the Chairperson and the Governance Department either prior to a meeting or before the specific item is discussed and dealt with in line with the Act and any relevant Council policies or guidelines.</p> <p>It is intended that the Sports and Recreation Advisory Committee will be a forum for discussing proposed policies and actions that may impact the Kingston Community.</p> <p>It is envisaged that members will be provided with information that is not available in the public realm. It will be critical that committee members are seen as a group that can be trusted with confidential information.</p> <p>The Sports and Recreation Advisory Committee's ability to fulfill its purpose will be severely hampered if that trust is undermined in any way. Given the level of confidentiality that will be associated with some of the information provided to members, any discussion with non-committee members is limited to information already available to the public.</p> <p>Confidential discussion will not be disclosed without the Chair's prior approval.</p>
<p>Meetings:</p>	<p>Meetings of the Sports and Recreation Advisory Committee will be held quarterly or more often by arrangement with the committee members and Chairperson. A schedule of meetings will be developed and agreed to annually. Under special circumstances a meeting may be cancelled or re-scheduled. A Council venue that is central for all members to access will be nominated for meetings.</p> <p>It is expected that each member of the Sports and Recreation Advisory Committee will attend a minimum of two (2) meetings each year.</p> <p>The Sport and Recreation Advisory Committee may hold joint meetings to deal with issues that cover the reach of more than one Advisory Committee.</p>

ADVISORY COMMITTEE TERMS OF REFERENCE

<p>Guiding Principles:</p>	<ul style="list-style-type: none"> • We are inclusive, open minded and respectful of everyone’s perspective • We put our personal agendas aside and provide advice for the greater good of the diverse City of Kingston community • Challenging and exploring are an integral part of what we do and how we operate • We represent and commit to the value of the committee • We actively participate and engage in the work of the committee • We are punctual, well prepared, timely with responses and we follow through • We are realistic about what we can achieve • We have a strong focus on outcomes
<p>Reporting:</p>	<ul style="list-style-type: none"> • A report of each Sport and Recreation Advisory Committee meeting is to be presented to a Councillor Information Session. • Minutes of each meeting will be provided to all Councillors of the City of Kingston. • A report of the Sports and Recreation Advisory Committee activities will be presented to the Council by 30 June annually.
<p>Related Documents:</p>	<p><u>Council Plan</u> <u>Living Kingston 2035</u> <u>Kingston Open Space Strategy</u> <u>Active Leisure Plan</u> <u>Active Youth Spaces Strategy</u> <u>Sport Specific Strategies (Soccer, Tennis)</u> <u>Various Reserve Master Plans</u></p>

Summary of Applications – Sport and Recreation Advisory Committee

Name	Summary	Interview	Selection Criteria	Recommendation
Nicole Adamson	<p>Currently holds positions of Non-Executive Director on the Board of Gymnastics Australia, sessional lecturer at Deakin University in Sports Management and a consultant to State and National sporting organisations.</p> <p>Professional experience in sports sector for the past 25 years.</p> <p>Extensive experience in the development of sport and recreation programs, facility planning and volunteer and club engagement.</p>	Y	Meets selection criteria	Appoint to committee.
Darren McLeod	<p>Currently holds position of Deputy Director of Sport at Monash University.</p> <p>Professional experience in sport leisure and major event industries for over 20 years.</p> <p>Active in community sporting clubs/committees.</p> <p>Extensive experience in sport and recreational service and program provision, strategy development and master planning processes.</p>	Y	Meets selection criteria	Appoint to committee

Ordinary Meeting of Council

10 December 2018

Agenda Item No: 11.4

TENDERS FOR CONTRACT NO 18/115 - PROVISION OF INTERNAL AUDIT SERVICES

Contact Officer: Paul Franklin, General Manager Corporate Services

Purpose of Report

This report seeks Council's acceptance of a tender for Contract No 18/115 for the provision of Internal Audit Services to Council from 1 July 2019 to 30 June 2022 with the option of one 3 year extension to June 2025.

Disclosure of Officer / Contractor Direct or Indirect Interest

No Council officer/s and/or Contractor/s who have provided advice in relation to this report have declared a Conflict of Interest regarding the matter under consideration.

OFFICER RECOMMENDATION

1. That Council award Contract 18/115 to Pitcher Partners Pty Ltd to a schedule of rates contract at an estimated sum of \$170,000 per annum (excluding GST) based on an internal audit program of approximately 1300 hours for an initial 3 year period concluding on 30 June 2022.
2. That one 3 year contract extension be exercised at the discretion of the Chief Executive Officer or delegate.

1. Background

The Council is committed to delivering effective audit mechanisms, which are designed to improve efficiency and effectiveness, improve risk management and promote an ethical organisational environment.

Internal Audit is a key element of the Council's approach to risk management and the organisation has had an internal audit function since October 1995. The Council also has an independent Audit Committee, which comprises three independent members, and two Councillors. The Chief Executive Officer and the General Manager Corporate Services attend meetings of the Audit Committee on an "ex-officio" basis. The Committee has been meeting regularly since November 1995 and currently meets on a quarterly cycle with special meetings as required.

2. Discussion

2.1 Council Plan Alignment

Planned Outcome 5 - Our well-governed and responsive organisation
Strategy 5.4 - A responsive and well managed organisation

3. The Service to be Provided

In the context of the previous audit work program, the successful tenderer is required to:

- Prepare a Strategic Internal Audit Plan for submission to the Audit Committee for approval;
- Undertake Strategic Internal Audit Plan projects in accordance with the adopted plan;
- Attendance at Audit Committee meetings Quarterly meetings;
- Provide status report of implementation of Audit Plan;
- Liaison with External Auditors; and
- Conduct Special Investigations as requested by the Chief Executive Officer.

The conduct Probity Audits of larger scale / high value procurement processes or commercial transactions has been excluded from this assessment and subject to later determination.

Tenderers were also asked to demonstrate a track record of previous relevant experience in comparable organisations; the capacity to undertake the required work load; and the ability to commit suitably skilled and qualified staff to Internal Audit assignments.

4. Tenders Received

Tenders were advertised in The Age on Saturday 20 October 2018 and closed at 2.00pm on Thursday 8 November 2018.

Tenders were received from:

- BDO EastCoast Partnership
- HLM Mann Judd Victoria
- KPMG
- CT Management/Centium
- Oakton Services Pty
- Pitcher Partners Consulting Pty Ltd

5. Evaluation Panel

The tenders were assessed by:

- Paul Franklin, General Manager Corporate Services
Hugh Parkes, Chair, City of Kingston Audit Committee

6. Evaluation Criteria

The evaluation criteria were applied in a two step process:

Mandatory Requirements

- | | |
|--|-----------|
| • Insurances Exist and at required value | Pass/Fail |
| • Financial Viability | Pass/Fail |

Quality Tender Criteria

- Strategic Audit Plan Development
- Delivery of Internal Audit Projects
- Quality of Outputs / Personnel
- Local Government Experience and Knowledge

If tenderers did not score over 35 (of a maximum score of 60) at this point they were excluded from further consideration as they did not demonstrate sufficient understanding, capability and/or capacity to undertake the work required by Council.

7. Tender Evaluation

The Evaluation Panel determined that only those tenderers that reached the threshold of 35 (of a maximum score of 60) would progress to being shortlisted for interview by the panel. These interviews were conducted on Wednesday 21 November 2018. The shortlisted tenderers were:

- Pitcher Partners Consulting Pty Ltd;
- Oakton Services Pty Ltd; and
- HLB Mann Judd Victoria

Following interview the panel re-scored the Quality Tender Criteria and then added the fee proposal to the scoring matrix to give the final recommendation.

Appendices

Appendix 1 - CON-18 115 - Tender Evaluation Matrix (Ref 18/603471) - Confidential

Author/s: Paul Franklin, General Manager Corporate Services
Reviewed and Approved By: Paul Franklin, General Manager Corporate Services

Ordinary Meeting of Council

10 December 2018

Agenda Item No: 11.5

LEGAL ADVICE REPORT NOTICE OF MOTION

Contact Officer: Paul Franklin, General Manager Corporate Services

Purpose of Report

To recommend an amendment to the Council Resolution of 27 November 2017 to no longer provide individual invoices as part of the monthly report.

Disclosure of Officer / Contractor Direct or Indirect Interest

No Council officer/s and/or Contractor/s who have provided advice in relation to this report have declared a Conflict of Interest regarding the matter under consideration.

OFFICER RECOMMENDATION

That Council determine that a copy of all invoices received from any law firm is no longer required to be provided to a Councillor Information Session each month as part of the Report on Legal Advice.

1. Executive Summary

At the Ordinary meeting on 27 November 2017 Council adopted the following resolution:

That Officers provide a monthly legal update briefing to Councillors through a CIS cycle.

This is to include:

- *An update on any ongoing legal disputes or cases Council are currently engaged in.*
- *An update on potential legal cases or disputes that have arisen*
- *An update on any legal advice sought between each briefing.*
- *A copy of all invoices received from any law firm between each briefing.*
- *That the cost of legal matters for 2015/16, 2016/17 and 2017/18 be included.*

In discussions with Councillors, it was felt that the provision of copy invoices, averaging approximately 300 pages per month, was not adding to the usefulness of the report. Should further detail be required this can still be sought from the General Manager Corporate Services.

The report will continue to provide:

- *An update on any ongoing legal disputes or cases Council are currently engaged in.*
- *An update on potential legal cases or disputes that have arisen*
- *An update on any legal advice sought between each briefing.*

Author/s: Paul Franklin, General Manager Corporate Services

Reviewed and Approved By: Paul Franklin, General Manager Corporate Services

Ordinary Meeting of Council

10 December 2018

Agenda Item No: 11.6

COUNCILLOR APPOINTMENTS TO COMMITTEES AND ORGANISATIONS 2019

Contact Officer: Phil DeLosa, Manager Governance

Purpose of Report

The purpose of this report is to appoint Councillors to the various committees and organisations requiring Councillor representation for 2019.

Disclosure of Officer / Contractor Direct or Indirect Interest

No Council officer/s and/or Contractor/s who have provided advice in relation to this report have declared a Conflict of Interest regarding the matter under consideration.

OFFICER RECOMMENDATION

1. That Council resolve to make the following Councillor appointments to each of the committees listed in the table below for 2019.

Legislative Committees	2019 Appointee
Planning Committee	All
Audit Committee	Mayor Cr Gledhill
Municipal Emergency Planning Committee	Cr Gledhill

Advisory Committees	2019 Appointee
Access and Equity	Cr Barth
Arts and Culture	Cr Bearsley
Business and Economic Development	Cr Hua Cr Gledhill
Community Safety	Appointment deferred
Public Spaces and Environment	Cr West
Sport and Recreation	Cr Gledhill Cr Oxley Cr Staikos
Youth	Cr Eden Cr Oxley

Consultative Committees	2019 Appointee
LF Payne Hall Usage Committee	Cr Eden Cr Bearsley Cr Oxley
Kingston Charitable Fund (KCF) Community Grants Panel	Mayor Cr Brownlees
Kingston Interfaith Network	Cr Bearsley
Kingston Historic Network	Cr Gledhill Cr West (alternate)

**City of Kingston
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10 December 2018

External Committees/Organisations	2019 Appointee
Association of Bayside Municipalities	Cr West Cr Gledhill (alternate)
Friends of Manatuto Committee	Cr Staikos Cr West (alternate)
Inter Council Aboriginal Consultative Committee	Cr Barth Cr West (alternate)
Local Government Waste Management Forum	Cr Hua Cr Staikos (alternate)
Metropolitan South Eastern Region MAV Representatives and Mayors	Mayor
Metropolitan Transport Forum	Cr Gledhill Cr West (alternate)
Moorabbin Airport Consultative Committee	Cr Brownlees Cr West Cr Staikos
Municipal Association of Victoria	Mayor
Municipal Association of Victoria Environment Committee	Cr West
Municipal Association of Victoria Planning Committee	Cr West
Victorian Local Governance Association	Cr Staikos

2. Further that the Councillor appointment to the Community Safety Advisory Committee be deferred pending receipt of an officer report regarding the future status of the Committee.

1. Executive Summary

Council annually appoints Councillors to various committees and organisations as representatives. These appointments are important as they provide a consultative link between Council and the committees and organisations.

The committees and organisations comprise:

- Legislative Committees;
- Advisory Committees;
- Consultative committees that include Council officers and/or community representatives; and
- External committees/organisations that require Councillor representation.

2. Discussion

2.1 Council Plan Alignment

Goal 5 - An Effective Voice

Direction 5.2 - Representative and inclusive of diverse views and communities

3. Conclusion

All committees will be notified of the Councillors' appointments.

**City of Kingston
Ordinary Meeting of Council**

Agenda

10 December 2018

Author/s: Phil DeLosa, Manager Governance

Reviewed and Approved By: Paul Franklin, General Manager Corporate Services

12. Notices of Motion

Ordinary Meeting of Council

10 December 2018

Agenda Item No: 12.1

NOTICE OF MOTION NO. 36/2018 - CR EDEN - NATIVE TREE VOUCHERS

I move :

That Council officers bring a report back by the February meeting of 2019 that details the options available for providing residents and ratepayers a free native tree voucher with each rates notice / or upon request.

Council officers should consider any community groups that grow native trees, departments internally to Kingston Council or not for profits that Council could partner with.

Cr David Eden

Ordinary Meeting of Council

10 December 2018

Agenda Item No: 12.2

NOTICE OF MOTION NO. 37/2018 - CR STAIKOS - COMMUNITY SPONSORSHIP PROGRAM

I move that The City of Kingston notes:

1. We live in a world where people have no option but to flee their homes and countries, and that globally, resettlement places for these people are rare and over-subscribed;
2. All countries, especially the wealthiest, like Australia, need to do their fair share by welcoming refugees in any way they can;
3. Refugees and migrants have made a positive contribution to our community; and
4. We need to create more opportunities for safe and legal entry into Australia, and one way to do this is through community led sponsorship.

I further move that:

1. Kingston is a community which welcomes refugees;
2. Kingston supports an expanded and improved Community Sponsorship Program; and
3. Calls on the federal government to improve and expand the Community Sponsorship Program to ensure the program:
 - a. Does not take places from others in need
 - b. Provides adequate support and services
 - c. Limits the costs on sponsors
 - d. Allows community, family and businesses to act as sponsors
 - e. Creates more places for people in need of protection to settle in Australia

Cr Steve Staikos

14. Confidential Items

14 Confidential Items

The following items were deemed by the Chief Executive Officer to be suitable for consideration in closed session in accordance with section 89 (2) of the Local Government Act 1989. In accordance with that Act, Council may resolve to consider these issues in open or closed session.

14.1 Open Space Opportunity

Agenda item 14.1 *Open Space Opportunity* is designated confidential as it relates to proposed developments (s89 2e)

14.2 Australia Day Award Nominations 2019

Agenda item 14.2 *Australia Day Award Nominations 2019* is designated confidential as it relates to any other matter which the Council or special committee considers would prejudice the Council or any person (s89 2h)

Confidential Appendices

10.1 CON 18/50 - Stormwater Drainage Maintenance

Appendix 1, CON 1850 Schedule of Items Analysis for Drain Cleaning and Inspection is designated confidential as it relates to (s89 2d)

10.7 Foreshore Shared Path Construction Project 1 - Naples Road, Mentone to Rennison Street, Parkdale

Appendix 1, Foreshore Shared Path - Naples to Rennison - Face Sheet of drawing CON-1863 is designated confidential as it relates to (s89 2d)

10.7 Foreshore Shared Path Construction Project 1 - Naples Road, Mentone to Rennison Street, Parkdale

Appendix 2, Contract 18-63 Foreshore Shared Path Naples to Rennison - Signed Tender Evaluation Summary is designated confidential as it relates to (s89 2d)

11.4 Tenders for Contract No 18/115 - Provision of Internal Audit Services

Appendix 1, CON-18 115 - Tender Evaluation Matrix is designated confidential as it relates to (s89 2d)

RECOMMENDATION

That in accordance with the provisions of section 89(2) of the *Local Government Act 1989*, the meeting be closed to members of the public for the consideration of the following confidential items:

14.1 Open Space Opportunity

This agenda item is confidential in accordance with the Local Government Act s89(2) as it relates to proposed developments (s89 2e)

14.2 Australia Day Award Nominations 2019

This agenda item is confidential in accordance with the Local Government Act s89(2) as it relates to any other matter which the Council or special committee considers would prejudice the Council or any person (s89 2h)

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10.1 CON 18/50 - Stormwater Drainage Maintenance

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This appendix is confidential in accordance with the Local Government Act s89(2) as it relates to (s89 2d)

10.7 Foreshore Shared Path Construction Project 1 - Naples Road, Mentone to Rennison Street, Parkdale

Appendix 2, Contract 18-63 Foreshore Shared Path Naples to Rennison - Signed Tender Evaluation Summary

This appendix is confidential in accordance with the Local Government Act s89(2) as it relates to (s89 2d)

11.4 Tenders for Contract No 18/115 - Provision of Internal Audit Services

Appendix 1, CON-18 115 - Tender Evaluation Matrix

This appendix is confidential in accordance with the Local Government Act s89(2) as it relates to (s89 2d)